

**ADMINISTRATIVE ACTION
FINDING OF NO SIGNIFICANT IMPACT**

Florida Department of Transportation

In cooperation with Federal Aviation Administration

Financial Management Number: 417540-1-22-01

Federal Project Number: 3911-022-P

FDOT Efficient Transportation Decision Making Project Number: 3752

SR 29 FROM OIL WELL ROAD TO SR 82, Collier County Florida

The location of the proposed action is a section of State Road (SR) 29 between Oil Well Road and SR 82 in Collier County, Florida. The total length of the project is 15.6 miles. The FDOT is proposing to widen existing two-lane undivided sections of SR 29 to four lanes from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road W to SR 82, as well as add a four-lane segment on new alignment from north of Seminole Crossing Trail to north of Westclox Street/New Market Road W. No improvements are proposed to existing SR 29 through the downtown area of Immokalee as part of this project.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated 05/26/2022 and executed by the Federal Highway Administration and FDOT.

The FDOT takes full responsibility for the accuracy, scope, and contents of the attached Environmental Assessment.

The FDOT Office of Environmental Management (OEM) has determined that this project will not have any significant impact on the human environment. The Finding of No Significant Impact is based on the attached Environmental Assessment which has been independently evaluated by FDOT OEM and determined to adequately and accurately discuss the environmental issues and impacts of the proposed project. It provides sufficient evidence and analysis for determining that an Environmental Impact Statement is not required.

06/19/2024

Date



Jennifer Marshall, P.E., Director, Office of Environmental Management
Florida Department of Transportation

The proposed action will not have any significant impact on the human environment. The project will result in one business relocation which will be carried out in accordance with Florida Statutes (F.S.) 421.55, Relocation of Displaced Persons, and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646 as amended by Public Law 100-17). Unavoidable wetland and habitat impacts will be mitigated in accordance with state and federal permitting requirements. Floodplain encroachments have been determined to be "Minimal" and will be offset with floodplain compensation areas. The proposed action may adversely impact but is not anticipated to result in jeopardy to the Florida scrub-jay, Florida panther, Audubon's crested caracara, and Florida bonneted bat based on the adherence to regulatory and mitigative requirements.

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FINDING OF NO SIGNIFICANT IMPACT SUMMARY

STATEMENT OF NO SIGNIFICANT IMPACT

The Florida Department of Transportation (FDOT) Office of Environmental Management (OEM) has determined that this project will not have a significant impact on the human environment pursuant to 40 Code of Federal Regulations (C.F.R.) § 1508.27. The Finding of No Significant Impact (FONSI) is based on the attached Environmental Assessment (EA), which has been independently evaluated by the FDOT OEM, and determined to adequately and accurately discuss the environmental issues and impacts of the proposed project. The EA provides sufficient evidence and analysis to determine that an Environmental Impact Statement is not required. The FDOT OEM takes full responsibility for the accuracy, scope, and contents of the EA. Determinations made by the Federal Highway Administration (FHWA) during the EA are noted as such throughout and are adopted by the FDOT OEM. This action is also based upon consideration of public comments received in response to the EA.

PROJECT DESCRIPTION

The location of the proposed action is a section of State Road (SR) 29 between Oil Well Road and SR 82 in Collier County, Florida (see EA, **Figure 1-1**). The total length of the project is 15.6 miles. The FDOT is proposing to widen existing two-lane undivided sections of SR 29 to four lanes from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road W to SR 82, as well as add a four-lane segment on new alignment from north of Seminole Crossing Trail to north of Westclox Street/New Market Road W. No improvements are proposed to existing SR 29 through the downtown area of Immokalee as part of this project (see EA, **Section 1.1**).

PURPOSE AND NEED

The purpose of this project is to improve traffic operational conditions along the SR 29 corridor between Oil Well Road and SR 82 to meet the following needs: accommodate future growth, reduce truck traffic in downtown Immokalee, correct current design deficiencies, improve mobility and connectivity within the regional transportation network, enhance economic competitiveness, and improve emergency evacuation capabilities (see EA, **Section 1.2**). SR 29 is designated as a Strategic Intermodal System (SIS) highway corridor for the entire length of the project. Additionally, SR 29 is classified as a rural principal arterial from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road W to SR 82; the roadway is also classified as an urban principal arterial from south of Farm Worker Way to north of Westclox Street/New Market Road W. Overall, the proposed action is expected to improve traffic operations, current design deficiencies, and safety conditions throughout the corridor.

PREFERED ALTERNATIVE

The Preferred Alternative is Central Alternative #2 (see EA, **Figure 2-4**). It provides a four-lane divided typical section with travel lanes varying between 11 feet and 12 feet wide. The right-of-way (ROW) width, the median type and width, and bicycle and pedestrian accommodations vary along the extent of the Preferred Alternative. The Preferred Alternative follows existing SR 29 from the start of the project at Oil Well Road to south of CR 846. From this point, the Preferred Alternative travels north from SR 29 on a new alignment (SR 29 Bypass) along the west side of the Immokalee Regional Airport to avoid impacts to the commercial/industrial areas of Immokalee, the State Farmers Market to the west, and Immokalee Airport Park. The Preferred Alternative then turns to the northwest just past Gopher Ridge Road to parallel Madison Avenue and Westclox Street/New Market Road W. It then travels along the east side of Collier Health Services Medical Center and the Florida State University College of Medicine before reconnecting to SR 29 north of Westclox Street/New Market Road W (the SR 29 Bypass Junction). The Preferred Alternative follows the existing alignment of SR 29 from north of Westclox Street/New Market Road W to the project terminus near SR 82. Subsequent to the Public Hearing, design refinements were made to the Preferred Alternative to meet the FDOT Design Manual (FDM) requirements and include the identification of stormwater management facilities (SMF), necessary to accommodate stormwater runoff, from CR 846 to SR 82. Partial two-lane roundabouts are proposed at SR 29 and CR 846, SR 29 and Alachua Street/Gopher Ridge Road, and SR 29 and Westclox Street/New Market Road W. The Preferred Alternative is described in more detail in **Section 2.6** of the EA with Typical Sections provided in **Appendix B** and **Appendix P** of the EA. Conceptual roadway plans are provided in **Appendix C** and **Appendix K** of the EA.

The discussion of impacts associated with the Preferred Alternative follows the same order as the EA. Environmental resources, including Aquatic Preserves and Outstanding Florida Waters, Wild and Scenic Rivers, Coastal Barrier Resources, Essential Fish Habitat, and Navigation are not present in the project study area and will not be affected.

SOCIAL AND ECONOMIC

SOCIAL

The analysis in the EA (**Section 3.1.1**) concluded that the Preferred Alternative will not result in significant impacts to any minority, ethnic, elderly, or handicapped groups, and/or low-income populations. The proposed action was developed without regard to race, color, national origin, age, sex, religion, disability, or family status in accordance with Title VI of the Civil Rights Act of 1964. The Preferred Alternative was designed to avoid impacts to residential areas. Therefore, no splitting or isolation of neighborhoods is anticipated to occur and no community services are anticipated to be displaced as a result of the proposed improvements. In addition, adverse secondary and cumulative impacts are not anticipated as the Preferred Alternative specifically avoids residential areas and community services as well as limits business impacts and relocations while maintaining community cohesion through increased access and improved safety in the corridor. No comment has been received to date regarding conflicts with Title VI of the Civil Rights Act of 1964 or related statutes. Extensive public outreach efforts were conducted and summarized in **Section 4.0** of the EA and in the *Comments and Coordination Report* (May 2020) and *Comments and Coordination Report Addendum* (June 2024).

ECONOMIC

The analysis in the EA (**Section 3.1.2**) concluded that the Preferred Alternative will not result in significant impacts to economic factors. The Preferred Alternative is anticipated to result in one business relocation. However, the project is anticipated to improve the local and regional area economies by improving access to local agricultural and ranching operations, commercial businesses, and freight activity centers. Therefore, the proposed project will enhance economic resources.

LAND USE CHANGES

The analysis in the EA (**Section 3.1.3**) concluded that the Preferred Alternative will not result in significant impacts to land use. The proposed widening of SR 29 is consistent with the Collier MPO's adopted 2045 Long Range Transportation Plan Cost Feasible Plan and aligns with the vision and goals of the Immokalee Area Master Plan. No adverse changes to surrounding land uses are anticipated as a result of the project. The existing and future land uses in the project area will continue to be supported as well as enhanced as the proposed widening will improve access for nearby businesses, residents, and agricultural operations.

MOBILITY

The analysis in the EA (**Section 3.1.4**) concluded that the Preferred Alternative will enhance mobility in the area. The Preferred Alternative is anticipated to complement plans for the widening of other sections of the SR 29 corridor to the north and south. In turn, this will provide a continuous four-lane connection from I-75 to US 27 in Glades County, enhance access to regional north-south and east-west transportation corridors, enhance the circulation and movement of goods, accommodate future growth, and improve emergency evacuation and response capabilities. Most importantly, it will divert regional truck traffic trips from downtown Immokalee, creating a more pedestrian friendly environment.

AESTHETIC EFFECTS

The analysis in the EA (**Section 3.1.5**) concluded that the Preferred Alternative will not result in significant impacts to aesthetics. The Preferred Alternative is not anticipated to result in the alteration or obstruction of scenic views of agricultural lands or views from Florida scenic highways or byways as none are located within the project study area. To stay consistent with the redevelopment initiatives of the Collier County Community Redevelopment Agency for Immokalee (and subsequently, the Immokalee Area Master Plan), the FDOT Context Classification Handbook (August 2017) was used to develop the typical sections for the proposed project. As such, the Preferred Alternative will not negatively impact these redevelopment initiatives.

RELOCATION POTENTIAL

The analysis in the EA (**Section 3.1.6**) concluded that the Preferred Alternative will not result in any significant relocation impacts. The Preferred Alternative is anticipated to require the acquisition of additional ROW resulting in one business relocation and no residential relocations. Opportunity exists for the business to be relocated along the new alignment portion of the Preferred Alternative. The Preferred Alternative, including design refinements, will require 81.6 acres of additional ROW to accommodate the improved roadway and 103.6 acres of additional ROW to accommodate associated stormwater ponds and floodplain compensation sites.

FARMLANDS

The analysis in the EA (**Section 3.1.7**) concluded that the Preferred Alternative will not result in any significant impacts to farmlands. FDOT prepared a Farmland Conversion Impact Rating Form (NRCS-CPA-106) for the project, which was submitted to the Natural Resources Conservation Service (NRCS) (see **Appendix D** of the EA). In an email dated July 24, 2018, NRCS provided their evaluation and determined that the proposed action would impact 160.5 acres of farmlands of prime or unique importance; however, the total points in Part VII of the NRCS-CPA-106 Form (131.9 points) were below the significance threshold (160 points). Therefore, no further consideration of protection would be needed, no additional corridors would need to be evaluated, and no additional coordination with NRCS would be required.

CULTURAL

SECTION 4(f)

The analysis in the EA (**Section 3.2.1** and **Appendices E, L, and Q**) concluded that the Preferred Alternative will not result in any impacts to Section 4(f) resources. A Section 4(f) *Determination of Applicability* (DOA) was prepared for the following four potential Section 4(f) resources: Collier Rural Land Stewardship Sending Area #5, 1st Street Plaza, 9th Street Plaza, and Immokalee Airport Park. The Section 4(f) DOA was submitted to FHWA who determined in an email dated June 6, 2013 that Immokalee Airport Park, 1st Street Plaza, and 9th Street Plaza are Section 4(f) resources. Immokalee Airport Park is within the project limits. The other two Section 4(f) resources are no longer within the project limits. A subsequent Section 4(f) DOA (Form No. 650-050-45) was completed for the Airport Viewing Area; FDOT OEM determined on June 26, 2018 that Section 4(f) does not apply to this resource.

Based upon comments received at the Public Hearing and further coordination with Collier County, the Preferred Alternative was modified to completely avoid impacts to Immokalee Airport Park. As such, FDOT completed a Section 4(f) *No Use Determination* (Form No. 650-050-49) for the Immokalee Airport Park; FDOT OEM determined on May 20, 2019 that there would be “No Use” of this resource.

Additional design refinements were made to the Preferred Alternative subsequent to the Public Hearing to meet the FDM requirements and include the identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82. As a result of the design refinements and associated SMFs, additional coordination with Collier County was initiated and a letter was submitted on February 14, 2024 to confirm the Immokalee Airport Park boundary. Concurrence on the park boundary was received on March 5, 2024 (see **Appendix Q**). With this confirmation, it was determined that the Preferred Alternative design refinements and associated proposed SMFs would still result in “No Use” of the Immokalee Airport Park. Improved direct replacement access to the park will be provided as part of this project.

HISTORIC SITES/DISTRICTS AND ARCHAEOLOGICAL SITES

The analysis in the EA (**Sections 3.2.2** and **3.2.3** and **Appendices F, M, and R**) concluded that the Preferred Alternative will not result in significant impacts to historic sites/districts or archaeological sites. The proposed action will have no significant impact on archaeological sites as no previously recorded or newly recorded archaeological sites were identified within the

archaeological Area of Potential Effect (APE) as part of the *Cultural Resource Assessment Survey* (CRAS). In addition, all shovel tests were negative for the presence of cultural materials and no environmental features were identified indicative of archaeological site potential. The historic resources survey resulted in the identification of 46 historic resources within the historic APE (two previously recorded resources and 44 newly recorded resources). The previously recorded resources include the Immokalee Ice Plant (8CR642) and the Immokalee Regional Airport (8CR1087). The 44 newly recorded resources include 35 buildings, two bridges, four canals, one road, and two resource groups. One of the previously identified resources, the Immokalee Ice Plant (8CR00642), is considered eligible for listing in the National Register of Historic Places (National Register). The proposed action will not directly or indirectly result in adverse impacts to this resource or diminish its integrity. The remaining 45 historic resources identified are considered National Register-ineligible. The State Historic Preservation Officer (SHPO) concurred with FDOT's determination that the proposed undertaking would have *No Adverse Effect* on historic properties on August 9, 2018. On February 19, 2020, the Seminole Tribe of Florida Tribal Historic Preservation Officer indicated that they have no objections to the project at this time and requested that they be notified of any archaeological, historical, or burial resources that are inadvertently discovered as the project advances. The Miccosukee Tribe of Florida indicated during the Efficient Transportation Decision Making (ETDM) Programming Screen that no further coordination was necessary if no archaeological sites were to be impacted. Since no archaeological sites were discovered, further coordination with the Miccosukee Tribe was not initiated. However, they were notified of all public meetings.

A *CRAS Addendum Report* (February 2024) was prepared as a result of design refinements to the Preferred Alternative to meet the FDM requirements and identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82. The analysis concluded that the Preferred Alternative will not result in significant impacts to archaeological sites as no previously recorded or newly recorded archaeological sites were identified within the archaeological APE. In addition, all shovel tests were negative for the presence of cultural materials and no environmental features were identified indicative of archaeological site potential. The historic resources survey resulted in the identification of two new resources that were recorded and evaluated: a ca. 1971 Mid-Century Modern style building (8CR01645) and a ca. 1970 Masonry Vernacular style building (8CR01646). Both buildings lack sufficient architectural features and are not significant embodiments of a type, period, or method of construction. In addition, a 0.25-mile segment of SR 29 (8CR01309) was updated within the APE. This linear resource is the same design as the segments of SR 29 which were determined National Register-ineligible. These three resources are not National Register-eligible, either individually or as a part of a historic district. Therefore, the proposed undertaking will result in no historic properties affected. The SHPO concurred with FDOT's findings that the project will result in no historic properties affected on March 21, 2024 (see **Appendix R**).

NATURAL

WETLANDS AND OTHER SURFACE WATERS

The analysis in the EA (**Section 3.3.1**) concluded that the Preferred Alternative will not result in significant impacts to Wetlands or Other Surface Waters. The proposed action was developed to avoid and/or minimize impacts to wetlands and other surface waters to the greatest extent

practicable. The Preferred Alternative will result in an estimated total of 14.48 acres of wetland impacts and 18.36 acres of other surface water impacts. A Uniform Mitigation Assessment Method (UMAM) analysis was performed, in accordance with Chapter 62-345, Florida Administrative Code (F.A.C.), to estimate the loss of wetland function due to project impacts. Per the UMAM analysis, the proposed action is expected to result in 10.47 units of functional loss. All practicable measures will be taken to further minimize harm to wetlands during subsequent stages of project development. Unavoidable wetland impacts will be mitigated pursuant to Section 373.4137, F.S., to satisfy all mitigation requirements of Part IV Chapter 373, F.S. and Title 33, U.S.C., Section 1344. The FDOT will explore and consider all appropriate and available mitigation options that satisfy state and federal requirements as agreed to by the applicable regulatory agencies including, but not limited to, the use of mitigation banks. The proposed action is located entirely within the service areas of several approved mitigation banks that currently have wetland credit availability, including Corkscrew Regional Mitigation Bank, Big Cypress Mitigation Bank, Panther Island Mitigation Bank, and Panther Island Expansion Mitigation Bank. The proposed project was evaluated for potential wetland impacts in accordance with Executive Order 11990, Protection of Wetlands. Based upon the above considerations, it is determined that there is no practicable alternative to the proposed construction in wetlands and that the proposed action includes all practicable measures to avoid and minimize harm to wetlands which may result from such use.

WATER QUALITY AND QUANTITY (STORMWATER)

The analysis in the EA (**Section 3.3.3**) concluded that the Preferred Alternative will not result in significant impacts to water quality and quantity (stormwater). Drainage along the existing roadway is accomplished through collection and conveyance by open roadside ditches, side drains, ditch bottom inlets, and cross drains. The stormwater runoff from the proposed action between north of Seminole Crossing Trail and CR 846 will be collected and conveyed to SMFs via curb, gutter, and pipes. Stormwater runoff for the remainder of the Preferred Alternative will be conveyed to the SMFs by an open drainage system for treatment and attenuation. Treatment and attenuation will be achieved through the construction of offsite wet ponds, which will require the acquisition of additional ROW. The proposed SMFs will be designed to include, at a minimum, the water quantity requirements for water quality impacts as required by the South Florida Water Management District (SFWMD), meeting state water quality and quantity requirements. Best management practices will also be utilized during construction.

FLOODPLAINS

The analysis in the EA (**Section 3.3.5**) concluded that the Preferred Alternative will not result in significant impacts to floodplains. The proposed action was developed to avoid and/or minimize impacts to floodplains to the greatest extent practicable. Floodplain impacts are anticipated due to the proposed widening of SR 29, the proposed widening of New Market Road, the proposed new alignment of a portion of SR 29, and proposed SMFs. Total floodplain encroachment for the proposed action is 27.84 acre-feet and is rated “Minimal” as outlined in the FDOT Project Development and Environment (PD&E) Manual and detailed in the *Location Hydraulic Report* (August 2018) and *Location Hydraulic Report Addendums* (March 2024). Through design and permitting of the project, the proposed drainage systems will perform hydraulically in a manner equal to or greater than the existing conveyance systems, and surface water elevations are not expected to increase upstream or downstream of the project limits. As a result, there will be no significant adverse impacts on natural and beneficial floodplain values. There will be no significant

change in flood risk, and there will not be a significant change in the potential for interruption or termination of emergency service or emergency evacuation routes. Floodplain compensation areas were identified for the Preferred Alternative to offset the minimal impacts identified. Therefore, it has been determined that this encroachment is not significant.

PROTECTED SPECIES AND HABITAT

The analysis in the EA (**Section 3.3.8** and **Appendices I, J, N, S, and T**) concluded that the Preferred Alternative will not result in significant impacts to protected species and habitat. A list of threatened and endangered species with the potential for occurrence within the project study area was compiled based on research and coordination with federal and state agencies. **Table 3-4** in **Section 3.3.8** of the EA summarizes the effect determinations of the identified species. These are also documented in the *Natural Resources Evaluation* (NRE) (July 2018) prepared for this project. The FDOT originally determined that the proposed action “*may affect, likely to adversely affect*” (*MALAA*)¹ the Florida scrub-jay and Florida panther. Based upon correspondence with the United States Fish and Wildlife Service (FWS) received on March 20, 2018 (see **Appendix I**), the FDOT committed to re-initiating Section 7 consultation with the FWS during the project’s design and permitting phase for the Florida scrub-jay and Florida panther. The proposed action “*may affect, not likely to adversely affect*” (*MANLAA*) the American alligator, Eastern indigo snake, Florida bonneted bat, wood stork, Audubon’s crested caracara, and snail kite. It was further determined that the proposed action will have “*no effect*” on the Florida grasshopper sparrow, red-cockaded woodpecker, Florida prairie-clover, and Garber’s spurge. In addition to the federally-listed species referenced above, the proposed action was also evaluated for impacts to state-listed species. It has been determined that there is “*no adverse effect anticipated*” for the Florida burrowing owl, little blue heron, tricolored heron, Southeastern American kestrel, gopher tortoise, Florida sandhill crane, roseate spoonbill, Big Cypress fox squirrel, pine woods bluestem, many flowered grass pink, sand butterfly pea, nodding pinweed, Small’s flax, Florida spiny-pod, celestial lily, Florida beargrass, yellow fringeless orchid, and coastal hoary-pea. The *NRE* was submitted to the FWS and the Florida Fish and Wildlife Conservation Commission (FWC) on July 20, 2018. The FWS responded via email on August 3, 2018 stating that they would respond to all species determinations at the time of re-initiation of Section 7 consultation during the final design and permitting phase and had no other comments on the project. On August 2, 2018, the FDOT received a comment from the FWC that the *NRE* did not specifically identify or discuss potential impacts of the project to the Immokalee Regional Airport Upland Management Area (UMA) and, consequently, impacts to habitat of the Florida scrub jay and gopher tortoise.

An *NRE Addendum* (August 2018) was prepared to address the comment from FWC and submitted to agencies for review on August 9, 2018. Findings and species effect determinations documented in the *NRE Addendum* remained consistent with the *NRE*. The FWC responded providing their agreement with the determinations in a letter dated August 21, 2018. Correspondence received from both FWS and FWC on the *NRE* and the subsequent addendum is included in **Appendix J**.

Subsequent to agency review and concurrence with the *NRE* and *NRE Addendum*, two additional addendums were prepared and are discussed below.

¹ Nomenclature for species effect determinations has changed from preparation of the July 2018 *NRE* and through the subsequent *NRE Addendums*. Species effect determination nomenclature for year 2023 is presented.

A second *NRE Addendum* (August 2019) was prepared after the Public Hearing to address potential project impacts to the Florida scrub-jay and gopher tortoise resulting from Preferred Alternative alignment refinements within the same corridor through the Immokalee Regional Airport UMA. The addendum updated acreages of impact to suitable habitat for the Florida scrub-jay and gopher tortoise. The findings and species effect determinations remained the same as the July 2018 *NRE* and August 2018 *NRE Addendum*. This addendum was submitted to agencies for review on August 9, 2019. The FWC concurred with the noted findings in a letter dated September 4, 2019 (see **Appendix N**).

A third *NRE Addendum* (September 2021) was prepared to initiate formal consultation with the FWS prior to the design and permitting phase. This addendum includes a summary of all species with prior and updated effect determinations, as well as the addition of the Eastern black rail. The third addendum also includes the Biological Assessment which addresses the prior *MALAA* determinations for the federally-listed Florida panther and Florida scrub-jay. The FDOT revised the effect determinations to *MALAA* for the following federally-listed species based upon updated literature and database searches, field reviews, and species-specific surveys: Eastern indigo snake and Florida bonneted bat. On November 17, 2021, pursuant with Section 7 of the *Endangered Species Act of 1973* (ESA), as amended, the FDOT OEM requested initiation of formal consultation with the FWS for the following federally-listed species: Florida panther, Florida scrub-jay, Eastern indigo snake and Florida bonneted bat. In addition, FDOT requested concurrence with the prior and updated “no effect” and *MANLAA* determinations as documented in the *NRE*.

On May 24, 2022 and May 25, 2022, the FWS responded to the request for formal consultation by submitting Requests for Additional Information (RAI) to the FDOT OEM. Through the RAI, the FWS recommended that the determination for the Eastern indigo snake be changed from *MALAA* to “no effect” as this species is not reasonably certain to occur within the project corridor. In addition, the FWS recommended that the determination for Audubon’s crested caracara be modified from *MANLAA* to *MALAA* given that there is a documented active nest located approximately 279 feet west of the project footprint and the project will result in habitat loss within the Primary Zone of this nest. The FDOT OEM provided responses to the RAI on December 12, 2023 (see **Appendix S**). Through follow-up coordination with the FWS, the FDOT committed to re-initiating Section 7 consultation for the Audubon’s crested caracara. The commitment is in addition to the prior commitment to re-initiate Section 7 consultation with the FWS for the Florida panther, Florida scrub-jay, and Florida bonneted bat during the project’s design and permitting phase. The FWS provided concurrence on March 8, 2024 (see **Appendix T**).

Table 3-5 in **Section 3.3.8** of the EA summarizes the effect determinations for those federally-listed species where *MALAA* has been assigned or where the effect determinations have changed as a result of further agency coordination that has taken place since the Public Hearing.

To comply with Section 7 of the ESA, the FDOT will follow required mitigation measures and has added specific commitments concerning the listed species that are included in **Section 5.0** of the EA.

PHYSICAL

HIGHWAY TRAFFIC NOISE

The analysis in the EA (**Section 3.4.1**) concluded that the Preferred Alternative will not result in significant highway traffic noise impacts. For the 2045 design year, the Preferred Alternative is predicted to result in exterior traffic noise levels ranging from 47.1 to 65.7 decibels on the “A”-weighted scale (dB(A)), and interior levels are predicted at 42.6 dB(A) at the 100 noise-sensitive receptors identified within the limits of the proposed action. Of the 100 noise sensitive sites evaluated, none of the sites are predicted to experience future traffic noise levels that approach, meet, or exceed the Noise Abatement Criteria (NAC) for their respective Activity Category. The results of the analysis also indicated that when compared to existing conditions, traffic noise levels would not increase more than 9.8 dB(A) above existing conditions with the proposed improvements at any of the evaluated sites. As such, none of the evaluated sites will experience a substantial increase in traffic noise [15 dB(A) or more] as a result of the proposed action. These findings are documented within the *Noise Study Report* (July 2018).

A *Noise Study Report Addendum Report* (March 2024) was prepared as a result of design refinements to the Preferred Alternative to meet the FDM requirements and identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82. As part of this analysis, eighteen additional noise-sensitive receptors were identified within a new residential development along Foundation Way. With the proposed Preferred Alternative design refinements and associated proposed SMFs, exterior traffic noise levels are predicted to range from 44.7 to 61.6 dB(A). The levels are not expected to approach, meet, or exceed the NAC at any receptor under existing conditions; however, substantial noise level increases [15 dB(A) or more] are predicted for eight receptors within the new residential development under future conditions. Although traffic noise abatement measures were considered for the noise-sensitive receptors, no feasible and reasonable measures were identified that could be implemented as part of the project to abate traffic noise for the eight impacted receptors.

A specific commitment has been added to **Section 5.0** of the EA indicating that FDOT will conduct a land use review during the design phase to identify any noise sensitive sites that have been permitted prior to the Date of Public Knowledge and evaluate the sites for traffic noise and abatement considerations.

CONTAMINATION

The analysis in the EA (**Section 3.4.3**) concluded that the Preferred Alternative will not result in significant impacts from contamination sites. The environmental screening resulted in the identification of four sites ranked “High,” 30 sites ranked “Medium,” and 41 sites ranked “Low” or “No” for potential contamination within the Preferred Alternative corridor. For those sites with a risk ranking of “Medium” and “High”, including any proposed stormwater treatment ponds and/or floodplain compensation sites outside the FDOT ROW, Level II screening (which includes testing), as warranted, will be conducted during the design phase if it is determined that construction activities could encounter contamination or if the site will be subject to ROW acquisition. Options to remediate along with associated costs will also be evaluated. At known contamination sites, estimated areas of contamination will be marked on design drawings and resolution of problems will be coordinated with the appropriate regulatory agencies.

Contamination cleanup, as needed, will occur prior to or during construction. Any necessary remediation activities will be overseen by the FDOT.

UTILITIES AND RAILROADS

The analysis in the EA (**Section 3.4.4**) concluded that the Preferred Alternative will not result in significant impacts to utilities or railroads. A *Utility Request Package* was submitted to the Utility Agencies/Owners (UAOs) on June 8, 2018 to obtain the locations of existing and/or planned utilities. A *Utilities Assessment Package* was completed on February 6, 2019 and coordination was completed with the UAOs for potential utility conflicts to obtain relocation cost estimates. The Preferred Alternative is anticipated to result in utility relocations, but the project will be designed to avoid and minimize impacts to these features to the extent feasible. The FDOT will continue to coordinate with potentially affected utility owners throughout the design and construction phases. Cost estimates will be finalized in the design phase. There are no at-grade or grade-separated railroad crossings within the project study area.

CONSTRUCTION

The analysis in the EA (**Section 3.4.5**) concluded that the Preferred Alternative will not result in significant impacts from construction. Construction activities for the proposed SR 29 improvements will have minor air, noise, vibration, traffic flow, and visual impacts for those residents and travelers within the immediate vicinity of the project which will be minimized with adherence to applicable provisions in the FDOT's *Standard Specifications for Road and Bridge Construction*. Access to local properties, businesses, and residences will be maintained to the extent practical through controlled construction scheduling and the implementation of the project's specific Traffic Control Plan(s).

BICYCLE AND PEDESTRIANS

The analysis in the EA (**Section 3.4.6**) concluded that the Preferred Alternative will not result in significant impacts to bicyclists and pedestrians. The Preferred Alternative is proposed to enhance and/or add pedestrian and bicycle facilities. The sidewalk and bicycle facilities will be designed and constructed to comply with the *Americans with Disabilities Act (ADA) of 1990*, as amended. The sidewalks will meet ADA requirements for access, width, and grade.

PUBLIC INVOLVEMENT

The public involvement information for the SR 29 Immokalee PD&E Study is summarized within **Section 4.0** of the EA and in the *Comments and Coordination Report* (May 2020) and *Comments and Coordination Report Addendum* (June 2024). Throughout the PD&E Study, the FDOT has participated in numerous coordination meetings with FHWA, Federal Aviation Administration (FAA), Collier County, the Collier Metropolitan Planning Organization Board and its Committees, the Immokalee Community Redevelopment Agency, a Stakeholders Advisory Committee, government and non-government agencies, landowners, and the public to solicit input on the project. Agency coordination for the project was initiated in 2005 through Florida's ETDM process. Coordination continued throughout the project at key decision points and through the review of technical documentation for agency specific resources.

Major milestone public meetings for the project are as follows:

- Agency and Public Purpose and Need Scoping Meetings – October 18, 2007
- Corridor Public Workshop – August 7, 2008
- Alignment Public Workshop – June 23, 2009
- Public and Agency Alternatives Scoping Meetings – February 17 & February 18, 2010
- Alternatives Public Workshop – April 3, 2014
- Alternatives Public Workshop #2 – November 9, 2017
- Public Hearing – November 15, 2018
- Project Update: FDOT In-Person Office Hours – April 18, 2024
- Project Update: FDOT Live Online Office Hour – April 23, 2024

The two noted Project Update: FDOT Office Hour events (April 18, 2024 in-person event; April 23, 2024 live online event) were held after the Public Hearing to inform the community and answer questions about the design refinements to the PD&E Study Preferred Alternative, including the identification of SMFs necessary to accommodate stormwater runoff from CR 846 to SR 82. The In-Person Office Hours event was attended by 32 people. A total of 22 people attended the Online Office Hour. Questions and comments generally cited during the two events pertained to access, safety, proximity of the new roadway to existing and planned development, concept plans, drainage, schedule, and correct project contacts. Citizens were informed of the events through a newsletter that was mailed and emailed, a press release, a Florida Administrative Register notification, a display ad in the Immokalee Bulletin, social media posts, and flyers displayed at eight venues around Immokalee frequented by the community (such as restaurants, retail establishments, community centers, etc.).

Spanish translators were present at the milestone meetings; Creole translators were available upon request. In addition, seven newsletters (in both English and Spanish) were distributed to public officials, property owners, and interested parties between September 2007 and March 2024. A project website (www.sr29collier.com) was also created and maintained throughout the duration of the PD&E Study; content on the website was provided in both English and Spanish. All public comments have been considered and substantive comments have been addressed. The comments are documented in the *Comments and Coordination Report* and *Comments and Coordination Report Addendum*.

COMMITMENTS

The project commitments are listed in **Section 5.0** of the EA and duplicated in the *Preliminary Engineering Report* (PER) (prepared under separate cover), which are transmitted to the Project Manager of subsequent phases as the project progresses. Additional commitments were added after the Public Hearing to address the FAA's review of the EA and future coordination with them and Collier County; FWC also requested a commitment for mitigation regarding impacts to the Immokalee Regional Airport UMA. Based on further coordination with the FWS, the FDOT added specific commitments concerning the following federally-listed species: Florida scrub-jay, Florida panther, Florida bonneted bat, and Audubon's crested caracara.

PLANNING CONSISTENCY/PROJECT FUNDING

Since the Environmental Assessment was signed and approved for public availability by FDOT OEM on October 19, 2018 and the Public Hearing was held on November 15, 2018, planning consistency/funding information for the project was updated to reflect the latest documentation within local and state plans. The proposed action will meet local and state goals and objectives as it is consistent with the Collier Metropolitan Planning Organization's (MPO) 2045 Long Range Transportation Plan (LRTP), the Collier MPO's Transportation Improvement Program (TIP) FY 2024 – FY 2028 (June 9, 2023), and the FDOT Current State Transportation Improvement Program (STIP) (see **Section 1.3** of the EA). The most up to date Planning Consistency Tables prepared for each project design segment along with appropriate pages from the LRTP, TIP, and STIP are included in **Appendix A** of the attached EA.

ENVIRONMENTAL ASSESSMENT

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SECTION 1.0

PROJECT DESCRIPTION AND PURPOSE AND NEED

1.1 PROJECT DESCRIPTION

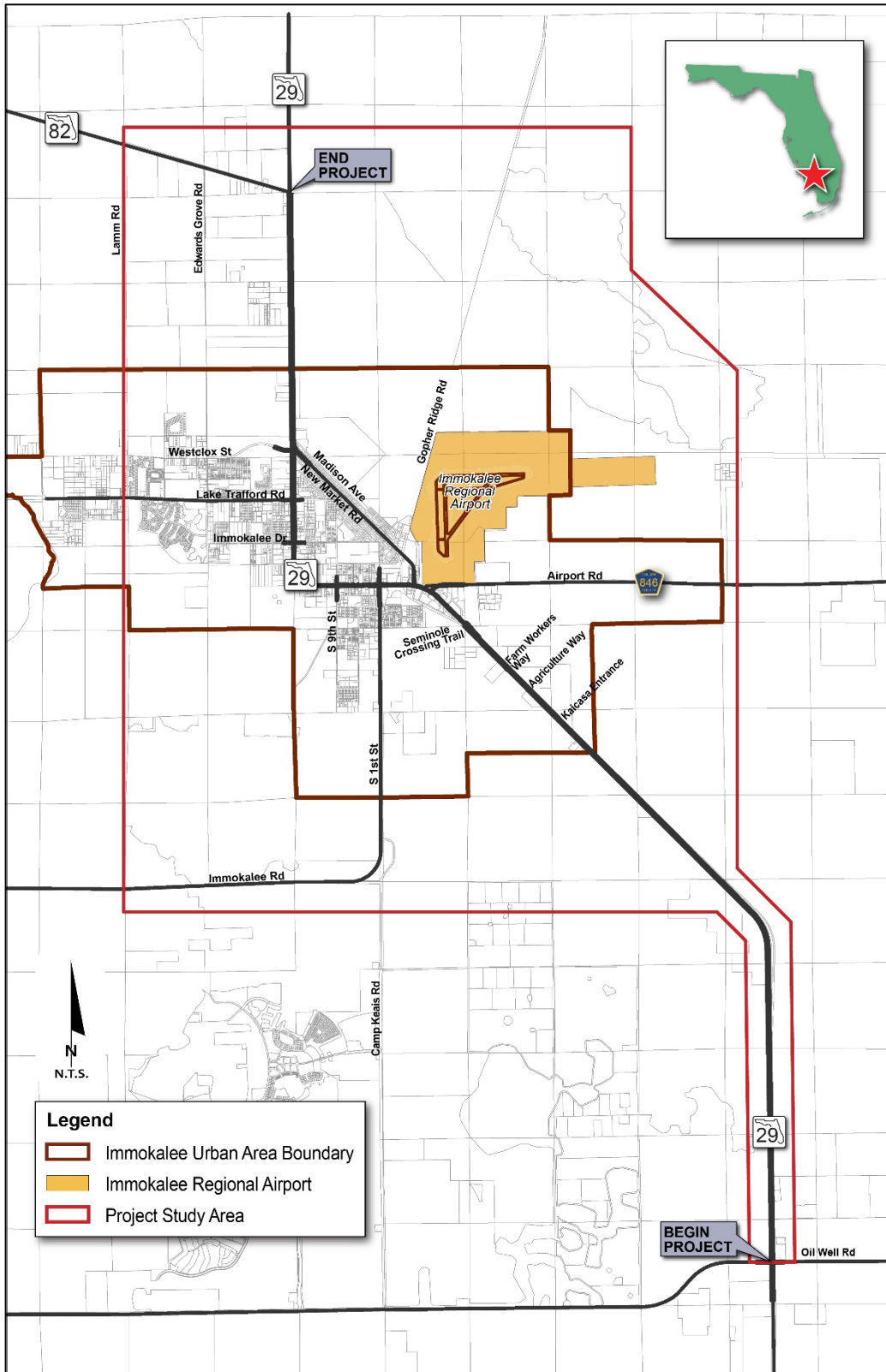
The Florida Department of Transportation (FDOT) District One conducted a Project Development and Environment (PD&E) Study, in accordance with the National Environmental Policy Act (NEPA), to assess the need for capacity and traffic operational improvements along a two-lane undivided section of State Road (SR 29) extending 15.6 miles from Oil Well Road (southern terminus) to SR 82 (northern terminus) in unincorporated Collier County, Florida. The project section of SR 29 specifically traverses the unincorporated community of Immokalee in eastern Collier County. **Figure 1-1** shows the location of the project.

This roadway project includes the proposed widening of existing two-lane undivided sections of SR 29 up to four lanes from Oil Well Road to north of Seminole Crossing Trail and from north of Westclox Street/New Market Road W to SR 82, as well as the addition of a four-lane segment on new alignment from north of Seminole Crossing Trail to north of Westclox Street/New Market Road W, bypassing the downtown area of Immokalee. No improvements are currently proposed to existing SR 29 through the downtown area of Immokalee as depicted on **Figure 1-2**.

The project segment of SR 29 is designated as a Strategic Intermodal System (SIS) highway corridor. Additionally, SR 29 is classified as a rural principal arterial from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road W to SR 82; the roadway is also classified as an urban principal arterial from south of Farm Worker Way to north of Westclox Street/New Market Road W. SR 29 is a major north-south corridor as it traverses the eastern portion of Collier County and the unincorporated community of Immokalee. Speed limits of 40 – 60 miles per hour (mph) are posted for the majority of the corridor. However, the speed limit is 35 mph from south of CR 846 to west of 9th Street due to frequent activity of commercial and agricultural trucks, as well as daily activity of pedestrians and bicyclists, using this section of SR 29.

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**FIGURE 1-1
PROJECT LOCATION MAP**



1.2 PURPOSE AND NEED

The purpose of this project is to improve traffic operational conditions along the SR 29 corridor between Oil Well Road and SR 82 to meet the following needs:

Accommodate Future Growth

Significant growth is anticipated to take place within the greater Immokalee area as indicated by the presence of the Town of Ave Maria Development of Regional Impact and a number of Planned Unit Developments. Based on 2010 U.S. Census Bureau data and projections developed for Collier County as part of the Collier Metropolitan Planning Organization's (MPO) 2040 Long Range Transportation Plan (LRTP), population within Collier County is projected to grow from 316,739 in 2010 to 497,702 in 2040 (57.1% increase). Likewise, Collier County employment is projected to grow from 170,862 in 2010 to 241,111 in 2040 (41.1% increase). According to the 2018 Design Traffic Technical Memorandum prepared for the project, the majority of the SR 29 corridor operates at or above the FDOT Levels of Service (LOS) C and D adopted for the roadway; only a small segment of the project corridor [from New Market Road to SR 82] operates below the adopted standard. However, if no improvements occur to the roadway, the majority of the SR 29 corridor is anticipated to operate under deficient conditions [with most segments operating at LOS F] by the 2045 design year. The improvement will:

- Enhance traffic operations and preserve operational capacity to accommodate projected travel demand spurred by increased growth as well as freight and commuter traffic [specifically truck traffic].
- Enhance the projected 2045 LOS for the corridor [with the exception of one segment that is anticipated to remain deficient].

Reduce Truck Traffic in Downtown Immokalee

Truck traffic currently represents 16.0% of the total volume of daily traffic along the SR 29 project segment. The Design Hour Truck is 8.0%; this is the percentage of trucks expected to use a highway segment during the 30th highest hour of the design year [2045]. Truck traffic in the corridor is projected to increase as a result of growth in the area. The project improvements will:

- Provide an alternative route for regional truck traffic trips.
- Enhance the livability of downtown Immokalee by reducing the conflicts between pedestrians/bicyclists and trucks, creating a more pedestrian friendly environment.
- Enhance the economic viability of downtown Immokalee.

Correct Current Design Deficiencies

The design of existing SR 29 is deficient given the present use of the roadway and current FDOT standards. The deficiencies include excessive access points, substandard curves limiting sight distances and design speeds, and locations with substandard shoulders and turn lanes. The proposed improvements will:

- Update the roadway to current design standards, increasing overall safety by reducing the potential exposure to conflict points associated with deficient existing design and access issues.
- Increase sight distances along the roadway.
- Provide sidewalks and bicycle lanes where none currently exist.

Improve Mobility and Connectivity within the Regional Transportation Network

SR 29 is a major central Florida interregional highway corridor as it traverses Collier, Hendry, and Glades Counties providing access to US 41 and I-75 to the south and SR 82, SR 80, and US 27 to the north. Through the southern portion of the state, SR 29 primarily runs parallel to other major north-south transportation facilities [I-75 and US 27]. In addition to I-75 and SR 82, SR 29 is part of Florida’s SIS network serving fast growing economic regions and a Rural Area of Opportunity. SR 29 is also one of four designated Freight Mobility Corridors in Collier County providing a north-south connection between I-75 and regional freight activity centers. The project improvements proposed along SR 29 are intended to:

- Complement plans to widen other sections of the SR 29 corridor to the north and south thereby 1) providing a continuous four-lane connection from I-75 to US 27 in Glades County, 2) alleviating a potential traffic bottleneck that could occur if no improvements take place on SR 29 from Oil Well Road to SR 82, and 3) improving the viability of SR 29 to serve as a parallel north-south alternative to north-south portions of I-75 and US 27.
- Enhance the circulation and movement of goods between existing and proposed freight facilities in south-central Florida. The SR 29 project improvements are an essential component of a unified approach that addresses the critical freight needs of the overall SR 29 corridor.
- Enhance access to major north-south facilities [I-75 and US 27] and connections to major east-west transportation corridors [SR 82], as well as residential and employment centers throughout Collier County.

Enhance Economic Competitiveness

On January 26, 2001, Immokalee was designated by Executive Order 04-250 as a Rural Area of Critical Economic Concern (now titled Rural Area of Opportunity). In addition to the Immokalee area being targeted for growth by Collier County, the area surrounding Collier County Immokalee Regional Airport is defined as a Primary Freight Activity Center as it supports industrial activities and agricultural packing and processing functions. A 60-acre portion of this area is a designated Foreign Trade Zone, a designation used to encourage activity and add value at facilities in competition with foreign companies. SR 29 also serves as a Strategic Intermodal System (SIS) highway corridor carrying high volumes of truck traffic and connecting to other SIS facilities [I-75 and SR 82]. This project will:

- Enhance the economic viability of the area by providing the infrastructure needed to bring additional businesses and employers into the area.
- Improve the circulation of goods as SR 29 serves as a key intrastate freight corridor providing access to local agricultural and ranching operations, as well as to fast growing economic regions located in central Florida and the populated coastal areas.

Improve Emergency Evacuation Capabilities

SR 29 is designated as a hurricane evacuation route by the Florida Division of Emergency Management. This facility is critical in evacuating residents of the eastern portion of Collier County. The project improvements will:

- Increase the capacity of traffic that can be evacuated during an emergency event.
- Enhance emergency response times.
- Enhance connections to other major arterials designated on the state evacuation route network, including SR 82 and north to US 27.

1.3 PLANNING CONSISTENCY

This project is consistent with the Collier Metropolitan Planning Organization’s (MPO) *2045 Long Range Transportation Plan* (LRTP), adopted on December 11, 2020, and is included in the Collier MPO’s *Transportation Improvement Program* (TIP) FY 2024 – FY 2028 (June 9, 2023). The FDOT Current State Transportation Improvement Program (STIP) includes the project as well. The most up to date Planning Consistency Tables prepared for each project design segment along with appropriate pages from the LRTP, TIP, and STIP are included in **Appendix A**. Planning consistency is summarized in **Tables 1-1 – 1-5**. **Figure 1-2** presents the project design segments.

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**TABLE 1-1
 PLANNING CONSISTENCY SUMMARY FOR FPID #417540-2:
 SR 29 FROM OIL WELL ROAD TO SUNNILAND NURSERY ROAD**

CURRENTLY ADOPTED CFP-LRTP	COMMENTS				
Yes	The Collier MPO 2045 LRTP was adopted in December 2020. This project is included in the Cost Feasible Plan (CFP), Table 6-1 – Collier MPO FY 2021 – FY 2025 TIP Summary. The latest Collier MPO TIP FY 2024 – FY 2028 was adopted June 9, 2023. This project is included in the TIP.				
PHASE	TIP/STIP	CURRENTLY APPROVED	\$	FY	COMMENTS
PE (Final Design)	TIP	Yes	\$7,440,000 \$7,440,000	FY 24 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$7,440,000 \$7,440,000	FY 24 All Years	
ROW	TIP	No	N/A	All Years	ROW is not programmed for this project.
	STIP	No	N/A	All Years	
CST	TIP	No	N/A	All Years	CST is not programmed for this project.
	STIP	No	N/A	All Years	

PE = Preliminary Engineering / ROW = Right-of-Way / CST = Construction.
 N/A = Not Available.

**TABLE 1-2
 PLANNING CONSISTENCY SUMMARY FOR FPID #417540-3:
 SR 29 FROM SUNNILAND NURSERY ROAD TO SOUTH OF AGRICULTURE WAY**

CURRENTLY ADOPTED CFP-LRTP	COMMENTS				
Yes	<i>The Collier MPO 2045 LRTP was adopted in December 2020. This project is included in the CFP, Table 6-1 – Collier MPO FY 2021 – FY 2025 TIP Summary. The latest Collier MPO TIP FY 2024 – FY 2028 was adopted June 9, 2023. This project is included in the TIP. Costs programmed <2024 are in prior TIP documents.</i>				
PHASE	TIP/STIP	CURRENTLY APPROVED	\$	FY	COMMENTS
PE (Final Design)	TIP	Yes	\$0	FY 24 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$9,177 \$9,177	FY 24 All Years	
ROW	TIP	No	N/A	All Years	ROW is not programmed for this project.
	STIP	No	N/A	All Years	
CST	TIP	No	N/A	All Years	CST is not programmed for this project.
	STIP	No	N/A	All Years	

PE = Preliminary Engineering / ROW = Right-of-Way / CST = Construction.
 N/A = Not Available.

**TABLE 1-3
 PLANNING CONSISTENCY SUMMARY FOR FPID #417540-4:
 SR 29 FROM SOUTH OF AGRICULTURE WAY TO CR 846 E**

CURRENTLY ADOPTED CFP-LRTP	COMMENTS				
Yes	<i>The Collier MPO 2045 LRTP was adopted in December 2020. This project is included in the CFP, Table 6-1 – Collier MPO FY 2021 – FY 2025 TIP Summary. The latest Collier MPO TIP FY 2024 – FY 2028 was adopted June 9, 2023. This project is included in the TIP. Costs programmed <2024 are in prior TIP documents.</i>				
PHASE	TIP/STIP	CURRENTLY APPROVED	\$	FY	COMMENTS
PE (Final Design)	TIP	Yes	\$0	FY 24 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$7,570 \$7,570	FY 24 All Years	
ROW	TIP	No	N/A	All Years	ROW is not programmed for this project.
	STIP	No	N/A	All Years	
CST	TIP	No	N/A	All Years	CST is not programmed for this project.
	STIP	No	N/A	All Years	

PE = Preliminary Engineering / ROW = Right-of-Way / CST = Construction.
 N/A = Not Available.

**TABLE 1-4
 PLANNING CONSISTENCY SUMMARY FOR FPID #417540-5:
 SR 29 FROM CR 846 E TO NORTH OF NEW MARKET ROAD W**

CURRENTLY ADOPTED CFP-LRTP	COMMENTS				
Yes	<i>The Collier MPO 2045 LRTP was adopted in December 2020. This project is included in the CFP, Table 6-1 – Collier MPO FY 2021 – FY 2025 TIP Summary. The latest Collier MPO TIP FY 2024 – FY 2028 was adopted June 9, 2023. This project is included in the TIP. Costs programmed <2024 are in prior TIP documents.</i>				
PHASE	TIP/STIP	CURRENTLY APPROVED	\$	FY	COMMENTS
PE (Final Design)	TIP	Yes	\$0	FY 24 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$560,000 \$560,000	FY 24 All Years	
ROW	TIP	Yes	\$349,493 \$7,063,557 \$7,413,050	FY 24 FY 25 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$1,172,778 \$7,063,557 \$8,236,335	FY 24 FY 25 All Years	
CST	TIP	No	N/A	All Years	CST is not programmed for this project.
	STIP	No	N/A	All Years	

PE = Preliminary Engineering / ROW = Right-of-Way / CST = Construction.
 N/A = Not Available.

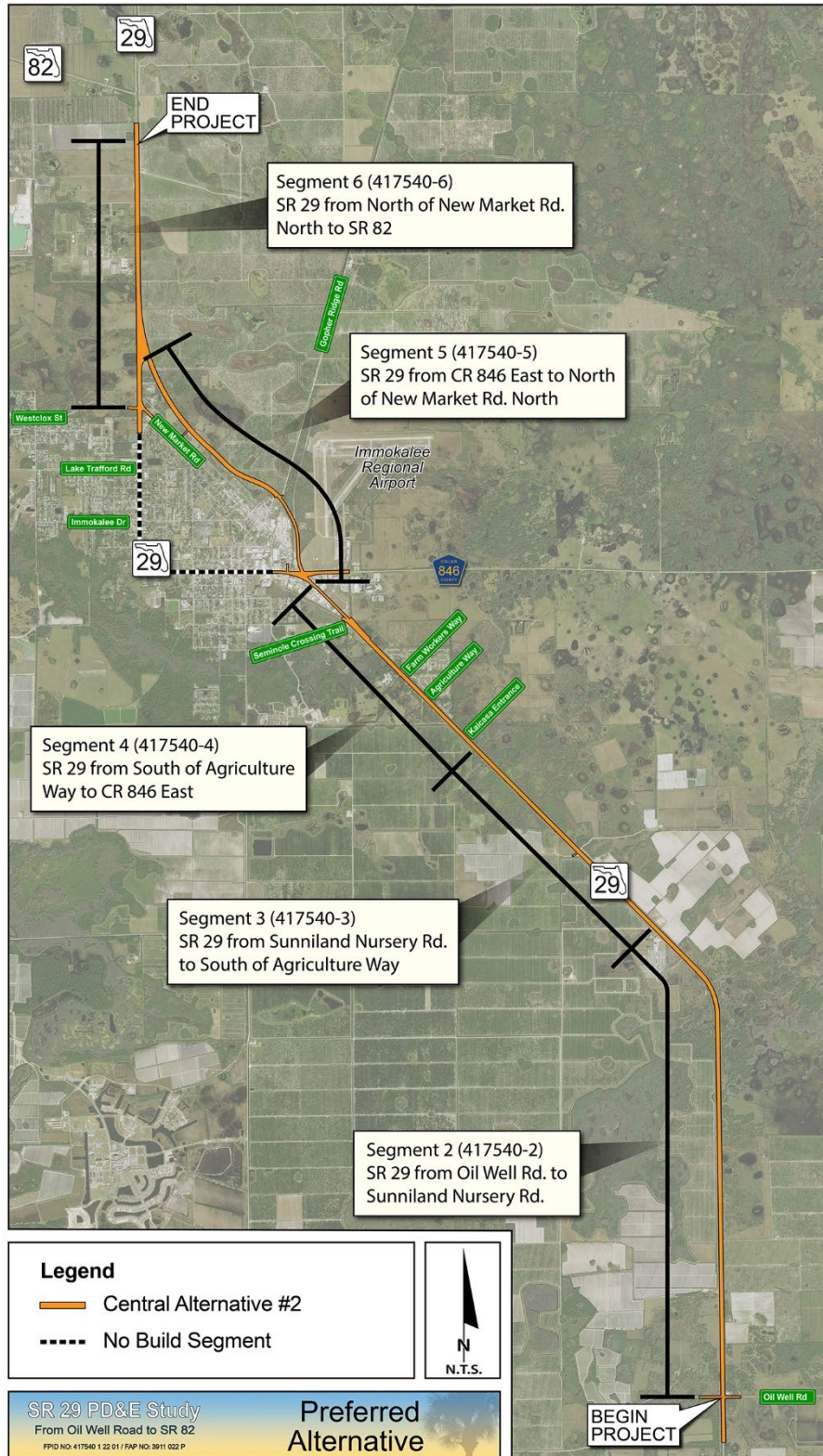
**TABLE 1-5
 PLANNING CONSISTENCY SUMMARY FOR FPID #417540-6:
 SR 29 FROM NORTH OF NEW MARKET ROAD W TO SR 82**

CURRENTLY ADOPTED CFP-LRTP	COMMENTS				
Yes	<i>The Collier MPO 2045 LRTP was adopted in December 2020. This project is included in the CFP, Table 6-1 – Collier MPO FY 2021 – FY 2025 TIP Summary. The latest Collier MPO TIP FY 2024 – FY 2028 was adopted June 9, 2023. This project is included in the TIP. Costs programmed <2024 are in prior TIP documents.</i>				
PHASE	TIP/STIP	CURRENTLY APPROVED	\$	FY	COMMENTS
PE (Final Design)	TIP	Yes	\$4,597,537 \$4,597,537	FY 24 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$4,597,537 \$4,597,537	FY 24 All Years	
ROW	TIP	Yes	\$1,121,956 \$1,253,897 \$2,375,853	FY 25 FY 26 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$247,956 \$875,000 \$1,253,897 \$2,376,853	FY 24 FY 25 FY 26 All Years	
CST	TIP	Yes	\$36,632,570 \$36,632,570	FY 27 All Years	Cost estimates between the TIP and STIP are consistent.
	STIP	Yes	\$43,203,295 \$43,203,295	FY 27 All Years	

PE = Preliminary Engineering / ROW = Right-of-Way / CST = Construction.
 N/A = Not Available.

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**FIGURE 1-2
PROJECT DESIGN SEGMENT MAP**



SECTION 2.0

ALTERNATIVES

2.1 ALTERNATIVES DEVELOPMENT

2.1.1 CORRIDOR ANALYSIS

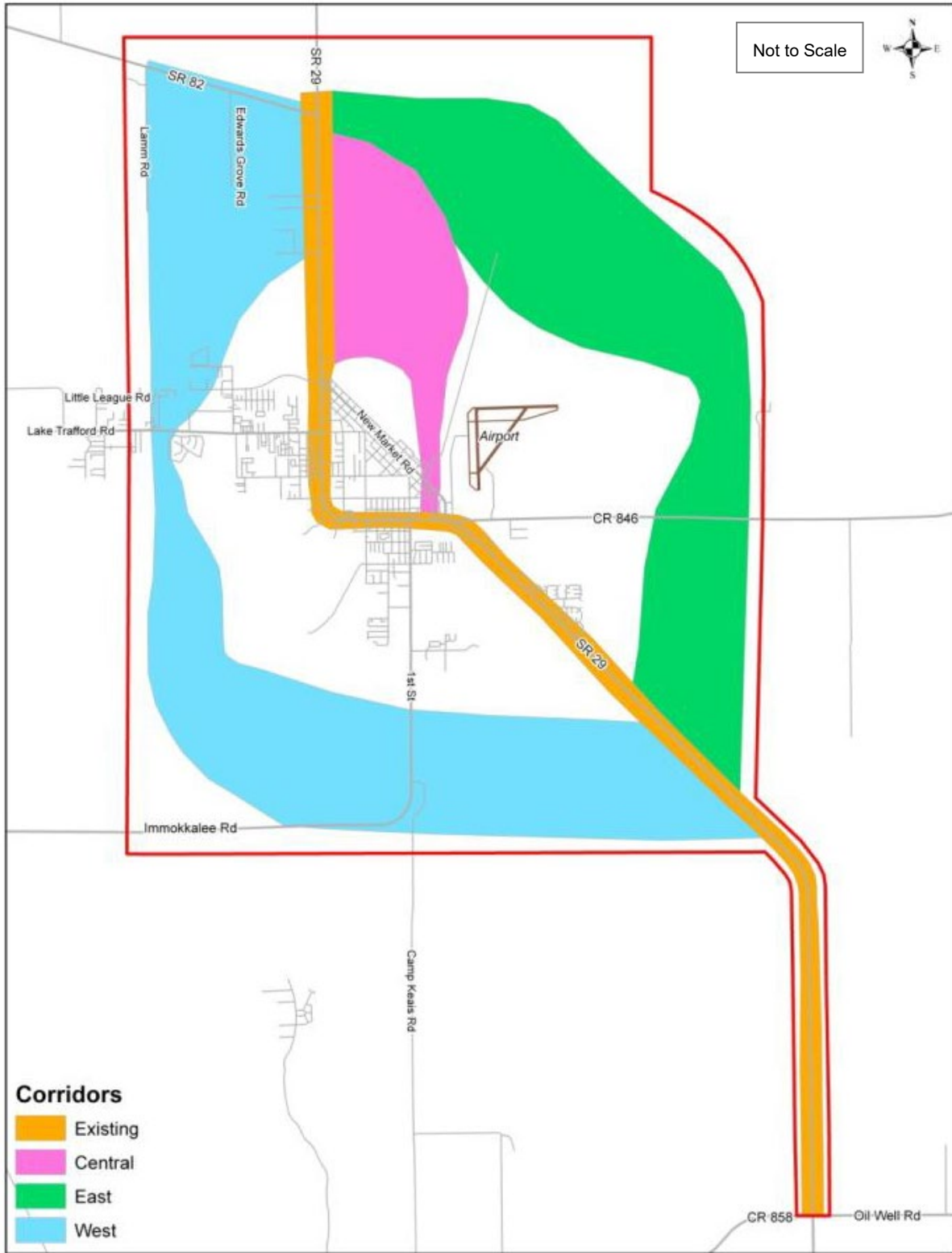
As part of the SR 29 Immokalee PD&E Study from Oil Well Road to SR 82, a *Corridor Evaluation Report* (March 2009) was prepared under separate cover and contains the full detail and results of the corridor evaluation. The need for the expansion of SR 29 in the study area was established based on the following criteria: accommodating future growth, reducing truck traffic in the downtown Immokalee area, correcting current design deficiencies, improving regional mobility and connectivity, enhancing economic competitiveness, and improving emergency evacuation capabilities. Based upon these criteria, corridor alternatives were developed and evaluated by identifying and mapping natural, physical, and socio-cultural features located within the project study area (see **Figure 2-1**). As the analysis advanced, these maps were refined to identify sensitive areas which should be avoided and areas in which impacts should be reduced to the greatest extent possible. After completion of the evaluation, it was determined that a greater level of analysis was needed before any corridor could be eliminated. The *Corridor Evaluation Report*, with the recommendation that all four study corridors (Existing, Central, East, and West) be advanced for further evaluation and analysis, was submitted to the Federal Highway Administration (FHWA), and they concurred with the findings and recommendation and agreed to move forward into the preliminary alignments development phase on April 6, 2009.

2.1.2 ALIGNMENT ANALYSIS

An *Alignments Report* (August 2010), prepared under separate cover, contains the full detail and results of the alignments evaluation. The report documents the history of the planning efforts of the project, the methodology and approach to the development of alignments within the corridors previously approved by FDOT and FHWA, the analysis and evaluation of the alignments developed, the outreach and involvement of the public and agencies, and the recommendations for alignments to be carried forward for the development of reasonable alternatives. A total of 31 alignments were considered: eight in the West Corridor, four in the Central Corridor, eighteen in the East Corridor, and the Existing Corridor. After analysis and feedback from the Stakeholders Advisory Committee (SAC), five representative alignments were selected for presentation at the June 23, 2009 Alignments Public Workshop. The representative alignments included:

- Alignment A (Existing Corridor),
- Alignment E (West Corridor),
- Alignment L (Central Corridor),
- Alignment S (East Corridor), and
- Alignment U (East Corridor).

**FIGURE 2-1
CORRIDOR ALTERNATIVES**



After conducting extensive public and agency outreach along with further analysis, the five representative alignments were reduced to the Existing and three modified alignments [Alignment HH (West Corridor), Alignment GG (Central Corridor), and Alignment FF (East Corridor)] (see **Figure 2-2**). These four alignments along with the No-Build, Transportation Systems Management and Operations (TSM&O), and Multimodal Alternatives were recommended for development and consideration as reasonable alternatives. The *Alignments Report* was submitted to FHWA and received approval on August 27, 2010.

2.1.3 ALTERNATIVE ANALYSIS

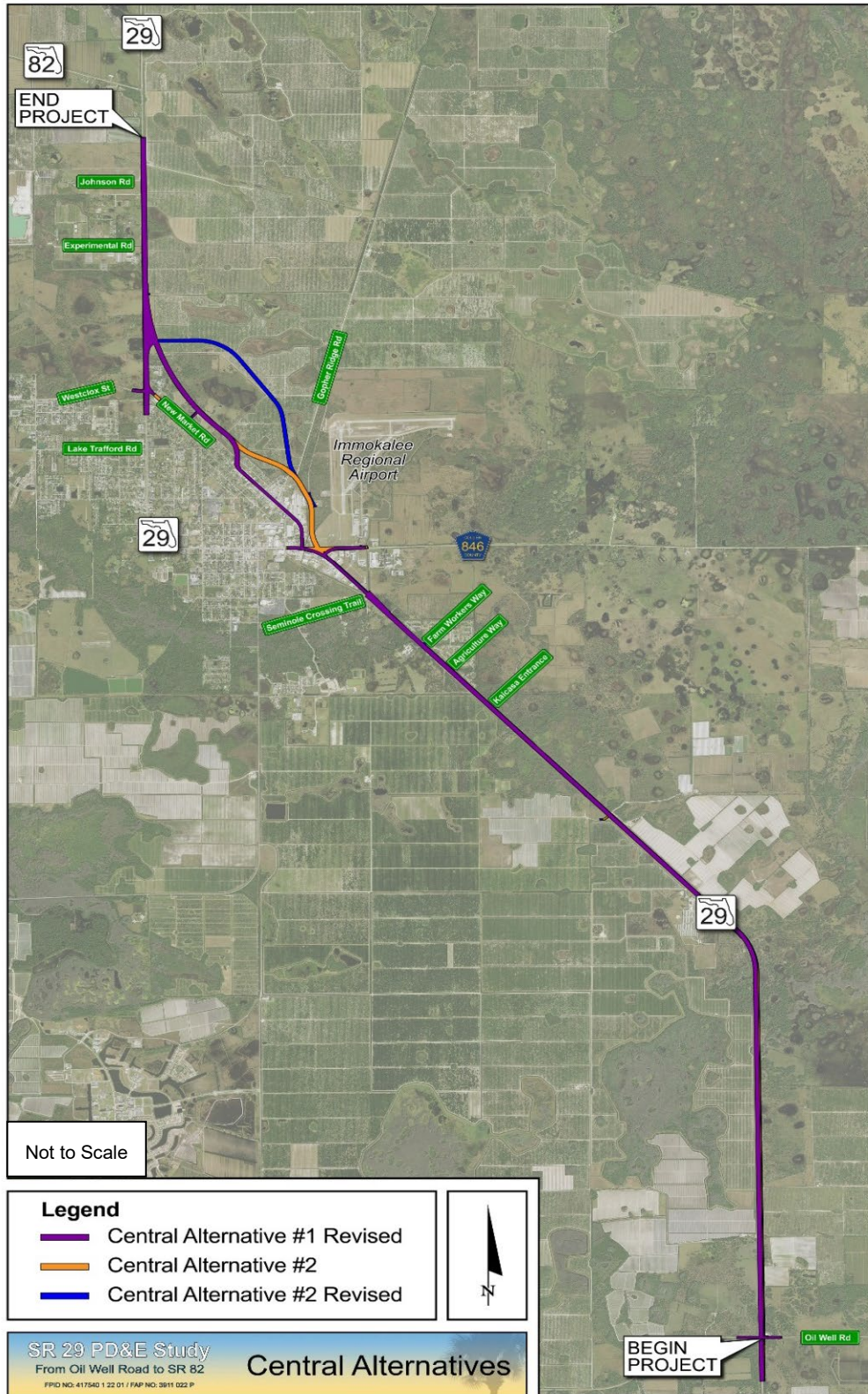
Based on refinements to the alignments at the conclusion of the Alignments Public Workshop, preliminary alternatives were developed. Coinciding with the preparation of the *Alignments Report*, an *Evaluation for Elimination of the West Preliminary Alternative Technical Memorandum* was prepared and concurred with by the FHWA on June 1, 2010. The decision to recommend the elimination of the West Preliminary Alternative was the result of direct impacts to natural resources, minority or low-income communities, public and agency comments, and estimated construction costs.

An *Alternatives Technical Report* (August 2014, revised February 2015) was prepared under separate cover and submitted to the FHWA, who concurred with the recommendation on February 16, 2015. The *Alternatives Technical Report* documented the analysis and elimination of alternatives along with the public and agency outreach. Preliminary alternatives included the following: No-Build Alternative, TSM&O Preliminary Alternative, Multimodal Preliminary Alternative, Existing SR 29 Alternative (from Alignment A), West Preliminary Alternative (from Alignment HH), Central Preliminary Alternative (from Alignment GG), Central Preliminary Alternative #1, East Preliminary Alternative (from Alignment FF), East Preliminary Alternative #1, and East Preliminary Alternative #2. Of these alternatives, six were eliminated and three were refined and recommended to advance: Central Alternative #1 Revised, Central Alternative #2, and Central Alternative #2 Revised; the No-Build Alternative was also advanced (see **Figure 2-3**).

2.2 ALTERNATIVES CONSIDERED BUT ELIMINATED

The TSM&O Alternative included analyzing intersection improvements and signal coordination to improve current and projected congestion on SR 29 from Oil Well Road to SR 82. The *Project Traffic Technical Memorandum* (September 2011), prepared under separate cover, identified a set of roadway improvements to existing SR 29 at eight specific locations along the corridor based upon projects identified in the Collier MPO's 2035 LRTP CFP. The Multimodal Alternative included analyzing existing, planned, and programmed transit service within the study area operated by Collier Area Transit (CAT), based on the improvements included in the Transit Development Plan developed in coordination with the Collier MPO's 2035 LRTP. This service included an existing CAT Route 5 that served Immokalee from other parts of the county at various times during the day. In addition, Routes 8a and 8b operated together as a circulator route that served Immokalee in a clockwise and counterclockwise loop. During a quarterly meeting with the FHWA on July 24, 2012, the TSM&O and Multimodal Alternatives were eliminated from further consideration due to their inability to meet the purpose and need for the project.

**FIGURE 2-3
ALTERNATIVES FOR FURTHER CONSIDERATION**



Upon further evaluation, the East Preliminary Alternatives were eliminated from further consideration. A letter documenting the justification for the elimination of the two East Preliminary Alternatives (East Preliminary Alternative #1 and East Preliminary Alternative #2) was prepared, and the FHWA concurred on December 18, 2013. The decision to recommend the elimination of the East Preliminary Alternatives from further evaluation was the result of direct and indirect effects to the endangered Florida panther and its habitat, direct and indirect effects to Section 106 and potential Section 4(f) resources, high estimated preliminary costs in comparison to other viable alternatives, and public and agency comments.

Coordination with FHWA regarding public comments received at the Alternatives Public Workshop #1 on April 3, 2014 and from project stakeholders after the workshop resulted in FHWA's concurrence with the elimination of the Existing SR 29 Alternative through the community of Immokalee on February 9, 2015. The Existing SR 29 Alternative was eliminated for the following reasons: did not satisfy the purpose and need of the project – specifically to reduce truck traffic in downtown; direct and indirect effects to cultural, historic, and Section 4(f) resources; and public comments.

The *Alternatives Technical Report* documented the analysis and elimination of the alternatives discussed above.

Following the Alternatives Public Workshop #2 held on November 9, 2017, Central Alternative #2 Revised was eliminated from further consideration based on the following findings:

- The location of Central Alternative #2 Revised is such that higher traffic volumes are expected along the existing SR 29 corridor and lower volumes are expected along the SR 29 Bypass as compared with the volumes of Central Alternatives #1 Revised and #2. As one of the project purposes is to divert traffic from existing SR 29 through downtown Immokalee, Central Alternative #2 Revised does not meet one of the study purposes.
- Central Alternative #2 Revised was the lowest ranked of the three Build Alternatives at Alternatives Public Workshop #2 in terms of public support.
- Central Alternative #2 Revised, which is similar in alignment and location to the formerly named “Central Alternative,” has historically not been supported by natural resource agencies due to its potential impacts to Florida panther habitat.
- Central Alternative #2 Revised impacts the largest portion of Florida panther habitat, floodplains, and potentially contaminated sites, and has the greatest potential for secondary and cumulative impacts.
- Central Alternative #2 Revised requires the most additional right-of-way of any Build Alternative.
- The estimated preliminary total costs for Central Alternative #2 Revised are the highest of the Build Alternatives.

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2.3 ALTERNATIVES CONSIDERED FOR ADDITIONAL STUDY

The three alternatives considered for additional study include: No-Build, Central Alternative #1 Revised, and Central Alternative #2 (see **Figure 2-3**).

NO-BUILD ALTERNATIVE

The No-Build Alternative assumes that no action will be taken to improve SR 29 within the project limits. This involves leaving the existing roadway as it is, with only routine maintenance as required.

Advantages of the No-Build Alternative include:

- No construction costs,
- No disruption to traffic due to construction,
- No disruption to the adjacent property owners due to construction,
- No right-of-way acquisitions or relocations, and
- No degradation or disruption of natural and other environmental resources due to construction.

Disadvantages of the No-Build Alternative include:

- Increased traffic congestion causing increased road user costs due to travel delay,
- Not consistent with the local transportation plans,
- Increased potential for vehicular crashes due to congested lanes and intersections,
- Increased emergency vehicle response times,
- Increased potential for crashes between vehicles and pedestrians/bicyclists due to inadequate sidewalks and bicycle lanes, and
- Increased vehicle emission pollutants due to higher levels of traffic congestion.

The No-Build Alternative will remain a viable alternative throughout this PD&E Study.

BUILD ALTERNATIVES

Both Build Alternatives (Central Alternative #1 Revised and Central Alternative #2) include a 4-lane divided typical section with travel lanes varying between 11 feet and 12 feet in width. Right-of-way, median type and width, and bicycle and pedestrian accommodations vary along the Build Alternatives.

The two alternatives are the same for much of their alignment, only diverging for approximately 1.3 miles on the east side of Immokalee by the airport. From the start of the project at Oil Well Road to north of Seminole Crossing Trail and from north of Westclox Street/New Market Road W to the end of the project south of SR 82, both alternatives follow the existing SR 29 corridor. The Build Alternatives differ in the following ways:

- **Central Alternative #1 Revised:** From Seminole Crossing Trail, Central Alternative #1 Revised remains on existing SR 29 to New Market Road. At New Market Road, this alternative follows the eastern portion of New Market Road and provides direct access to the agribusiness/commercial areas of Immokalee and State Farmers Market. This alternative

continues just past Flagler Street, then turns northward on new alignment to avoid a residential neighborhood. It then parallels New Market Road. At this point, the two Build Alternatives are on the same alignment. It then travels along the east side of Collier Health Services Medical Center and the Florida State University College of Medicine, before reconnecting to SR 29 north of Westclox Street/New Market Road W and continuing north to SR 82.

- **Central Alternative #2:** From Seminole Crossing Trail, Central Alternative #2 travels north from SR 29 on new alignment along the west side of the Immokalee Regional Airport to avoid the commercial/industrial areas of Immokalee and the State Farmers Market to the west. This alternative then turns to the northwest just past Gopher Ridge Road to parallel New Market Road. At this point, the two Build Alternatives are on the same alignment. It then travels along the east side of Collier Health Services Medical Center and the Florida State University College of Medicine, before reconnecting to SR 29 north of Westclox Street/New Market Road W and continuing north to SR 82.

2.4 COMPARATIVE ALTERNATIVES EVALUATION

The No-Build Alternative and the two remaining Build Alternatives (Central Alternative #1 Revised and Central Alternative #2) were evaluated based on environmental effects, right-of-way needs, project costs, and engineering factors. The matrix shown as **Table 2-1** provides the results of the alternatives evaluation process. The matrix quantifies considerations such as potential residential and business relocations, impacts to environmental resources, and the acres of right-of-way needed for roadway improvements and stormwater facilities. The potential for the proposed improvements to impact archaeological/historical sites, noise sensitive sites, and threatened and endangered species were also qualified in the matrix. The bottom half of the matrix details cost estimates for right-of-way acquisition, construction, design, and construction engineering and inspection. The estimates are based on 2018 unit costs. Costs for both design and construction engineering and inspection are estimated as 15% of the total construction cost. Construction costs were estimated in May 2018 using the FDOT’s Long Range Estimate (LRE) web-based computer system.

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**TABLE 2-1
ALTERNATIVES EVALUATION MATRIX**

Evaluation Criteria	No -Build Alternative	Central Alternative #1 Revised	Central Alternative #2
Design Features			
Length (miles)	15.59 miles	16.38 miles	16.38 miles
Traffic Control Measures	Stop Control and Traffic Signals	Traffic Signals & Roundabout	Traffic Signals & Roundabout
Travel Lane Width (feet)	12 feet	11 to 12 feet	11 to 12 feet
Posted Speed (miles per hour) - Subject to change pending speed study after construction	35 to 60 MPH	40 to 60 MPH	40 to 60 MPH
Right-of-Way Impacts			
Area of ROW to be Acquired for Roadway (acres)	0	56.18	77.82
Area of ROW to be Acquired for Stormwater Ponds/Floodplain Compensation Sites (acres)	0	102.07	104
Business Impacts			
Number of Business Relocations	0	9	1
Number of Parcels Impacted	0	20	4
Residential Impacts			
Number of Residential Relocations	0	3	0
Number of Parcels Impacted	0	2	0
Environmental Impacts			
Number of Historical Sites Impacted (National Register Listed/Eligible)	0	0	0
Number of Archaeological Sites Impacted (National Register Listed/Eligible)	0	0	0
Number of Public Recreational Facilities/Parks Impacted	0	0	1
Wetlands – Roadway (acres)	0	14.33	14.33
Surface Waters – Roadway (acres)	0	14.99	15.41
Floodplain Encroachment (acres)	0	25.36	25.36
Potential Involvement of Threatened and Endangered Species (none, low, medium, high)	None	Medium	Medium
Number of Potential Petroleum or Hazardous Materials Contaminated Sites	0	72 (34 Medium or High Risk)	67 (31 Medium or High Risk)
Number of Receivers Potentially Impacted By Noise	0	2	2
Estimated Total Project Costs (2018 cost)			
Engineering Design (15% of Construction Cost)	\$0	\$15,560,000	\$16,386,000
Wetland Mitigation ¹	\$0	\$1,800,000	\$1,800,000
Wildlife Habitat Mitigation ²	\$0	\$3,272,000	\$4,396,000
Utilities Relocation	\$0	\$0	\$0
ITS/ATMS Relocation	\$0	\$227,000	\$227,000
ROW Acquisition	\$0	\$16,830,000	\$18,300,000
Construction	\$0	\$103,732,000	\$109,241,000
Construction Engineering and Inspection (15% of Construction Cost)	\$0	\$15,560,000	\$16,386,000
Preliminary Estimate of Total Project Cost	\$0	\$156,981,000	\$166,736,000

¹ Wetland mitigation cost estimate based on FDOT Environmental Mitigation Payment Processing Handbook, Page 5, Fiscal Year 2021/2022 (\$125,594 per acre of impact).

² Wildlife habitat mitigation cost includes mitigation for Florida panther and Florida scrub-jay. Florida panther mitigation cost estimate based on \$850 per panther habitat unit (PHU). Florida scrub-jay mitigation cost estimate based on \$25,000 per acre of impact with assumed 2:1 mitigation cost ratio.

2.5 CENTRAL ALTERNATIVE #1 REVISED ELIMINATION

The comparative alternatives evaluation, as described in Section 2.4, led to the elimination of Central Alternative #1 Revised from additional study and the selection of Central Alternative #2 as the Recommended Alternative. Central Alternative #2 better satisfies the Purpose and Need of the project than Central Alternative #1 Revised in the following ways:

- Central Alternative #2 provides a more direct route than Central Alternative #1 Revised.
- Central Alternative #1 Revised has two more signalized intersections than Central Alternative #2 (one at SR 29 and New Market Road E and one at New Market Road E and Charlotte Street). Central Alternative #1 Revised also has a jog or offset alignment on SR 29 between CR 846 and New Market Road E.
- Central Alternative #2 is less disruptive to the existing street network and does not require any permanent street closures. Central Alternative #1 Revised requires street closures on New Market Road W near Flagler Street, Flagler Street near Madison Avenue W, and Madison Avenue W near Glades Street.
- Central Alternative #2 has far fewer business relocations and parcel impacts (one business relocation and four parcel impacts) than Central Alternative #1 Revised (nine business relocations and twenty parcel impacts).
- Central Alternative #2 has no residential relocations or parcel impacts, while Central Alternative #1 Revised has three residential relocations and two parcel impacts.
- At the second Alternatives Public Workshop held on November 8, 2017, more people expressed a preference for Central Alternative #2 than for Central Alternative #1 Revised.
- Central Alternative #2 avoids the access impacts to existing businesses along New Market Road that Central Alternative #1 Revised creates. Central Alternative #2 leaves New Market Road as a two-lane undivided roadway with uncontrolled access to adjacent businesses, while Central Alternative #1 Revised converts a portion of New Market Road to a four-lane divided roadway with a raised median and six median openings with controlled access to adjacent businesses.
- There are three fewer High or Medium-ranked potential petroleum or hazardous materials contaminated sites along Central Alternative #2 than along Central Alternative #1 Revised.

A full discussion of the alternatives evaluated is provided in Section 4.0 of the *Preliminary Engineering Report* (PER), prepared under separate cover for this project.

The No-Build Alternative and Central Alternative #2 were carried forward for further consideration at a Public Hearing on November 15, 2018.

2.6 PREFERRED ALTERNATIVE

Following the Public Hearing and comment period, in consideration of public input, engineering analysis, environmental studies, and interagency coordination, Central Alternative #2 was selected as the Preferred Alternative. Due to comments received at the Public Hearing and further coordination with Collier County, Central Alternative #2, the Preferred Alternative, was refined within the same corridor from north of Seminole Crossing Trail to north of Westclox Street to avoid impacts to Immokalee Airport Park.

Additional design refinements were made to the Preferred Alternative to meet the FDOT Design Manual (FDM) requirements and include the identification of stormwater management facilities (SMF), necessary to accommodate stormwater runoff from CR 846 north to SR 82. These additional design refinements were as follows:

CR 846 to SR 29 Bypass Junction: The proposed new signalized intersection at CR 846 and the proposed intersection at Gopher Ridge Road have been revised to incorporate roundabouts at these locations. The proposed right-of-way requirement previously varied from 108 feet to 200 feet and has been increased to vary from 144 feet to 250 feet. The two 11-foot travel lanes in each direction have been increased to 12-foot travel lanes in each direction from CR 846 to Gopher Ridge Road. The 6-foot sidewalk and 7-foot buffered bicycle lanes in each direction have been replaced with 12-foot shared use paths from CR 846 to Gopher Ridge Road. Twelve-foot shared use paths have been added to both sides of the corridor from Gopher Ridge Road to the SR 29 Bypass Junction. As a result of criteria updates, the proposed design speeds, ranging from 45-50 mph, have been updated and range from 45-55 mph. Three SMFs have been identified. The three proposed SMFs will require approximately 22 acres of offsite right-of-way. Stormwater runoff will be conveyed to the proposed SMFs by an open drainage system within the existing mainline right-of-way.

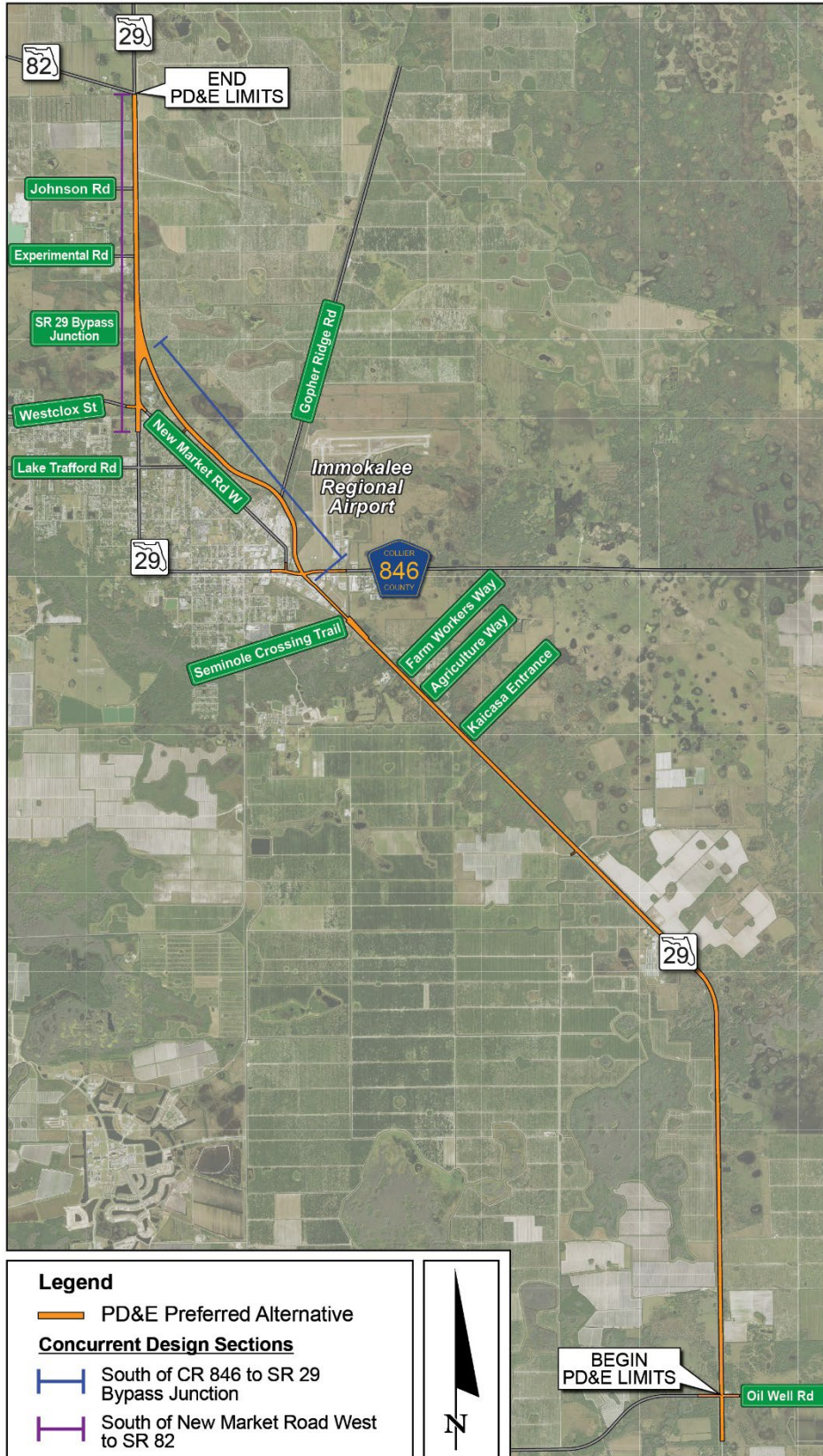
North of Westclox Street/New Market Road W to SR 82: The currently signalized intersection at New Market Road W and SR 29 has been revised to incorporate a roundabout at this location. A 10-foot shared use path has been added on the east side of the roadway from north of New Market Road W to SR 82, thus providing a 10-foot shared use path on both sides of the corridor. The mainline roadway improvements required for the proposed project will not require any additional right-of-way. As a result of criteria updates, the proposed design speeds, ranging from 50-60 mph, have been unified at 55 mph. Six SMFs have been identified. The six proposed SMFs will require approximately 20.3 acres of offsite right-of-way. Stormwater runoff will be conveyed to the proposed SMFs by an open drainage system within the existing mainline right-of-way.

The Preferred Alternative (i.e., the proposed action) provides a 4-lane divided typical section with travel lanes varying between 11 feet and 12 feet in width. The right-of-way width, the median type and width, and bicycle and pedestrian accommodations also vary for the different roadway segments within the project limits. Partial two-lane roundabouts were evaluated and incorporated at SR 29 and CR 846, SR 29 and Alachua Street/Gopher Ridge Road, and at SR 29 and Westclox Street/New Market Road W. Section 6.0 of the *PER*, prepared under separate cover, provides detailed information on the Preferred Alternative.

Figure 2-4 shows the location of the refined Preferred Alternative. **Table 2-2** provides the evaluation matrix for the refined Preferred Alternative. Potential impacts presented for the Preferred Alternative in Table 2-2 are discussed in Section 3.0. The approved typical sections developed for the Preferred Alternative pre-Public Hearing and post-Public Hearing are included in **Appendix B** and **Appendix P**, respectively. The concept plans developed for the Preferred Alternative pre-Public Hearing and post-Public Hearing are included in **Appendix C** and **Appendix K**, respectively.

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**FIGURE 2-4
PREFERRED ALTERNATIVE**



**TABLE 2-2
PREFERRED ALTERNATIVE EVALUATION MATRIX**

Evaluation Criteria	Preferred Alternative
Design Features	
Length (miles)	16.45 miles
Traffic Control Measures	Stop Control Traffic Signals & Roundabouts
Travel Lane Width (feet)	11 to 12 feet
Posted Speed - Subject to change pending speed study after construction	35 to 55 MPH
Right-of-Way Impacts	
Area of ROW to be Acquired for Roadway (acres)	81.6
Area of ROW to be Acquired for Stormwater Ponds/ Floodplain Compensation Sites (acres)	103.6
Business Impacts	
Number of Business Relocations	1
Number of Parcels Impacted	4
Residential Impacts	
Number of Residential Relocations	0
Number of Parcels Impacted	0
Environmental Impacts	
Number of Historical Sites Impacted (National Register Listed/Eligible)	0
Number of Archaeological Sites Impacted (National Register Listed/Eligible)	0
Number of Public Recreational Facilities/Parks Impacted	0
Area of Wetlands – Roadway (acres)	14.33
Area of Wetlands – SMFs from CR 846 to SR 82	0.15
Area of Surface Waters – Roadway (acres)	15.41
Area of Surface Waters – SMFs from CR 846 to SR 82	2.95
Area of Floodplain Encroachment (acres)	27.84
Potential Threatened and Endangered Species Impacts (none, low, medium, high)	Medium to High
Number of Potential Petroleum or Hazardous Materials Contaminated Sites	75 (34 Medium or High Risk)
Number of Receivers Potentially Impacted By Noise	8
Estimated Total Project Costs	
Engineering Design (15% of Construction Cost)	\$16,906,000
Wetland Mitigation ¹	\$1,787,000
Wildlife Habitat Mitigation ²	\$4,546,000
Utilities Relocation	\$0
Intelligent Transportation Systems (ITS)/ Advanced Traffic Management Systems (ATMS) Relocation	\$227,000
ROW Acquisition	\$19,700,000
Construction	\$112,708,000
Construction Engineering and Inspection (15% of Construction Cost)	\$16,906,000
Preliminary Estimate of Total Project Cost	\$172,780,000

SMF = Stormwater Management Facility

¹ Wetland mitigation cost estimate based on FDOT Environmental Mitigation Payment Processing Handbook, Page 5, Fiscal Year 2021/2022 (\$125,594 per acre of impact).

² Wildlife habitat mitigation cost includes mitigation for Florida panther and Florida scrub-jay. Florida panther mitigation cost estimate based on \$850 per panther habitat unit (PHU). Florida scrub-jay mitigation cost estimate based on \$25,000 per acre of impact with assumed 2:1 mitigation cost ratio. Caracara mitigation = \$150,000.

SECTION 3.0

ENVIRONMENTAL ANALYSIS

3.1 SOCIAL AND ECONOMIC

The project was screened through the Environmental Screening Tool (EST) as part of the Efficient Transportation Decision Making (ETDM) Programming Screen phase (ETDM Project #3752). Socio-economic data was generated as part of the screening event and is presented in the *Final Programming Screen Summary Report*, prepared under separate cover (re-published on August 10, 2018), and the Sociocultural Data Report (June 2018).

3.1.1 SOCIAL

Community Services

Community services typically serve the needs of the surrounding area and are viewed as focal points/destinations for adjacent neighborhoods and communities. Community services include religious centers, cemeteries, schools, parks, recreational facilities, and public buildings and facilities (i.e., community centers, health care facilities, and social service facilities). Parks and recreational facilities are discussed in Section 3.2.4, Recreational Areas. Community services located within a quarter-mile of the Central Alternative #2 (Preferred Alternative) are provided in **Table 3-1**.

TABLE 3-1
COMMUNITY SERVICES LOCATED WITHIN A
QUARTER-MILE OF PREFERRED ALTERNATIVE

Facility Type	Number/Quantity of Facility Type
Community/Cultural/Civic Centers	5
Fire Stations	1
Government Buildings	2
Healthcare Facilities	2
Law Enforcement Facilities	1
Religious Centers	4
Schools	3
Social Service Facilities	5

Most of the identified services are located west of the proposed Preferred Alternative in the Immokalee urban area. Prominent community focal points identified within a quarter-mile include Immokalee Health Park and the associated Florida State University College of Medicine, as well as the University of Florida/Institute of Food and Agricultural Sciences Southwest Florida Research and Education Center. Many of the social services provided in the area primarily serve low-income populations (i.e., food assistance and housing assistance). Right-of-way acquisition

associated with the Preferred Alternative will affect an access point to the Village Oaks Elementary School (a community service facility), including a pedestrian overpass to the school; however, the pedestrian overpass will be reconstructed as part of the project. No community services are anticipated to be displaced as a result of the proposed improvements. The proposed widening of SR 29 will improve emergency response times and access for the people living and working within the project limits. Access to community services will be maintained with minimal disruption during construction, and the project construction contractors will be required by the FDOT's *Standard Specifications for Road and Bridge Construction* to maintain access for emergency services to all adjacent properties throughout construction.

Community Cohesion

The proposed improvements were specifically designed to avoid residential areas; therefore, no splitting or isolation of neighborhoods is anticipated to occur.

Pedestrian, bicycle, and transit facilities will be included as part of the project. These improvements will enhance mobility along the corridor and between neighborhoods. In addition, FDOT Context Classifications will be applied to the design of the Preferred Alternative to ensure it fits the scale of the built environment and meets the local character of the area and desired aesthetics of the community. Therefore, community cohesiveness will benefit from the Preferred Alternative.

Nondiscrimination Considerations

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations*, signed by the President on February 11, 1994, directs federal agencies to take appropriate and necessary steps to identify and address disproportionately high and adverse effects of federal projects on the health or environment of minority and low-income populations to the greatest extent practicable and permitted by law.

United States Census Bureau 2010 Census Block Group data indicates that the project area (applying a quarter-mile buffer), in comparison to Collier County, contains higher percentages of minority, low-income, and Limited English Proficiency populations. Specifically, 64 Census Block Groups encompassing the Preferred Alternative, Central Alternative #2, contain a minority population greater than 40%. In addition, a significantly higher number of households within the Census Block Groups of Central Alternative #2 are below poverty level (32.20% compared to the county average of 9.48%). Further over 34% of the population within the quarter-mile of the Preferred Alternative “speaks English not well or not at all” compared to approximately 10% of the county as a whole. Despite the presence of minority and low-income populations within the project area, no disproportionate impacts are anticipated as the project was specifically designed to limit impacts to residential areas. A comprehensive *Public Involvement Plan* (PIP) (July 20, 2007, revised March 8, 2018), prepared under separate cover, was developed for this project. The PIP was originally approved on August 3, 2007, with an update approved on April 3, 2018, and the public involvement activities are summarized in Section 4.0 Comments and Coordination.

Public outreach to date includes special provisions to have both Creole and Spanish translators available for all public outreach activities.

This project has been developed without regard to race, color, national origin, age, sex, religion, disability, or family status in accordance with Title VI of the Civil Rights Act of 1964. No minority or low-income populations have been identified that would be adversely impacted by the proposed project, as determined above.

No comment has been received to date regarding conflicts with Title VI of the Civil Rights Act of 1964 or related statutes.

3.1.2 ECONOMIC

SR 29 serves as one of the primary north-south highways in Collier County providing access to county-designated target growth areas, including Immokalee and the surrounding Collier County Rural Land Stewardship Area. The Immokalee area is also a Community Redevelopment Area (tax increment financing is used to leverage redevelopment efforts) and a designated Rural Area of Opportunity, a legislative land use designation applied to encourage and facilitate the location and expansion of major economic development projects of significant scale in such rural communities. Other initiatives within the project area that are in place to incentivize economic development and revitalization, include: the Immokalee Enterprise Zone, the Empowerment Alliance of Southwest Florida Enterprise Community, and the South Immokalee Neighborhood Front Porch Community. In addition, the Immokalee Regional Airport is a Primary Freight Activity Center of Collier County as it supports industrial activities and agricultural packing and processing functions. A 60-acre portion of the airport is also a designated Foreign Trade Zone, a designation used to encourage activity and add value at facilities in competition with foreign companies.

The Immokalee Regional Airport and Seminole Casino Hotel Immokalee are the major economic hubs within the Immokalee area; the Seminole Casino Hotel Immokalee is the community's largest tourist attraction. Further, the Immokalee area is one of the leading producers of winter vegetables in the United States. Agricultural employment opportunities have created a diverse workforce including farm workers from Haiti, Guatemala, and Mexico.

SR 29 and New Market Road are the main corridors for regional and local truck traffic. SR 29 serves as a SIS highway corridor carrying high volumes of truck traffic and connecting to other SIS facilities; New Market Road provides direct access to and from agribusiness/commercial areas of Immokalee and the State Farmer's Market.

The proposed project is intended to provide an alternative route for regional truck traffic, which will:

- Enhance the livability of downtown Immokalee by reducing the conflicts between pedestrians/bicyclists and trucks and creating a more pedestrian friendly environment;

- Improve access for local traffic, which is critical to the viability of businesses in downtown Immokalee and along New Market Road;
- Improve the circulation of freight and access to area destinations and economic hubs for residents, employees, and visitors;
- Improve access and traffic circulation to local agricultural and ranching operations and commercial businesses, along with freight activity centers located along the corridor; and
- Enhance the economic viability of the area by providing the infrastructure needed to bring additional businesses and employers into the area.

The project is anticipated to support the vision of the community as a tourist destination. However, as detailed within the *Conceptual Stage Relocation Plan (CSRP)* (June 2018), prepared under separate cover, the Preferred Alternative is anticipated to result in one business relocation. Overall, the proposed improvements are anticipated to benefit local and regional economies.

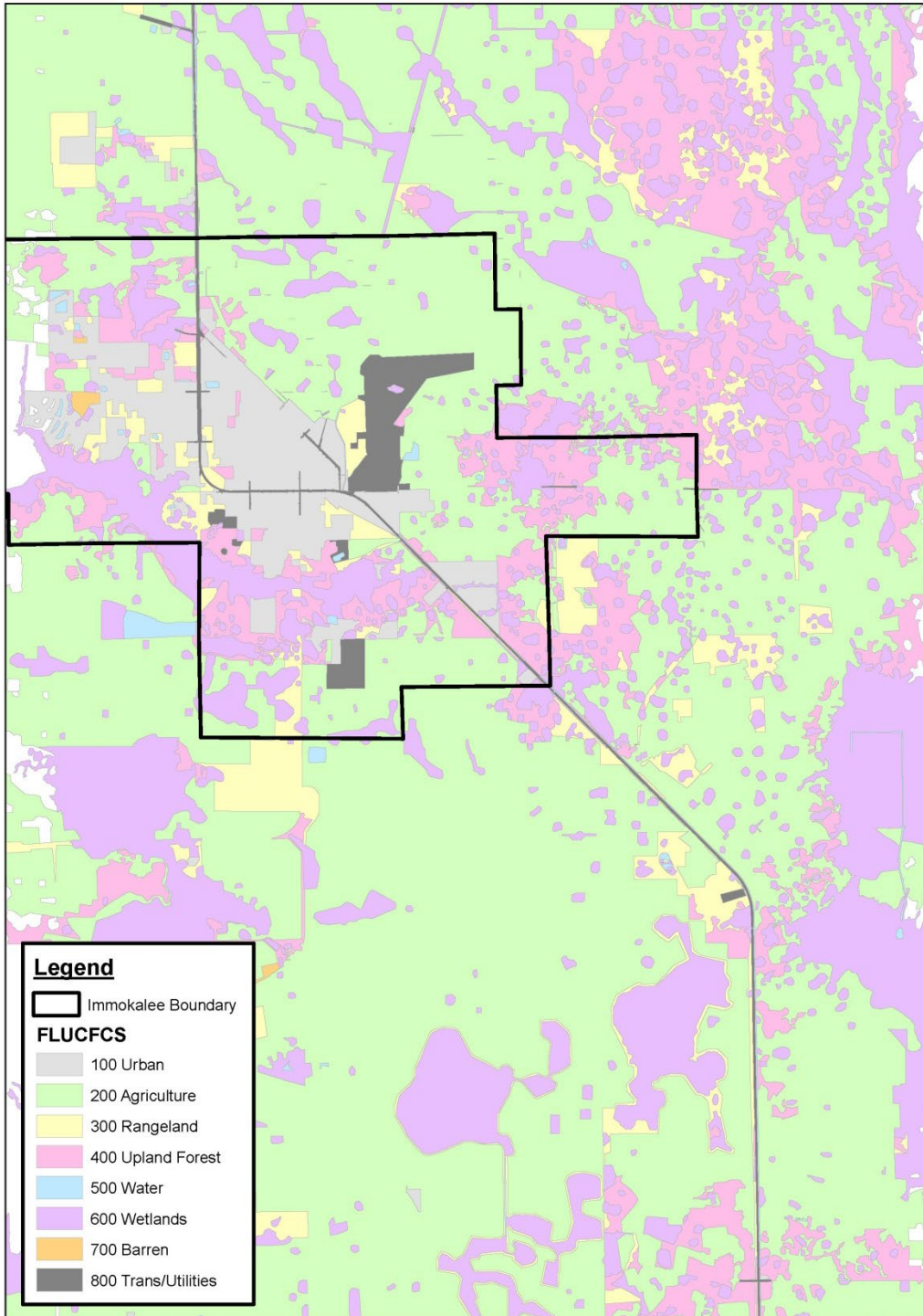
3.1.3 LAND USE CHANGES

Existing Land Use

Existing land use in the project area includes agricultural activities, which are predominant north and south of the urban boundary of Immokalee and east of the Preferred Alternative. Residential (a mix of low, medium, and high density dwelling units), industrial, and commercial activities with pockets of institutional uses are within the core of Immokalee and directly to the west of the proposed improvements. Commercial and industrial activities exist near the Immokalee Regional Airport. Five Planned Unit Developments exist within a quarter-mile (1,320-foot) buffer of the Preferred Alternative. The Town of Ave Maria Development of Regional Impact is located southwest of the project corridor. Further, the Seminole Tribe of Indians Immokalee Reservation is located to the west of the SR 29 project corridor within the Immokalee urban boundary. **Figure 3-1** shows the existing land uses for the area based on the Florida Land Use, Cover and Forms Classification System (FLUCFCS) code.

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**FIGURE 3-1
EXISTING LAND USES**



Other notable land use designations within the project area² include:

- Big Cypress Area of Critical State Concern – located to the east of the southern portion of the SR 29 project corridor,
- Collier County Rural Lands Stewardship Area Overlay – the entire project corridor is within this overlay with the exception of the project segment that traverses Immokalee,
- Front Porch Community – South Immokalee Neighborhood – located south of CR 846/Main Street east of Hancock Street and west of the project corridor, and
- State of Florida designated Enterprise Zone [Immokalee (Collier County) EZ-1101] and a United States Department of Housing and Urban Development (HUD) designated Empowerment Zone/Enterprise Community (Empowerment Alliance of Southwest Florida Enterprise Community).

Future Land Use

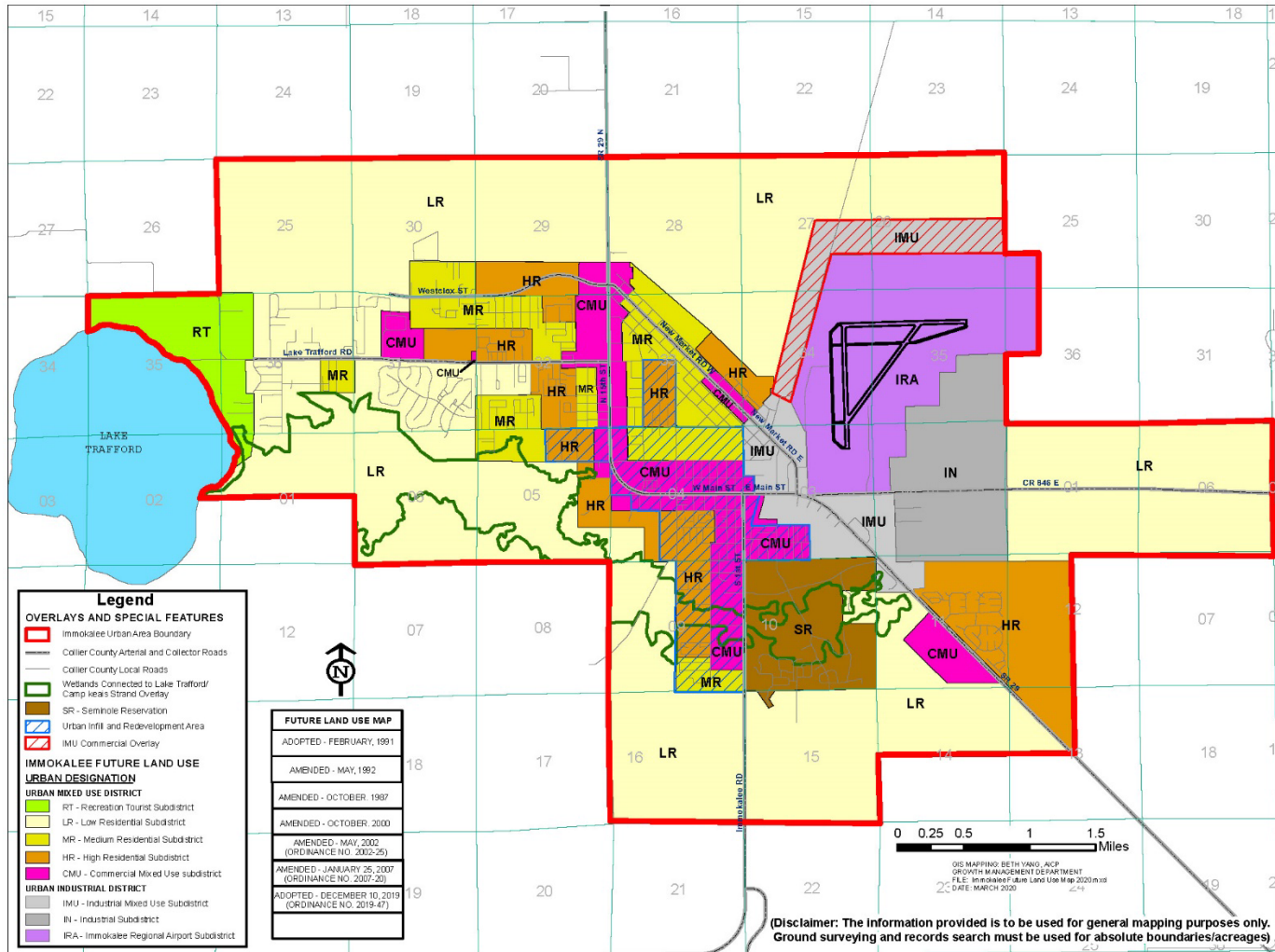
Based on the 2012-2025 Future Land Use Map of the Collier County Growth Management Plan (**Figure 3-2**), amended March 2021, the project occurs within the Collier County Rural Lands Stewardship Area Overlay with the exception of the segment that traverses Immokalee. It should be noted that the Immokalee Area Master Plan has undergone significant restudy in the past few years. The Collier County Community Redevelopment Agency led the effort to gain input from stakeholders, residents, and businesses, which ultimately established a vision for the future of Immokalee. The Future Land Use Map that resulted from this effort (**Figure 3-3**), adopted December 2019, indicates that the project area will continue to support residential, industrial, and commercial uses; agricultural uses on the outskirts of the Immokalee urban boundary will be maintained through the land use classification of low density residential subdistrict.

The existing and future land uses in the project area will continue to be supported as well as enhanced by the project, including improved access for nearby businesses, residents, and agricultural operations. The proposed widening of SR 29 is consistent with the Collier MPO's adopted 2045 LRTP CFP and aligns with the vision and goals of the Immokalee Area Master Plan. Therefore, no adverse changes to surrounding land uses are anticipated as a result of the project.

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² Since May 2020, Florida Power and Light Company constructed the FPL Immokalee Solar Energy Center at 3350 SR 29 N, Immokalee, FL 34142. The 74.5 megawatt facility is on 578 acres east of SR 29 and north and south of SR 82. Since May 2020, the Immokalee Foundation's Career Pathways Learning Lab is constructing a new 18-home subdivision north of New Market Road and west of Gopher Ridge Road at the corner of Calle Amistad and Dade Street.

**FIGURE 3-3
IMMOKALEE FUTURE LAND USE MAP**



3.1.4 MOBILITY

SR 29 is a major north-south corridor as it traverses the eastern portion of Collier County and the unincorporated community of Immokalee. SR 29 is critical in evacuating residents and supports hurricane response efforts in the eastern portion of Collier County as a designated hurricane evacuation route of the Florida Division of Emergency Management. SR 29 also serves as a SIS highway corridor carrying high volumes of truck traffic and connecting to other SIS facilities. This facility is additionally a designated Freight Mobility Corridor of Collier County, providing access to local agricultural and ranching operations, existing and proposed freight facilities in south-central Florida, as well as fast growing economic regions located in central Florida and the populated coastal areas. These roadway improvements will enhance access to the Immokalee Regional Airport for passengers, businesses, pilots, airport employees, and air cargo.

SR 29 serves daily pedestrian and bicycle activity as it traverses downtown Immokalee. There is a continuous sidewalk on the west side of the SR 29 project corridor from Farm Worker Way to New Market Road; along SR 29 from New Market Road to Westclox Street/New Market Road W and along the entirety of New Market Road, there are continuous sidewalks on both sides of the corridors. At SR 29 and Farm Worker Way, there is a grade-separated pedestrian bridge to accommodate students traveling to/from Village Oaks Elementary School. There are no pedestrian accommodations within the rural sections of the SR 29 project corridor, from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road W to SR 82. The existing sidewalk width varies from five to eight feet along the majority of SR 29 and New Market Road. In addition, marked bicycle lanes exist along the SR 29 project corridor from south of the Kaicasa Entrance to North 1st Street and from North 9th Street to north of Westclox Street/New Market Road W. Paved shoulders exist on both sides of SR 29 within the rural sections, south of the Kaicasa Entrance and north of Westclox Street/New Market Road W; however, pavement markings do not follow bicycle lane standards. There are no bicycle accommodations along the entirety of New Market Road or along SR 29 from North 1st Street to North 9th Street.

CAT Routes 19, 22, and 23 operate along SR 29 and/or New Market Road through some portions of the study area serving the community of Immokalee.

Complementing plans for the widening of other sections of the SR 29 corridor to the north and south, this project will provide a continuous four-lane connection from I-75 to US 27 in Glades County, enhance access to regional north-south and east-west transportation corridors, enhance the circulation and movement of goods, accommodate future growth, and improve emergency evacuation and response capabilities. Most importantly, it will divert regional truck traffic trips from downtown Immokalee creating a more pedestrian friendly environment. Bicycle, pedestrian, and transit features will be included as part of the project. The sidewalk and bicycle facilities in the project will be designed and constructed to comply with the Americans with Disabilities Act (ADA) of 1990, as amended. The sidewalks will meet ADA requirements for access, width, and grade.

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3.1.5 AESTHETIC EFFECTS

The topography along the SR 29 project corridor is relatively flat. There is no unusual vegetation present nor are there high vista points. However, given that agricultural land (consisting primarily of pasture lands, citrus groves, and cultivated row crops) comprises most of the corridor (particularly north and south of the urban boundary of Immokalee), scenic views exist. The community of Immokalee has placed a high value on the aesthetic character of its downtown/core area, and the Immokalee Beautification Advisory Committee completed a streetscape project along a section of the SR 29 corridor within the downtown area which included street lighting and street furniture. The streetscaping is part of an organized local effort to stimulate economic development and improve quality of life for residents in Immokalee. Neither alteration nor obstruction of scenic views of agricultural lands (pasture lands and groves) is anticipated as a result of the project. In addition, there are no Florida scenic highways or byways located within the SR 29 study area.

Aesthetics are an important consideration in any transportation project. Throughout the PD&E Study, FDOT coordinated with the Collier County Community Redevelopment Agency for Immokalee, including the Immokalee Beautification Advisory Committee, as well as consulted the Immokalee Area Master Plan, to stay consistent with the aesthetic vision and redevelopment initiatives of the Collier County Community Redevelopment Agency for Immokalee and Immokalee Beautification Advisory Committee. The FDOT Context Classification Handbook (August 2017) was also used to develop the typical sections for the proposed project.

3.1.6 RELOCATION POTENTIAL

In accordance with the FDOT PD&E Manual, a *Conceptual Stage Relocation Plan (CSRP)* (June 2018), prepared under separate cover, was completed to identify community characteristics, analyze the impact of the project on the community, and to identify residences and businesses that would be impacted by the project and any special relocation needs.

Initial right-of-way acreage estimates to accommodate the Preferred Alternative included an additional 77.82 acres of roadway right-of-way and approximately 104.00 acres of additional right-of-way for offsite stormwater retention ponds and/or floodplain compensation sites. One business and no residential relocations are expected to result from the proposed roadway improvement and potential stormwater retention pond/floodplain compensation site locations. No handicapped or disabled residential occupants are expected to be displaced as a result of the Preferred Alternative. Concept plans showing the location of the business relocation and expected residential and business impacts are included in **Appendix C** and were displayed at the Public Hearing for public review and comment.

Subsequent to the Public Hearing, design refinements were made to the Preferred Alternative to meet the FDM requirements and include the identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82 (see **Appendix K**). The Preferred Alternative, including the design refinements, will require 81.6 acres of additional roadway right-of-way and 103.6 acres of additional right-of-way for stormwater management compensation sites. The refinements did not result in any additional business relocations or residential relocations.

In order to minimize the unavoidable effects of right-of-way acquisition and displacement of people, a Right-of-Way and Relocation Assistance Program will be carried out in accordance with Florida Statute (F.S.) 421.55, Relocation of Displaced Persons, and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646 as amended by Public Law 100-17).

The FDOT provides advance notification of impending right-of-way acquisition. Before acquiring right-of-way, all properties are appraised on the basis of comparable sales and land use values in the area. Owners of property to be acquired will be offered and paid fair market value for their property rights.

No person lawfully occupying real property will be required to move without at least 90 days written notice of the intended vacation date, and no occupant of a residential property will be required to move until decent, safe, and sanitary replacement housing is made available. "Made available" means that the affected person has either by himself obtained and has the right of possession of replacement housing, or FDOT has offered the relocatee decent, safe, and sanitary housing which is within his financial means and available for immediate occupancy.

At least one relocation specialist is assigned to each highway project to carry out the Relocation Assistance and Payments Program. A relocation specialist will contact each person to be relocated to determine individual needs and desires and to provide information, answer questions, and give help in finding replacement property. Relocation services and payments are provided without regard to race, color, religion, sex, or national origin.

All tenants and owner-occupant relocatees will receive an explanation regarding all options available to them, such as (1) varying methods of claiming reimbursement for moving expenses; (2) rental replacement housing, either private or publicly subsidized; (3) purchase of replacement housing; and (4) moving owner-occupied housing to another location.

Financial assistance is available to the eligible relocatee to:

- Reimburse the relocatee for the actual reasonable costs of moving from home, business, and farm operation acquired for a highway project.
- Make up the difference, if any, between the amount paid for the acquired dwelling and the cost of a comparable decent, safe, and sanitary dwelling available on the private market, as determined by the FDOT.
- Provide reimbursement of expenses incidental to the purchase of a replacement dwelling.
- Make payment for eligible increased interest cost resulting from having to get another mortgage at a higher interest rate. Replacement housing payments, increased interest payments, and closing costs are limited to \$31,000 combined total.

A displaced tenant may be eligible to receive a payment, not to exceed \$7,200, to rent a replacement dwelling or room, or to use as down payment, including closing costs, on the purchase of a replacement dwelling.

The brochures that describe in detail the FDOT’s Relocation Assistance Program and Right-of-Way Acquisition Program are “Residential Relocation Under the Florida Relocation Assistance Program”, “Relocation Assistance Business, Farms, and Non-profit Organizations”; “Sign Relocation Under the Florida Relocation Assistance Program”; “Mobile Home Relocation Assistance”; and “Relocation Assistance Program Personal Property Moves”. All of these brochures are distributed at all public hearings and made available upon request to any interested person.

3.1.7 FARMLANDS

The United States Department of Agriculture (USDA) oversees the Farmland Protection Policy Act (FPPA). The FPPA’s ultimate goal is to minimize the extent to which federal programs contribute to the unnecessary and irreversible conversion of farmland to non-agricultural uses. For purposes of implementing FPPA, farmland is defined as prime or unique farmlands or farmland that is determined by the state or unit of local government agency to be farmland of statewide or local importance. FDOT submitted a *Farmland Conversion Impact Rating Form* (NRCS-CPA-106) (see **Appendix D**) requesting determination of involvement with prime, unique, statewide, or locally important farmland to the Natural Resources Conservation Service (NRCS). In an email dated July 24, 2018 (see **Appendix D**), they returned the form with their evaluation. In coordination with the NRCS, it was determined that the Preferred Alternative (Corridor B on the form) would impact approximately 160.5 acres of farmlands of prime or unique importance. The total points in Part VII of the NRCS-CPA-106 form (131.9 points) were below the significance threshold (160 points); therefore, no significant impacts to prime or unique farmlands will occur as a result of the project, no further consideration of protection is needed, no additional corridors need to be evaluated, and no additional coordination with NRCS is required at this time.

Since it has been determined that Important Farmlands as defined by 7 Code of Federal Regulations (C.F.R.) Part 658 are located in the project vicinity, if additional right-of-way is needed during the future project design phase(s), project involvement with Important Farmlands will be reevaluated and coordination will occur with the NRCS as appropriate. Therefore, the Preferred Alternative is not expected to result in significant farmland impacts.

3.2 CULTURAL

3.2.1 SECTION 4(F)

The project was examined for potential Section 4(f) resources in accordance with Section 4(f) of the Department of Transportation Act of 1966 (Title 49 United States Code (U.S.C.) Section 1653(f), amended and recodified in Title 49 U.S.C. Section 303 in 1983). Section 4(f) requires that prior to the use of any land for transportation purposes from a publicly owned park, recreation area, wildlife or waterfowl refuge, or a historic property on or eligible for inclusion in the National Register of Historic Places (National Register), it must be documented that there are no prudent or feasible alternatives which avoid such “use” and that the project includes all possible planning to minimize harm to the Section 4(f) resources.

Consistent with the FDOT PD&E Manual, a Section 4(f) *Determination of Applicability* (DOA) was prepared under separate cover for the following four potential Section 4(f) resources: Collier Rural Land Stewardship Sending Area #5; 1st Street Plaza; 9th Street Plaza; and Immokalee Airport Park. The Section 4(f) DOA was submitted to FHWA who determined in an email dated June 6, 2013 (see **Appendix E**) that Immokalee Airport Park, 1st Street Plaza, and 9th Street Plaza are Section 4(f) resources. Immokalee Airport Park is within the project limits. The other two Section 4(f) resources are no longer within the project limits. There will be no permanent acquisition of land from the three resources (Immokalee Airport Park, 1st Street Plaza, and 9th Street Plaza), no temporary occupancies of land that are adverse in terms of the statute’s preservation purpose, and no proximity impacts which significantly impair the protected functions of the properties from the Preferred Alternative. A subsequent Section 4(f) DOA (Form 650-050-45), prepared under separate cover, for the Airport Viewing Area was completed and it was determined on June 26, 2018 that Section 4(f) does not apply to this resource (see **Appendix E**). Additional information is available in the Section 4(f) DOAs.

Based upon comments received at the Public Hearing and further coordination with Collier County after the Public Hearing, Central Alternative #2, the Preferred Alternative, was modified to completely avoid impacts to Immokalee Airport Park. As such, a Section 4(f) *No Use Determination* (Form No. 650-050-49) was prepared under separate cover, and it was determined on May 20, 2019 that the Preferred Alternative will result in “No Use” of the Immokalee Airport Park (see **Appendix L**). In addition, a Section 4(f) DOA (Form 650-050-45), prepared under separate cover, was completed for the Immokalee Airport Conservation Easement with the refinement of the Preferred Alternative after the Public Hearing. It was determined on May 20, 2019 in consultation with FAA, the Cooperating Agency and Official with Jurisdiction (OWJ) over the Immokalee Airport Conservation Easement, that Section 4(f) does not apply to this resource (see **Appendix L**).

Subsequent to the Public Hearing, design refinements were recently made to the Preferred Alternative to meet the FDM requirements and include the identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82. As a result of the design refinements and associated SMFs, additional coordination with Collier County was initiated and a letter was submitted on February 14, 2024 to confirm the Immokalee Airport Park boundary. Concurrence on the park boundary was received on March 5, 2024 (see **Appendix Q**). With this confirmation, it was determined that the Preferred Alternative design refinements and associated proposed SMFs would still result in “No Use” of the Immokalee Airport Park. Improved direct replacement access will be provided from SR 29 to Immokalee Airport Park through a new driveway connection along with a shared-use path.

Therefore, the project will not result in any impacts to Section 4(f) properties.

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3.2.2 HISTORIC SITES/DISTRICTS

A *Cultural Resource Assessment Survey* (CRAS) was conducted in accordance with requirements set forth in the National Historic Preservation Act of 1966, as amended, and Chapter 267, F.S. The investigations were carried out in conformity with the FDOT PD&E Manual and the standards contained in the Florida Division of Historical Resources' (FDHR) Cultural Resource Management Standards and Operations Manual (FDHR 2003; FDOT 1999). In addition, the survey met the specifications set forth in Chapter 1A-46, Florida Administrative Code (F.A.C.).

The CRAS included background research and a field survey, including review of the Florida Master Site File (FMSF) and National Register. The assessment resulted in the identification of a total of 46 historic resources (50 years of age or older) within the historic Area of Potential Effect (APE) (two previously recorded resources and 44 newly recorded historic resources). The previously recorded resources include the Immokalee Ice Plant (8CR642) and the Immokalee Regional Airport (8CR1087). The 44 newly recorded include 35 buildings (8CR1180-8CR1196, 8CR1236-8CR1238, 8CR1245-8CR1246, 8CR1323-8CR1329, 8CR1331-8CR1334, and 8CR1369-8CR1370); two bridges (8CR1496 and 8CR1497); four canals (8CR1256, 8CR1368, 8CR1498, and 8CR1499); one road (8CR1309); and two resource groups (8CR1252 and CR1500). Updated or new FMSF forms were prepared for all of the historic resources. Forty-five of the resources are considered ineligible for listing in the National Register.

One of the previously identified resources, the Immokalee Ice Plant (8CR00642), is considered National Register-eligible. The Immokalee Ice Plant (8CR642) was constructed in 1945 and, although there have been several additions, it maintains much of its integrity. This resource is representative of Immokalee's conversion from a community of individual isolated farmsteads to a more modern agricultural community and is considered eligible for the National Register under Criterion A for its role in Immokalee's Community Planning and Development, Agriculture, and Industry and the original evaluation is still applicable. None of the proposed improvements directly or indirectly impact the Ice Plant or diminish its integrity. Coordination occurred with the State Historic Preservation Officer (SHPO)/Florida Division of Historical Resources (FDHR) Transportation Compliance Review Program staff to discuss the potential effects of the proposed improvements on the potentially eligible Immokalee Ice Plant. The level of documentation needed to determine the effects to the Ice Plant were also discussed and it was noted that it appeared that there would be no adverse effect to the Ice Plant and it was agreed that the effects analysis could be included in this CRAS transmittal letter. Therefore, based on the application of the criteria of adverse effect, it was determined that the proposed project will not adversely affect those characteristics of the Immokalee Ice Plant that qualify this resource for listing in the National Register. SHPO concurred with this determination on August 9, 2018 (see more detailed information below).

Coordination and field reviews have occurred with the Seminole Tribe of Florida (STOF) Tribal Historic Preservation Officer (THPO) at the Immokalee Reservation to discuss the project and to review aerial photographs of the project area and surroundings. The objective was to gather information regarding the potential locations of Seminole camps and to identify areas of potential

concern to the STOF. Three areas of concern were identified, all of which were located along those portions of SR 29 to the west of the proposed improvements and outside of the project APE. No locations of known Seminole camps were noted within or in proximity to the proposed improvements. On February 19, 2020, after the Public Hearing, the Seminole Tribe of Florida Tribal Historic Preservation Officer indicated that they have no objections to the project at this time and requested that they be notified of any archaeological, historical, or burial resources that are inadvertently discovered as the project advances (see **Appendix M**). The Miccosukee Tribe of Florida indicated during the ETDM Programming Screen that no further coordination was necessary if no archaeological sites were to be impacted (see **Appendix F**). Since no archaeological sites were discovered, further coordination with the Miccosukee Tribe was not initiated. Tribal representatives were notified of all public meetings.

The *CRAS* (July 2018), prepared under separate cover, and the *CRAS* transmittal letter with the Immokalee Ice Plant effects analysis were submitted to the SHPO. On August 9, 2018, the SHPO concurred with the recommendations and finding that the project would have *No Adverse Effect* to historic properties (see **Appendix F**).

Subsequent to the Public Hearing, a *CRAS Addendum Report* (February 2024) was prepared, under separate cover, to supplement and update cultural resource findings of the *CRAS* (Janus Research 2018) following design refinements made to the Preferred Alternative to meet the FDM requirements and identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82. The historical/architectural APE of the *CRAS Addendum Report* remained in-keeping with the 2018 *CRAS* and included the archaeological APE and adjacent parcels up to 200 feet from edge of existing right-of-way and 100 feet from preferred pond sites. The archaeological and historical/architectural field surveys were conducted in November and December 2023.

The historical/architectural field survey and historical background research resulted in the identification of 19 historic resources within the APE. Of these, two new resources were identified, recorded, and evaluated: a ca. 1971 Mid-Century Modern style building (8CR01645) and a ca. 1970 Masonry Vernacular style building (8CR01646). These buildings lack sufficient architectural features and are not significant embodiments of a type, period, or method of construction. In addition, a 0.25-mile segment of SR 29 (8CR01309) was updated within the APE to account for modifications in extending the northern portion of the original study corridor to SR 82. This linear resource is a common example of a four-lane divided roadway found throughout Collier County and is the same design as the segments that were previously recorded and evaluated as National Register-ineligible by the SHPO within the APE to the south. Background research pertaining to these resources did not reveal any historic associations with significant persons and/or events. Therefore, the resources are not National Register-eligible, either individually or as a part of a historic district. The remaining 16 historic resources that were previously recorded within the APE (8CR01087, 8CR01184, 8CR01185, 8CR01187, 8CR01188, 8CR01189, 8CR01236, 8CR01237, 8CR01238, 8CR01333, 8CR01334, 8CR01368, 8CR01370, 8CR01496, 8CR01498, 8CR01500) were not re-evaluated since the SHPO had already determined that they were National Register-ineligible, and no significant changes were observed during the field survey. Of the 12 extant

historic resources identified, two previously recorded historic resources, the boundary of Immokalee Regional Airport (8CR01087) and Eutopia Canal (8CR01498), are located within proposed pond sites, Ponds 501B and 501C. In addition, Madison Avenue Canal (8CR01368) is located adjacent to Ponds 502A and 502B, and the Eutopia Canal (8CR01498) is located adjacent to Pond 501C. SR 29 (8CR01309) is located adjacent to six proposed pond sites (601A, 602B, 603/604B, 605A, 606A, and 607A).

Based on the results of the background research and field investigations, no historic resources that are listed, eligible, or that appear potentially eligible for listing in the National Register are located within the APE. Therefore, the proposed undertaking will result in *no historic properties affected*. The *CRAS Addendum Report* and *CRAS Addendum Report* transmittal letter, documenting the noted findings, were submitted to the SHPO on February 23, 2024. The SHPO concurred with FDOT's recommendations and findings that the project will result in *no historic properties affected* on March 21, 2024 (**Appendix R**).

3.2.3 ARCHAEOLOGICAL SITES

A *CRAS* was completed as referenced in Section 3.2.2 above. No previously recorded or newly recorded archaeological sites were identified within the archaeological APE as part of the project *CRAS*. In total, 122 round shovel tests were excavated during the investigation and all shovel tests were negative for the presence of cultural materials. In addition, the majority of the archaeological APE consists of citrus groves, open pasture, pine flatwoods with saw palmetto, and empty lots; no environmental features were identified indicative of archaeological site potential. As a result of this survey, no archaeological sites were discovered.

As referenced above in Section 3.2.2, a *CRAS Addendum Report* (February 2024) was prepared under separate cover after the Public Hearing. No historic or pre-Contact period cultural materials were recovered from excavations performed through the 100 additional shovel tests (supplementing the ones conducted as part the of 2018 *CRAS*) or were noted on the surface during the archaeological field surveys performed in November and December 2023. The archaeological APE was determined to have a low probability for the occurrence of archaeological sites. Based on results of the background research and field investigations, no archaeological sites that are listed, eligible, or that appear potentially eligible for listing in the National Register are located within the APE.

The proposed project is expected to have no significant impact on archaeological sites.

Although unlikely, should construction activities uncover any archaeological materials, activity in the immediate area of the remains should stop while a professional archaeologist evaluates the material. In the event that human remains are found during construction or maintenance activities, Chapter 872.05, F.S. applies and FDOT's *Standard Specifications for Road and Bridge Construction* require that all construction activities cease. The Seminole Tribe of Florida THPO and the Miccosukee Tribe of Florida will be notified. Activity may not resume until authorized by the District Medical Examiner or the State Archaeologist.

3.2.4 RECREATION AREAS

During project development, three recreational resources were identified within the SR 29 study area: 1st Street Plaza, 9th Street Plaza, and Immokalee Airport Park. Additional information on these resources is available in the Section 4(f) *DOAs*, prepared under separate cover. The planned improvement to SR 29 will avoid impacts to the 1st Street Plaza, 9th Street Plaza, and Immokalee Airport Park.

3.3 NATURAL

3.3.1 WETLANDS AND OTHER SURFACE WATERS

In accordance with Executive Order 11990, *Protection of Wetlands*, dated May 23, 1977; US Department of Transportation Order 56601.A, *Preservation of the Nation's Wetlands*, dated August 24, 1978; and the FDOT PD&E Manual, a *Natural Resources Evaluation (NRE)* (July 2018) was prepared under separate cover as part of the PD&E Study. Detailed information about the biotic communities as well as the analysis conducted is contained in Sections 3.0 and 5.2 of the *NRE*. The purpose of this evaluation was to assure the protection, preservation, and enhancement of wetlands to the fullest extent possible.

The Preferred Alternative follows the existing SR 29 corridor to the greatest extent feasible while maintaining a bypass option. The bypass is intended to divert freight truck traffic from downtown Immokalee, improving congestion/traffic operations in the area and enhancing safety for residents. Design of the bypass segment minimizes wetland impacts by relocating the bypass section closer to the Immokalee urban boundary within previously disturbed, primarily upland habitats. As such, the bypass design also reduces potential secondary wetland impacts (such as habitat fragmentation and degradation).

Multiple field reviews were conducted between April 2010 and October 2020. During the field inspections, preliminary habitat boundaries and classification codes established through in-office literature reviews and aerial photograph interpretation were verified. Approximate wetland and Other Surface Water (OSW) boundaries were field verified in accordance with the State of Florida Wetlands Delineation Manual (Chapter 62-340, F.A.C.) and the guidelines found within the Regional Supplement to the United States Army Corps of Engineers (USACE) Wetlands Delineation Manual: Atlantic and Gulf Coastal Plain Region (USACE 2010). The individual wetland and OSW habitats located within the Preferred Alternative mainline, identified by FLUCFCS code and United States Fish and Wildlife Service (FWS) classification as well as by acreage, are summarized in **Table 3-2**.

The Preferred Alternative mainline will result in approximately 14.33 acres of wetland impacts and approximately 15.41 acres of OSW impacts for a total of approximately 29.74 acres of wetland and OSW impacts.

A Uniform Mitigation Assessment Method (UMAM) analysis was performed to estimate the loss of wetland function as a result of the proposed improvement impacts. The UMAM analysis did

not include OSWs since they consist primarily of upland-cut linear ditches that are proposed to be replaced in kind. Based on the calculations, the Preferred Alternative mainline will result in approximately 9.21 units of functional loss. The existing wetlands and OSWs within the project study area are low quality habitat due to their proximity to the existing SR 29 corridor.

**TABLE 3-2
MAINLINE – INDIVIDUAL WETLANDS AND OTHER SURFACE WATERS**

Wetland/ OSW ID	FLUCFCS Description	FLUCFCS Code	FWS Wetland Classification*	Acres in Preferred Alternative
Wetlands				
WL-1	Mixed Wetland Hardwoods	617	PFO1/3C	0.83
WL-2	Wetland Forested Mixed	630	PFO1/2C	1.68
WL-3	Cypress	621	PFO2C	0.56
WL-4	Wetland Forested Mixed	630	PFO1/2C	2.55
WL-5	Freshwater Marshes	641	PEM1C	0.62
	Mixed Wetland Hardwoods	617	PFO1/3C	0.16
WL-6	Wetland Forested Mixed	630	PFO1/2C	3.89
WL-7	Freshwater Marshes	641	PEM1C	0.76
WL-8	Mixed Wetland Hardwoods	617	PFO1/3C	0.96
WL-9	Freshwater Marshes	641	PEM1C	0.77
WL-10	Freshwater Marshes	641	PEM1C	0.44
WL-11	Freshwater Marshes	641	PEM1C	0.81
WL-12	Freshwater Marshes	641	PEM1C	0.30
<i>Total Wetlands</i>				<i>14.33</i>
Other Surface Waters				
Linear Ditches	Streams and Waterways	510	PUB2F	14.78
Reservoirs	Reservoirs <10 acres	534	PSS1C / PUB2C	0.63
<i>Total Other Surface Waters</i>				<i>15.41</i>
Total				29.74

* **FWS Wetland Descriptions:**

- PEM1C: Palustrine, Emergent, Persistent, Seasonally Flooded
- PFO1/2 C: Palustrine, Forested, Broad-Leaved Deciduous/Needle-Leaved Deciduous, Seasonally Flooded
- PFO1/3 C: Palustrine, Forested, Broad-Leaved Deciduous/Broad-Leaved Evergreen, Seasonally Flooded
- PSS1C: Palustrine, Scrub-Shrub, Broad-Leaved Deciduous, Seasonally Flooded
- PUB2F: Palustrine, Unconsolidated Bottom, Sand, Semi-permanently Flooded

Subsequent to the Public Hearing, design refinements were made to the Preferred Alternative to meet the FDM requirements and included the identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82. As summarized in **Table 3-3** and documented in the *Preliminary Pond Siting Report Addendums* (March 2024), the three proposed SMFs for the Preferred Alternative segment extending from CR 846 to SR 29 Bypass Junction will result in no wetland impacts and an estimated total of 0.24 acres of OSW impacts. The six proposed SMFs for the Preferred Alternative segment extending from North of Westclox Street/New Market Road W to SR 82 will result in an estimated total of 0.15 acres of wetland impacts and 2.71 acres of OSW impacts. Based on the calculations, these nine proposed SMFs will result in 1.26 units of functional loss.

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**TABLE 3-3
PROPOSED STORMWATER MANAGEMENT FACILITIES –
WETLANDS AND OTHER SURFACE WATERS**

Proposed SMF ID	FLUCFCS Code & Description	FWS Wetland Classification*	Wetland Acres	OSW Acres
501B	510 – Streams and Waterways	R2UB4Fx	0.00	0.13
502A	510 – Streams and Waterways	R2UB4Fx	0.00	0.11
503B	N/A	N/A	0.00	0.00
601A	641 – Freshwater Marshes	PEM	0.15	0.00
602B	510 – Streams and Waterways	PEM1Cx	0.00	0.10
603/604B	510 – Streams and Waterways	PEM1Cx	0.00	0.99
605A	510 – Streams and Waterways	PEM1Cx	0.00	1.16
606A	N/A	N/A	0.00	0.00
607A	510 – Streams and Waterways	PEM1Cx	0.00	0.46
Total			0.15	2.95

N/A = Not Applicable

* **FWS Wetland Descriptions:**

R2UB4Fx: Riverine, Lower Perennial, Unconsolidated Bottom, Organic, Semi-permanently Flooded, Excavated

PEM1Cx: Palustrine, Emergent, Persistent, Seasonally Flooded

The Preferred Alternative, including the design refinements north of CR 846, will result in an estimated total of 32.84 acres of wetland and OSW impacts (29.74 acres of wetland and 2.95 acres of OSW) equating to 10.47 units of functional loss. More specific wetland and OSW impacts related to pond sites south of CR 846 will be determined during the Design phase.

Avoidance and minimization of project impacts were demonstrated by using the existing, previously disturbed SR 29 corridor for the majority of the project. The use of mitigation bank credits to offset adverse impacts resulting from the project is the preferred mitigation option. The project study area is located entirely within the service areas of several approved mitigation banks that currently have wetland credit availability: Corkscrew Regional Mitigation Bank, Big Cypress Mitigation Bank, Panther Island Mitigation Bank, and Panther Island Expansion Mitigation Bank.

Compensatory mitigation for unavoidable wetland impacts associated with this project will be completed through the use of mitigation banks and any other mitigation options that satisfy state and federal requirements, Part IV of Chapter 373, F.S., and Title 33, U.S.C., Section 1344.

The proposed project was evaluated for potential wetland impacts in accordance with Executive Order 11990, *Protection of Wetlands*. Based upon the above considerations, it is determined that there is no practicable alternative to the proposed construction in wetlands and that the proposed action includes all practicable measures to avoid and minimize harm to wetlands which may result from such use.

3.3.2 AQUATIC PRESERVES AND OUTSTANDING FLORIDA WATERS

The project is not located within a designated aquatic preserve and/or Outstanding Florida Waters (OFWs); therefore, no further documentation regarding these resources is required as per the FDOT PD&E Manual.

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3.3.3 WATER QUALITY AND WATER QUANTITY (STORMWATER)

The SR 29 project corridor is located within the jurisdiction of the South Florida Water Management District (SFWMD). The project corridor traverses three major watersheds, which contain four regional drainage basins:

- Okaloacoochee Watershed: Silver Strand Basin (Water Body ID (WBID) 3278W)
- Okaloacoochee Watershed: Immokalee Basin (WBID 3278L)
- Cocohatchee-Corkscrew Watershed: Cow Slough Basin (WBID 3278E)
- Caloosahatchee River Watershed: Townsend Canal Basin (WBID 3235L)

All four drainage basins are Class III waters. As of 2023, three of the four are listed as impaired through the FDEP 303(d) Impaired Waters List with the exception being Okaloacoochee Watershed: Immokalee Basin (WBID 3278L). Drainage along the existing roadway is accomplished through collection and conveyance by open roadside ditches, side drains, ditch bottom inlets, and cross drains. Typically, roadside ditches are present for the length of the project. These ditches and depressional areas provide some degree of attenuation and water quality treatment. The runoff in the ditches is co-mingled with offsite runoff and ultimately conveyed to the outfall. From 13th Street to 9th Street, runoff is collected by curb and gutter and conveyed to the outfall by a storm drain system. Water quality treatment for the east side of SR 29 is provided in shallow retention areas between the road and the Barron Canal. Runoff from the west side of SR 29 sheet flows directly to existing grade with no permitted treatment.

The stormwater runoff from the proposed improvements between north of Seminole Crossing Trail and CR 846 will be collected and conveyed to SMFs by curb, gutter, and pipes. Stormwater runoff for the remainder of the Preferred Alternative will be conveyed to the SMFs by an open drainage system, potentially providing treatment where none currently exists. The water quality treatment and runoff attenuation will be achieved through the construction of offsite wet ponds, which will require the acquisition of additional right-of-way.

The preliminary SMF sites are conceptually depicted on figures found in **Appendix G** for the purpose of determining the location, type, and design of facilities that have the capacity to provide stormwater management for the project. These sites are subject to change. Final pond configuration and pond aesthetics (e.g., fencing, landscaping, side slopes, etc.) will be determined during final design. Additional information on preliminary pond sites is contained in the *Preliminary Pond Siting Report* (PSR) (August 2018), prepared under separate cover.

Subsequent to the Public Hearing, design refinements were made to the Preferred Alternative to meet the FDM requirements and the identification of proposed SMFs, necessary to accommodate stormwater runoff, from CR 846 to SR 82, nine proposed SMF sites were identified for the portion of the Preferred Alternative from CR 846 to SR 82 (see **Appendix K**). *Preliminary PSR Addendums* (March 2024), prepared under separate cover, contain additional information on SMFs for the northern portion of the Preferred Alternative. The proposed SMFs will be designed to meet,

at a minimum, the water quantity and water quality requirements of the SFWMD. Best management practices will be incorporated during construction.

In accordance with the FDOT PD&E Manual, a *Water Quality Impact Evaluation* (WQIE) (June 2018) was prepared under separate cover for the project. As a result of the design refinements made to the Preferred Alternative to meet the FDM requirements and identification of proposed SMFs necessary to accommodate stormwater runoff from CR 846 to SR 82, updated *WQIEs* were prepared under separate cover. Water quality and quantity issues will be addressed through compliance with the design requirements of authorized regulatory agencies during the permitting process.

3.3.4 WILD AND SCENIC RIVERS

According to the National Park Service (NPS) Nationwide Rivers Inventory, there are no wild and scenic rivers within the project limits; therefore, the coordination requirement for the Wild and Scenic Rivers Act does not apply to this project.

3.3.5 FLOODPLAINS

In accordance with the FDOT PD&E Manual, a *Location Hydraulic Report* (LHR) (August 2018) was prepared under separate cover for the project.

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Maps (FIRMS) for Collier County (Map Numbers 12021C0290H, 12021C0280H, 12021C0165H, 12021C0145H, and 12021C0135H), the 100-year base floodplain is within the project corridor. The entire project is within Zone AH, which is the flood insurance rate zone that corresponds to areas of one-percent-annual-chance shallow flooding (usually areas of ponding) where average depths are between one and three feet. Whole-foot base flood elevations derived from detailed hydraulic analyses range from an elevation of 19 feet (just south of Oil Well Road) to an elevation of 36.5 feet (at SR 82).

As a result of design refinements made to the Preferred Alternative to meet the FDM requirements and the identification of proposed SMFs necessary to accommodate stormwater runoff from CR 846 to SR 82, *LHR Addendums* (March 2024) were prepared under separate cover for the project. Total floodplain encroachment for the proposed improvements is 27.84 acre-feet and is rated as “Minimal” [as per levels of significance of encroachment as outlined in the FDOT PD&E Manual and detailed in the *LHR* (August 2018) and *LHR Addendums* (March 2024)] and can best be described as Project Activity Category 4 – “Projects on Existing Alignment Involving Replacement of Existing Drainage Structures with No Record of Drainage Problems”. There are no FEMA regulatory floodways located within the project limits. Additional information regarding floodplains can be found in the *LHR* (August 2018) and *LHR Addendums* (March 2024).

The proposed drainage systems will perform hydraulically in a manner equal to or greater than the existing conveyance systems, and surface water elevations are not expected to increase upstream or downstream of the project limits. Minimal impact on the existing floodplains within and

adjacent to the roadway improvement project is anticipated. As a result, there will be no significant adverse impacts on natural and beneficial floodplain values. There will be no significant change in flood risk, and there will not be a significant change in the potential for interruption or termination of emergency service or emergency evacuation routes. In addition, potential floodplain compensation areas were identified for the Preferred Alternative to offset the impacts identified on a 1:1 basis. Therefore, it has been determined that this encroachment is not significant.

3.3.6 COASTAL ZONE CONSISTENCY

In a letter dated October 5, 2007 (**Appendix H**), the FDEP, through the Florida State Clearinghouse, determined that this project is consistent with the Florida Coastal Management Program (FCMP). The state's final concurrence of the project's consistency with the FCMP will be determined during the environmental permitting stage.

3.3.7 COASTAL BARRIER RESOURCES

Based on review of coastal barrier resources system data and associated maps, it has been determined that the project is neither in the vicinity of nor leads directly to a designated coastal barrier resource unit pursuant to the *Coastal Barrier Resources Act of 1982* (CBRA) and the *Coastal Barrier Improvement Act of 1990* (CBIA).

3.3.8 PROTECTED SPECIES AND HABITAT

This project was evaluated for potential impacts to threatened and endangered animal and plant species in accordance with 50 C.F.R. Part 402.12, Section 7(c) of the Endangered Species Act of 1973 (ESA), as amended; the Wildlife Code of the State of Florida (Chapter 68, F.A.C.); and the FDOT PD&E Manual. The evaluation included literature review, database searches, and field assessments of the project area to identify the potential occurrence of protected species and/or presence of federally-designated critical habitat. Field assessments of the study area were conducted by project biologists in April and October 2010, April 2011, January 2012, August 2017, March 2018, and October 2020. The purpose of the evaluation was to document current environmental conditions along the corridor and potential impacts to wildlife, habitat, or listed species; evaluate the project area's current potential to support species listed as endangered, threatened, or of special concern; identify current permitting and regulatory agency coordination requirements for the project; and request comments from regulatory agencies with jurisdiction over the study. Based on this evaluation, it was determined that no federally-designated critical habitat is present within the project area.

An *NRE* (July 2018) was prepared under separate cover as part of consultation required under Section 7 of the ESA, as amended, and per the requirements of the FDOT PD&E Manual. A total of 30 federal or state listed protected species were identified as having the potential to occur within the project study area. The evaluation included coordination with the FWS and the Florida Fish and Wildlife Conservation Commission (FWC), and the Florida Natural Areas Inventory (FNAI). **Table 3-4** below summarizes the effect determination for each of these species as a result of the proposed project based on the FDOT findings and commitments to offset potential impacts. Based

upon correspondence with the FWS received on March 20, 2018 (**Appendix I**), the FDOT committed to re-initiating Section 7 consultation with the FWS during the project's design and permitting phase for the Florida scrub-jay and Florida panther. Potential impacts to listed species and their habitats are described in more detail in the *NRE* and subsequent addendums.

The *NRE* was submitted to the FWS and FWC on July 20, 2018. The FWS responded via email on August 3, 2018 indicating that they would respond to all species determinations at the time of re-initiation of Section 7 consultation during the final design and permitting phase and they had no other comments on the project. On August 2, 2018, the FDOT received a comment from the FWC that noted a concern with the *NRE* in that the document did not specifically identify or discuss potential impacts of the project to the Immokalee Regional Airport Upland Management Area (UMA) (which contains the Immokalee Airport Conservation Easement) and, consequently, impacts to habitat of the Florida scrub-jay and gopher tortoise. An *NRE Addendum* was prepared under separate cover and submitted to agencies for review on August 9, 2018. Findings and species effect determinations documented in the *NRE Addendum* remained consistent with the *NRE*. The FWC responded providing their agreement with the findings and determinations in a letter dated August 21, 2018. Correspondence received from both the FWS and FWC on the *NRE* and *NRE Addendum* is included in **Appendix J**.

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**TABLE 3-4
SUMMARY OF LISTED SPECIES AND EFFECT DETERMINATIONS**

Scientific Name	Common Name	Effect Determination	Status	
			Federal	State
Federally-Listed & Candidate Wildlife Species				
<i>Alligator mississippiensis</i>	American alligator	May Affect, Not Likely to Adversely Affect	T (S/A)	FT (S/A)
<i>Ammodramus savannarum floridanus</i>	Florida grasshopper sparrow	No Effect	E	F,E
<i>Aphelocoma coerulescens</i>	Florida scrub-jay	May Affect, Likely to Adversely Affect	T	F,T
<i>Drymarchon corais couperi</i>	Eastern indigo snake	May Affect, Not Likely to Adversely Affect	T	F,T
<i>Eumops floridanus</i>	Florida bonneted bat	May Affect, Not Likely to Adversely Affect	E	F,E
<i>Mycteria americana</i>	Wood stork	May Affect, Not Likely to Adversely Affect	T	F,T
<i>Picoides borealis</i>	Red-cockaded woodpecker	No Effect	E	F,E
<i>Polyborus plancus audubonii</i>	Audubon's crested caracara	May Affect, Not Likely to Adversely Affect	T	F,T
<i>Puma concolor coryi</i>	Florida panther	May Affect, Likely to Adversely Affect	E	F,E
<i>Rostrhamus sociabilis plumbeus</i>	Snail kite	May Affect, Not Likely to Adversely Affect	E	F,E
Federally-Listed Plant Species				
<i>Dalia carthagensis floridana</i>	Florida prairie-clover	No Effect	E	NL
<i>Chamaesyce garberi</i>	Garber's spurge	No Effect	E	NL
State-Listed Wildlife Species				
<i>Athene cunicularia floridana</i>	Florida burrowing owl	No Adverse Effect Anticipated	NL	T
<i>Egretta caerulea</i>	Little blue heron	No Adverse Effect Anticipated	NL	T
<i>Egretta tricolor</i>	Tricolored heron	No Adverse Effect Anticipated	NL	T
<i>Falco sparverius paulus</i>	Southeastern American kestrel	No Adverse Effect Anticipated	NL	T
<i>Gopherus polyphemus</i>	Gopher tortoise	No Adverse Effect Anticipated	C ⁽¹⁾	T
<i>Grus canadensis pratensis</i>	Florida sandhill crane	No Adverse Effect Anticipated	NL	T
<i>Platalea ajaja</i>	Roseate spoonbill	No Adverse Effect Anticipated	NL	T
<i>Sciurus niger avicennia</i>	Big Cypress fox squirrel	No Adverse Effect Anticipated	NL	T
State-Listed Plant Species				
<i>Andropogon arctatus</i>	Pine woods bluestem	No Adverse Effect Anticipated	NL	T
<i>Calopogon multiflorus</i>	Many flowered grass pink	No Adverse Effect Anticipated	NL	E
<i>Centrosema Arenicola</i>	Sand butterfly pea	No Adverse Effect Anticipated	NL	E
<i>Lechea cernua</i>	Nodding pinweed	No Adverse Effect Anticipated	NL	T
<i>Linum carteri var. smallii</i>	Small's flax	No Adverse Effect Anticipated	NL	E
<i>Matelea floridana</i>	Florida spiny-pod	No Adverse Effect Anticipated	NL	E
<i>Nemastylis floridana</i>	Celestial lily	No Adverse Effect Anticipated	NL	E
<i>Nolina atopocarpa</i>	Florida beargrass	No Adverse Effect Anticipated	NL	T
<i>Platanthera integra</i>	Yellow fringeless orchid	No Adverse Effect Anticipated	NL	E
<i>Tephrosia angustissima var. curtissii</i>	Coastal hoary-pea	No Adverse Effect Anticipated	NL	E

F = Federally Listed / E = Endangered / T = Threatened / T(S/A) = Threatened due to similar appearance / NL = Not Listed

¹ The gopher tortoise is currently a candidate (C) species for federal protection under the ESA.

Note: Nomenclature for species effect determinations has changed from preparation of the July 2018 NRE and through the subsequent NRE Addendums. Species effect determination nomenclature for year 2023 is presented.

Subsequent to agency review and concurrence with the *NRE* and *NRE Addendum*, two additional addendums were prepared and are discussed below.

A second *NRE Addendum* (August 2019) was also prepared under separate cover after the Public Hearing to address potential project impacts to the Florida scrub-jay and gopher tortoise resulting from Preferred Alternative alignment refinements within the same corridor through the Immokalee Regional Airport UMA. The addendum updated acreages of impact to suitable habitat for the Florida scrub-jay and gopher tortoise. The findings and conclusions of the second *NRE Addendum* remained the same as the August 2018 *NRE Addendum* in that the Preferred Alternative “*may affect, likely to adversely affect*” (*MALAA*)³ the Florida scrub-jay and will result in “*no adverse effect anticipated*” on the gopher tortoise. This addendum was submitted to agencies for review on August 9, 2019. The FWC concurred with the noted findings of the second *NRE Addendum* in a letter dated September 4, 2019 (see **Appendix N**).

A third *NRE Addendum* (September 2021) was prepared to initiate formal consultation with the FWS prior to the design and permitting phase. This addendum includes a summary of all species with prior and updated effect determinations, as well as the addition of the Eastern black rail. The third addendum also includes the Biological Assessment which addresses the prior *MALAA* determinations for the federally-listed Florida panther and Florida scrub-jay. The FDOT revised the effect determinations to *MALAA* for the following federally-listed species: Eastern indigo snake and Florida bonneted bat. The revised determinations were made based upon updated literature and database searches, field reviews, and species-specific surveys. On November 17, 2021, pursuant with Section 7 of the ESA, as amended, the FDOT OEM requested initiation of formal consultation with the FWS for the four above noted federally-listed species: Florida panther, Florida scrub-jay, Eastern indigo snake, and Florida bonneted bat. In addition, FDOT requested concurrence with the prior and updated “*no effect*” and “*may affect, not likely to adversely affect*” (*MANLAA*) determinations as documented in the *NRE*.

On May 24, 2022 and May 25, 2022, the FWS responded to the request for formal consultation by submitting Requests for Additional Information (RAI) to the FDOT OEM. Through the RAI, the FWS recommended that the determination for the Eastern indigo snake be changed from *MALAA* to “*no effect*” as this species is not reasonably certain to occur within the project corridor. In addition, the FWS recommended that the determination for Audubon’s crested caracara be modified from *MANLAA* to *MALAA* given that there is a documented active nest located approximately 279 feet west of the project footprint and the project will result in habitat loss within the Primary Zone of this nest. The FDOT OEM provided responses to the RAI on December 19, 2023 (see **Appendix S**). Through follow-up coordination with the FWS, the FDOT committed to re-initiating Section 7 consultation for the Audubon’s crested caracara. The commitment is in addition to the prior commitment to re-initiate Section 7 consultation with the FWS for the Florida panther, Florida scrub-jay, and Florida bonneted bat during the project’s design and permitting phase. The FWS provided concurrence on March 8, 2024 (see **Appendix T**). The evaluation,

³ Nomenclature for species effect determinations has changed from preparation of the July 2018 *NRE* and through the subsequent *NRE Addendums*. Species effect determination nomenclature for year 2023 is presented.

potential impacts, and mitigation measures pertaining to each of the four noted species are summarized as follows:

Florida panther: Updated literature reviews, database searches, and field reviews were completed in October 2020 and in conjunction with species specific surveys from January 2021 through May 2021. The FDOT will re-initiate Section 7 consultation for this species during the design and permitting phase for the portion of the project extending south of CR 846 to Oil Well Road, which is not currently funded for future phases. Calculation of impacts will be completed at that time and compensation will be provided through the purchase of panther habitat units (PHUs) from a FWS approved mitigation bank. The FDOT has also committed to the construction of a wildlife crossing between Oil Well Road and CR 846 to accommodate the species. The portion of the project extending north from CR 846 to SR 82 is funded through construction. Section 7 consultation will be re-initiated for this segment during the design and permitting phase. This project segment, including SMFs, is anticipated to result in 93.04 acres of Secondary Panther Zone impacts and no Primary Panther Zone impacts. These potential impacts equate to a value of 243.71 PHUs (see **Appendix S**). Compensation will be provided through the purchase of 243.71 PHUs from a FWS approved mitigation bank. To address potential impacts to the Florida panther, the FDOT commits to implementing best management practices consistent with the Florida Panther Conservation Plan.

Florida bonneted bat: Florida bonneted bat acoustic surveys were conducted from March 2021 through May 2021. A total of twenty-five (25) acoustic survey stations were established based on the minimum requirements of one station per every 0.60 miles for linear projects. The results of the acoustic surveys determined that Florida bonneted bat roosting activity is not present within the Action Area. In addition, no roosts have been identified. The presence of Florida bonneted bat echolocations confirms that the species utilizes habitat within the project area for foraging. However, the results of the survey did not determine that there was high activity. Conservation measures will be implemented by the FDOT during project construction to minimize impacts to this species. As a conservation measure for potential impacts to the Florida bonneted bat, FDOT has committed to contributing \$10,000.00 to the FWS Florida Bonneted Bat Fund administered by the Wildlife Foundation of Florida.

Florida scrub-jay: Species specific surveys were completed in October 2020. Type I, II, and III suitable Florida scrub-jay habitat is located in the northern portion of the project, specifically at the Immokalee Regional Airport and the Collier property adjacent to the bypass corridor. Two resident families of scrub-jays (five individuals total) are located on the Collier property. The FDOT proposes to mitigate at a ratio of two acres per one acre of impact for the loss of 52.14 total acres of occupied territory located on the Collier property and a ratio of four acres per one acre of impact for the loss of 15.75 acres of habitat within the UMA. Therefore, FDOT will provide a total of 167.28 acres of occupied scrub-jay habitat (104.28 associated with the loss of two scrub-jay territories within the Collier property + an additional 63 acres associated with potential habitat loss within the UMA = 167.28) as a conservation measure to compensate for the loss of scrub-jay habitat resulting from the project. The credits will be purchased from an approved mitigation bank in consultation with the FWS.

Audubon’s crested caracara: A species specific survey was conducted from January 2021 through April 2021 in accordance with the FWS Crested Caracara Draft Survey Protocol – Additional Guidance (2016-2017 Breeding Season) (FWS 2016). A total of twelve survey stations were established throughout the limits of the project. Active nesting activity was observed at two stations, Station 1 and Station 10. An active nest was documented south of the SR 29 and CR 846 intersection at Station 10, located approximately 279 feet west of the Preferred Alternative and one mile north of Oil Well Road. The FDOT will re-initiate Section 7 consultation during the design and permitting phase for this subject nest. There is an active nest located in the portion of the project north of CR 846 at Station 1, which is approximately 0.55 miles west of SR 29 and south of SR 82. Approximately 0.60 miles (3,100 feet) of the project is within the secondary zone of this nest. The FDOT’s purchase of high-quality upland and wetland credits will mitigate the loss of secondary habitat. The FDOT has also committed to implementing Audubon’s crested caracara conservation measures.

Table 3-5 summarizes the effect determinations for those federally-listed species where MALAA has been assigned or where the effect determinations have been revised as a result of further agency coordination that has taken place since the Public Hearing. All other effect determinations in Table 3-4 have not been revised.

**TABLE 3-5
MAY AFFECT, LIKELY TO ADVERSELY AFFECT OR REVISED EFFECT DETERMINATIONS**

Scientific Name	Common Name	Original Effect Determination	Revised Effect Determination
Federally-Listed & Candidate Wildlife Species			
<i>Aphelocoma coerulescens</i>	Florida scrub-jay	May Affect, Likely to Adversely Affect	Not Revised
<i>Drymarchon corais couperi</i>	Eastern indigo snake*	May Affect, Not Likely to Adversely Affect	No Effect
<i>Eumops floridanus</i>	Florida bonneted bat*	May Affect, Not Likely to Adversely Affect	May Affect, Likely to Adversely Affect
<i>Polyborus plancus audubonii</i>	Audubon’s crested caracara*	May Affect, Not Likely to Adversely Affect	May Affect, Likely to Adversely Affect
<i>Puma concolor coryi</i>	Florida panther	May Affect, Likely to Adversely Affect	Not Revised
<i>Laterallus jamaicensis jamaicensis</i>	Eastern black rail*	Not listed in 2018	May Affect, Not Likely to Adversely Affect

* Species that have revised effect determinations.

FDOT’s commitments addressing listed and protected species are discussed in **Section 5.0**. Based on adherence to these commitments, this project is expected to have no significant impacts to protected species or habitat.

3.3.9 ESSENTIAL FISH HABITAT

The National Marine Fisheries Service (NMFS) provided a “No Involvement” degree of effect during the ETDM Programming Screen; therefore, an Essential Fish Habitat assessment is not required for this project.

3.4 PHYSICAL

3.4.1 HIGHWAY TRAFFIC NOISE

A *Noise Study Report* (NSR) (July 2018) was prepared under separate cover following FDOT procedures that comply with 23 C.F.R. Part 772, *Procedures for Abatement of Highway Traffic Noise and Construction Noise*. The analysis used methodologies established by the FDOT and documented in the FDOT PD&E Manual. The prediction of existing traffic and future traffic noise levels with and without the roadway improvements was performed using the FHWA's Traffic Noise Model (TNM-Version 2.5). Detailed information on the noise analysis performed for each alternative is documented in the *NSR*.

Within the project limits, 100 noise-sensitive receptors were determined to have the potential to be impacted by traffic noise as a result of the proposed project improvements (please refer to Appendix A of the *NSR* for aerials with receiver locations). The land use review, during which these noise-sensitive sites were identified, was completed on April 25, 2018. Of the 100 evaluated noise-sensitive receptors, there are 92 residences, two schools, two receptors within one park, one medical facility, two restaurants, and one public institution (fire department).

The Preferred Alternative for SR 29 is predicted to result in exterior traffic noise levels ranging from 47.1 to 65.7 decibels on the "A"-weighted scale (dB(A)), and interior levels are predicted at 42.6 dB(A) at the 100 evaluated noise-sensitive receptors. Of the 100 noise sensitive sites evaluated, none of the sites are predicted to experience future traffic noise levels that approach, meet, or exceed the Noise Abatement Criteria (NAC) for their respective Activity Category. The results of the analysis also indicate that when compared to existing conditions, traffic noise levels would not increase more than 9.8 dB(A) above existing conditions with the proposed improvements at any of the evaluated sites. As such, none of the evaluated sites will experience a substantial increase in traffic noise [15 dB(A) or more] as a result of the proposed project. Therefore, noise abatement measures were not warranted for the noise sensitive sites identified adjacent to the Preferred Alternative.

Subsequent to the Public Hearing, a *NSR Addendum* (March 2024) was prepared, under separate cover, to address design refinements made to the Preferred Alternative to meet the FDM requirements and the identification of proposed SMFs necessary to accommodate stormwater runoff from CR 846 to SR 82. In addition, land use reviews were performed on December 12, 2023 and February 6, 2024 to identify land use changes and all noise sensitive sites that received a building permit subsequent to the noise study completed and documented in the *NSR* (July 2018). As part of this analysis, eighteen additional noise-sensitive receptors were identified within a new residential development along Foundation Way. Additionally, the Preferred Alternative design refinements resulted in a reduction in the exterior traffic noise levels from a range of 47.1 to 65.7 dB(A) to a range of 44.7 to 61.6 dB(A). The levels are not expected to approach, meet, or exceed the NAC at any receptor under existing conditions; however, substantial noise level increases [15 dB(A) or more] are predicted for eight receptors within the new residential development under future conditions. Although traffic noise abatement measures were considered for the noise-sensitive receptors, no feasible and reasonable measures meeting the NAC criteria were identified

that could be implemented as part of the project to abate traffic noise for the eight impacted receptors.

Another land use review will be performed during the project design phase to identify all noise sensitive sites that received a building permit subsequent to the noise study but prior to the project's Date of Public Knowledge. The date that the FONSI is approved by the FDOT OEM will be the Date of Public Knowledge. If the review identifies noise sensitive sites that have been permitted after the noise study but prior to the Date of Public Knowledge, then those sites will be evaluated for traffic noise impacts and abatement considerations.

Based on the traffic noise analysis, the consideration of noise abatement measures to mitigate traffic noise impacts, the Preferred Alternative is expected to have no significant impact on potential noise sensitive sites.

3.4.2 AIR QUALITY

The project is located in an area which is designated attainment for all of the National Ambient Air Quality Standards under the criteria provided in the Clean Air Act. Therefore, the Clean Air Act conformity requirements do not apply to this project. This project is expected to improve traffic flow by adding capacity to relieve congestion, which should reduce operational greenhouse gas emissions.

Therefore, the Preferred Alternative is expected to have no significant impact on air quality.

3.4.3 CONTAMINATION

Pursuant to FHWA's Technical Advisory T 6640.8A and the FDOT PD&E Manual, a Level I contamination screening evaluation was performed for the project and a *Contamination Screening Evaluation Report (CSER)* (July 2018) was prepared under separate cover. The Level I assessment was performed to identify and evaluate sites containing hazardous materials, petroleum products, or other sources of potential environmental contamination along the SR 29 project corridor.

The *CSER* included standard environmental site assessment practices of reviewing records of regulatory agencies, site reconnaissance, literature review, and personal interviews of individuals and business owners within the limits of the project. For purposes of this report, the project study area included the limits of the mainline project and a 1,320-foot area extending from the centerline of the mainline.

Subsequent to the Public Hearing, a *CSER Addendum* (March 2024) was prepared, under separate cover, to supplement and update findings of the *CSER* (July 2018) to address the design refinements made to the Preferred Alternative. The project study area used was consistent with the area evaluated in the *CSER* (July 2018).

Based on the *CSER* (July 2018) and the *CSER Addendum* (March 2024) and site reviews for the Preferred Alternative, four sites ranked "High", 30 sites ranked "Medium", and 41 sites ranked

“Low” or “No” for potential contamination within the project corridor. Seven SMFs/pond sites within the northern portion of the project corridor also ranked “Medium” (see **Table 3-6**).

For the sites that ranked “Low,” no further action is required at this time. These sites/facilities have the potential to impact the proposed project, but based on select variables, these sites have been determined to have low risk to the project at this time. Variables that may change the risk ranking include a facility’s non-compliance with environmental regulations, new discharges to the soil or groundwater, and modifications to current permits. Should any of these variables change, re-assessment of these facilities will be conducted during subsequent project development phases.

For those locations with a risk ranking of “Medium” and “High”, including any proposed stormwater treatment ponds and/or floodplain compensation sites outside the FDOT right-of-way, Level II screening (which includes testing), as warranted, will be conducted during the design phase if it is determined that construction activities could encounter contamination or if the site will be subject to right-of-way acquisition.

Future project design plans will contain marked contamination polygons and general notes as applicable. FDOT will conduct Level III evaluation of contamination and/or hazardous waste remediation and/or abatement, as necessary. The Contractor will be responsible for obtaining and ensuring compliance with any necessary dewatering permit(s) and consulting with the District Contamination Impact Coordinator. Any dewatering operations in the vicinity of potentially contaminated areas shall be limited to low-flow, short-term operations. A dewatering plan may be necessary to avoid potential contamination plume exacerbation.

Additionally, Section 120, Excavation and Embankment – Subarticle 120.1.2, Unidentified Areas of Contamination of the FDOT’s *Standard Specifications for Road and Bridge Construction* will be provided in the project construction documents. This specification requires that in the event that any hazardous material or suspected contamination is encountered during construction, or if any spills caused by construction-related activities should occur, the Contractor shall be instructed to stop work immediately and notify the FDOT, as well as the appropriate regulatory agencies for assistance.

Contamination is not expected to have a significant impact on construction of the Preferred Alternative based on 1) the future completion of Level II field screening for the “High” and “Medium” risk-ranked sites identified, 2) the completion of contamination remediation activities as determined necessary (following future Level III testing activities), 3) the inclusion of the appropriate contamination demarcation in the construction plans, and 4) adherence to standard specs related to handling known and unknown contamination.

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**TABLE 3-6
MEDIUM/HIGH RANKED SITES**

Site/ Ranking	Site Name And Address/ Parcel Number	Acquisition For:	Site/ Ranking	Site Name And Address/ Parcel Number	Acquisition For:
FA-2 Medium	CDC Land Investments Inc. Parcel Number 00231840000	Pond 6 Pond 7	Site 26 High	Combs Oil Co Immokalee Bulk Facility (also Balgas, Combs Oil Co Immokalee Truck Stop, and N & R Gas Station) 525 E Main Street (also listed at 527 E Main Street)	None
FA-12 Medium	Floyd Crews Property 861 County Road 846 Parcel Number 00120842009	Project Corridor	Site 28 High	Davis Oil Company (also Sunoco Gas Station, Gator Food Store, and Oleum Corp) 726 E Main Street (also listed at 730 E Main Street) Parcel Number 00116560007	Project Corridor
FA-14 Medium	All Star Truck Brokers (also J&B Rentals of Immokalee LLC, David H Carter Trust property) 19301 Immokalee Rd	Project Corridor	Site 30 High	Davis Oil Company Service Center Aka Fina Service Station 524 E Main Street	None
FA-15 Medium	Gopher Ridge I Joint Venture Parcel Number 00087520008 and Parcel Number 00087440007	Pond 31-C2 (Pond 502A) Project Corridor	Site 32 Medium	Doug's Garage 535 New Market Road E	None
FA-16 Medium	Gopher Ridge I Joint Venture Parcel Number 00087520008	Project Corridor	Site 33 Medium	Flores Tire (also Lebonberger) 528 New Market Road E	None
FA-17 Medium	Gopher Ridge I Joint Venture Parcel Number 00087440007	Project Corridor	Site 38 Medium	Collier County (BOCC) – Immokalee Airport Area Brownfield Former Hanger Areas D, E, F and G 105 Airpark Blvd. Parcel Number 115560008	(Pond 501B) Project Corridor
FA-18 Medium	Gopher Ridge I Joint Venture Parcel Number 00068760007	Project Corridor	Site 38A Medium	Immokalee Airport Former Airwork Fuel Farm Area	(See Above)
FA-19 Medium	Barron Collier Partnership Parcel Number 00067880001	Pond 38	Site 38B Medium	Immokalee Airport Airwork Pesticide Staging Area	(See Above)
FA-22 Medium	Collier Citrus LTD Consolidated Citrus LTD Partnership Parcel Number 00139720002	Pond 17	Site 38C Medium	Immokalee Airport Former Johnson Fuel Farm Area	(See Above)
FA-23 Medium	Collier Citrus LTD Consolidated Citrus LTD Partnership Parcel Number 00139720002	Pond 16 FPC C	Site 38D Medium	Immokalee Airport Former Unnamed Hanger Area	(See Above)
FA-24 Medium	Gargiulo Inc. Parcel Number 00140261000	Pond 15	Site 38E Medium	Immokalee Airport Former South Johnson Hanger Area	(See Above)
FA-25 Medium	Collier Citrus LTD Consolidated Citrus LTD Partnership Parcel Number 00140450002	Pond 11	Site 38F Medium	Immokalee Airport Former Crapse Hanger Area	(See Above)
FA-26 Medium	Collier Citrus LTD Consolidated Citrus LTD Partnership Parcel Number 00140450002	Pond 10	Site 38G Medium	Immokalee Airport Former North Johnson Hanger Area	(See Above)
FA-27 Medium	Collier Citrus LTD Consolidated Citrus LTD Partnership Parcel Number 00231684004	Pond 8 Pond 9	Site 39 Medium	South Florida Packers (also Nobles Collier and A&A Produce) 212 Jerome St	Project Corridor
Site 3 Medium	Sunniland Country Store 13213 CR 858	None	Site 56 Medium	M & M Salvage and Used Auto Parts, Inc. (also Immokalee Waste Tire Site/Robert's Auto Salvage, W & T Salvage Yard, and Jay's Towing) 106 Dixie Avenue E	Design Pond 502A
Site 10 Medium	Peninsula Improvement Corp 100 Farm Worker VI E Parcel Number 00137120002	Project Corridor	Site 69 Medium	Sunniland Pipeline	Pond 35 (Pond 602B), (Pond 603/604B), Pond 39 (Pond 605A) Project Corridor
Site 11 Medium	Circle K #7424 1117 E Main Street	None	Site 72 High	Howard Fertilizer Spill	(Pond 606A) Project Corridor
Site 19 Medium	Liquid Plant Inc. 1001 CR 846 East Parcel Number 00116520005	Pond 27A-C2	Site 74 Low	Row Crops 3637 SR 29 NOTE: Pond 607A is ranked Medium given the use of row crops on Site 74	(Pond 607A)

(Pond XXXX) = Proposed SMF/pond identified as part of Preferred Alternative design refinements from CR 846 to SR 82.

3.4.4 UTILITIES AND RAILROADS

The preliminary utility coordination and investigation effort was conducted through written and verbal communications with the existing utility owners. A Sunshine State 811 of Florida Design Ticket System listing of existing Utility Agencies/Owners (UAOs) was acquired on March 5, 2018. The utility types obtained from the Sunshine State 811 of Florida Design ticket are listed in **Table 3-7**.

A *Utility Request Package* was submitted via email to the UAOs on June 8, 2018 to obtain the locations of existing and/or planned utilities. **Table 3-7** below was updated with facilities information received in 2020⁴. Widening SR 29 will require relocation of some existing utilities. A *Utilities Assessment Package*, prepared under separate cover, was completed after the Public Hearing held on February 6, 2019 and coordination was completed with the UAOs for potential utility conflicts and to obtain relocation cost estimates. Cost estimates will be finalized in the final design phase. The FDOT’s coordination with potentially affected utility owners started during the PD&E Study and will continue throughout the design and construction phases. Project design will seek to avoid and minimize impacts to existing utilities to the extent feasible within roadway right-of-way. A full discussion of utilities can be found in Sections 2.12 and 6.9 of the *PER* prepared under separate cover.

There are no at-grade or grade-separated railroad crossings within the project study area.

**TABLE 3-7
EXISTING UTILITIES OVERVIEW**

Utility Type	Utility	Summary of Facilities
Cable TV/ Communications/ Fiber Optic	Collier County Traffic Operations Section	Collier County operates and maintains the ATMS infrastructure that includes the signalized intersection on SR 29 at Farm Worker Way, North 1 st Street, North 9 th Street, Immokalee Drive, and Lake Trafford Road.
	Collier County Information Technology (IT)	No utilities within the project limits.
	Comcast	Existing aerial Comcast facilities run along SR 29 on the west side of the roadway from Farm Workers Way to Jerome Drive. Existing aerial Comcast facilities run along CR 846 on the south side of the roadway throughout the project limits. There is an existing network of aerial and underground facilities in the downtown Immokalee area from CR 846 to Flagler Street. Existing aerial Comcast facilities run along SR 29 on the east side of the roadway from south of Westclox Street/New Market Road W to south of SR 82.
	Crown Castle Fiber	Overhead fiber optic crosses SR 29 at dirt road north of Johnson Road. Buried fiber optic runs from SR 29 westward at same dirt road.
	Summit Broadband Inc.	Fiber Optic runs along north side of CR 846 crossing roadway at 12 th Street continuing along SR 29. Fiber Optic runs along west side of SR 29 from south of Westclox Street/New Market W to north of SR 82.
	Lipman Family Companies	Information not yet received from UAO

⁴ Since May 2020, Florida Power and Light Company constructed the FPL Immokalee Solar Energy Center at 3350 SR 29 N, Immokalee, FL 34142. The 74.5 megawatt facility is on 578 acres east of SR 29 and north and south of SR 82. There are two sets of transmission lines aerially crossing SR 29 at the southern end of the Solar Energy Center.

**TABLE 3-7
EXISTING UTILITIES OVERVIEW (CONTINUED)**

Utility Type	Utility	Summary of Facilities
Cable TV/ Communications/ Fiber Optic (continued)	CenturyLink – Naples	Buried copper and fiber telephone lines along the east side of SR 29 south of Oil Well Road. Buried fiber crosses SR 29 south of Oil Well Road. Buried fiber runs along south side of Oil Well Road. Buried copper runs along south side of Oil Well Road east of SR 29. Buried copper and fiber run along east side of SR 29 before fiber crosses SR 29 at station 125+10.00. Fiber continues on west side of SR 29 until Trans Gro Road where copper begins again. Buried copper and fiber run along west side of SR 29 until Seminole Crossing Trail. Fiber is consistent while copper varies. North of Seminole Crossing Trail copper and fiber run below the existing geometry of the roadway. Buried fiber and copper run along north side of CR 846. Buried copper and fiber run along both sides of New Market Road as well as below existing roadway until Charlotte Street. Buried copper and fiber run on both sides of SR 29 from south of Westclox Street/New Market Road W to end of project limits at SR 82.
Water/Sewer	Immokalee Water & Sewer District	South of Agriculture Way to New Market Road, there is a network of varying size PVC water mains and PVC force mains. North of New Harvest Road to New Market Road, there is a network of gravity sanitary sewers including manhole covers. 8" PVC water main on west side of SR 29 from south of Westclox Street/New Market Road W to Heritage Boulevard. 10" PVC gravity sanitary sewer runs across Westclox Street/New Market Road W west of SR 29. 12" PVC water main crosses SR 29 at Heritage Boulevard.
Electric	Lee County Electric Co-Op	Overhead electric along west side of SR 29 from Oil Well Road to New Market Road with multiple crossings, primarily at cross streets. Overhead electric along south side of CR 846. Overhead electric along east and west sides of New Market Road with various crossings ending at Flagler Street. Overhead electric along west side of proposed bypass for Central Alternative #2 with multiple crossings at the wastewater treatment plant. Overhead electric crosses proposed roadway at Alachua Street. Overhead electric along east side of SR 29 from Westclox Street/New Market Road W to SR 82 with multiple crossings, primarily at cross streets.
Petroleum Pipeline	Calumet Pipeline Holdings (Suniland Petroleum Pipeline)	Calumet Pipeline Holdings (Suniland Petroleum Pipeline) owns an abandoned 6" tar coated steel pipeline throughout the project limits. The pipeline runs along SR 29 from Oil Well Road to CR 846. At CR 846, the pipeline orients to the northwest and follows New Market Road until reaching SR 29. At SR 29, the pipeline turns north running adjacent to the roadway beyond the project limits at SR 82. The location of the pipeline relative to the existing roadway is unknown.

3.4.5 CONSTRUCTION

Construction activities for the proposed SR 29 improvements may cause minor short-term air quality, noise, traffic congestion, and visual impacts for those residents and travelers within the immediate vicinity of the project.

The air quality effect will be temporary and will primarily be in the form of emissions from diesel-powered construction equipment and dust from embankment and haul road areas. Air pollution associated with the creation of airborne particles will be effectively controlled through the use of

watering or the application of other controlled materials in accordance with FDOT's *Standard Specifications for Road and Bridge Construction*.

Noise and vibration impacts will be from heavy equipment movement and construction activities. These impacts will be minimized by adherence to noise control measures found in the most current edition of FDOT's *Standard Specifications for Road and Bridge Construction*. Specific noise level problems that may arise during construction will be addressed by the Construction Engineer in cooperation with the appropriate Environmental Specialist.

Maintenance of traffic and sequence of construction will be planned and scheduled to minimize traffic delays throughout the project. Signage will be used as appropriate to provide pertinent information to the traveling public. The local news media will be notified in advance of road closings and other construction related activities that would inconvenience the community so that motorists, residents, and businesspersons can plan alternate routes. Applicable provisions of the FDOT's *Standard Specifications for Road and Bridge Construction* will be followed. A sign providing the name, address, and telephone number of an FDOT contact person will be displayed on-site as well as included on the project website to assist the public in obtaining immediate answers to questions.

Access to local properties, businesses, and residences will be maintained to the extent practical during construction through controlled construction scheduling and the implementation of the project's specific Traffic Control Plan(s).

For residents living along the project, some of the construction materials stored for the project may be displeasing visually; however, this is a temporary condition and should pose no substantial conflict in the short term.

3.4.6 BICYCLES AND PEDESTRIANS

Within the rural sections of SR 29, from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road W to SR 82, there are no existing pedestrian accommodations. At SR 29 and Farm Worker Way, there is a grade-separated pedestrian bridge to accommodate students traveling to/from Village Oaks Elementary School. Along SR 29 from Farm Worker Way to New Market Road, there is a continuous sidewalk on the west side of the corridor. Along SR 29 from New Market Road to Westclox Street/New Market Road W and along the entirety of New Market Road, there are continuous sidewalks on both sides of the corridors. Along the majority of SR 29 and New Market Road, the sidewalks vary from five to eight feet wide and have a continuous grass buffer or on-street parking buffer. There are crosswalks at each of the signalized intersections along SR 29 and New Market Road within the study area. Also, there are three midblock crossings along SR 29 from North 1st Street to North 9th Street.

Within the rural sections of SR 29, from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road W to SR 82, a paved shoulder of five feet exists on either side of the roadway. There are no bicycle accommodations along the entirety of New Market Road or along SR 29 from North 1st Street to North 9th Street. Along SR 29 from south of Farm

Worker Way to 13th Street and from North 9th Street to north of Westclox Street/New Market Road W, there are designated four-foot to five-foot bicycle lanes on either side of the roadway.

The Preferred Alternative includes proposed improvements to SR 29 that provide pedestrian and bicycle facilities as summarized in **Table 3-8**. The sidewalk and bicycle facilities in the project will be designed and constructed to comply with the Americans with Disabilities Act (ADA) of 1990, as amended. The sidewalks will meet ADA requirements for access, width, and grade. The project is anticipated to enhance and/or add bicycle and pedestrian facilities.

The pedestrian and bicycle network of the area is complemented by the Collier County transit network. Collier Area Transit (CAT) is the transit service provider for Collier County. CAT Routes 19, 22, and 23 travel along SR 29 and/or New Market Road through some portions of the study area. **Figure 3-4** shows the CAT bus routes along and around SR 29 and New Market Road within the study corridor.

**TABLE 3-8
PROPOSED PEDESTRIAN AND BICYCLE FACILITIES**

SR 29 Segment	Pedestrian	Bicycle
Oil Well Road to South of Kaicasa Entrance	None	5-foot paved outside shoulder (both directions) 4-foot paved inside shoulder (both directions)
South of Kaicasa Entrance to North of Seminole Crossing Trail	10-foot shared-use path (west)	5-foot paved outside shoulder (both directions) 4-foot paved inside shoulder (both directions)
North of Seminole Crossing Trail to South of CR 846	6-foot sidewalk (both directions)	7-foot buffered bicycle lane (both directions)
South of Westclox Street/New Market Road W to Heritage Boulevard	10-foot shared-use path (both directions)	5-foot paved outside shoulder (both directions) 4-foot paved inside shoulder (both directions)
Heritage Boulevard to SR 29 Bypass Junction	12-foot shared-use path (both directions)	5-foot paved outside shoulder (both directions) 4-foot paved inside shoulder (both directions) - varies
Experimental Road to South of SR 82	10-foot shared-use path (both directions)	5-foot paved outside shoulder (both directions) 4-foot paved inside shoulder (both directions)
(Bypass) South of CR 846 to Gopher Ridge Road	12-foot shared-use path (both directions)	Same as Pedestrian Facilities
(Bypass) Gopher Ridge Road to Experimental Road	12-foot shared-use path (both directions)	5-foot paved outside shoulder (both directions) 4-foot paved inside shoulder (both directions)

**FIGURE 3-4
EXISTING TRANSIT ROUTES**



3.4.7 NAVIGATION

There are no navigable waters of the United States within the SR 29 study area. Both the USACE and the United States Coast Guard (USCG) confirmed this during their review of the project in the EST as part of the ETDM Programming Screen phase. These agencies additionally indicated in their respective reviews that no further involvement or coordination is required regarding navigation.

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3.5 ANTICIPATED PERMITS

On February 15, 2024, a federal court validated the FDEP’s 404 permitting program. Therefore, 404 permitting will proceed with the USACE. Both the USACE and SFWMD regulate impacts to wetlands within the project study area. Other resource agencies, including the NMFS, United States Environmental Protection Agency (USEPA), and FWC, review and comment on wetland permit applications. In addition, the FDEP regulates stormwater discharges from construction sites. The complexity of the permitting process will depend greatly on the degree of the impact to jurisdictional areas. Each permit will be obtained during design or prior to construction. It is anticipated that the following permits will be required for this project:

<u>Permit</u>	<u>Issuing Agency</u>	<u>Status</u>
Section 404 Clean Water Act Permit	USACE	To Be Acquired
Environmental Resource Permit (ERP)	SFWMD	To Be Acquired
National Pollutant Discharge Elimination System (NPDES) Construction Generic Permit	FDEP	To Be Acquired
Gopher Tortoise Relocation Permit	FWC	To Be Acquired

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SECTION 4.0

COMMENTS AND COORDINATION

A comprehensive *Public Involvement Plan* (PIP) (July 20, 2007, revised March 8, 2018), prepared under separate cover, was developed for this project. The PIP was originally approved on August 3, 2007, with the revision approved on April 3, 2018. This program was implemented in compliance with the FDOT PD&E Manual; Section 339.155, F.S.; Executive Orders 11990, *Protection of Wetlands*, and 11988, *Floodplain Management*; Council on Environmental Quality (CEQ) Regulations for implementing the procedural provisions of the National Environmental Policy Act (NEPA); and 23 C.F.R. Part 771. A full discussion of public involvement activities is included in the *Comments and Coordination Report* (May 2020), prepared under separate cover. A *Comments and Coordination Report Addendum* (June 2024) was also prepared, under separate cover, to document additional coordination and engagement activities that took place after the Public Hearing.

4.1 DISCUSSION OF ETDM PROGRAMMING SCREEN AND ADVANCE NOTIFICATION

The project was screened through the EST as part of the ETDM Programming Screen phase (ETDM Project #3752). Four separate screening events took place, spanning 2005 to 2009, due to the challenges associated with this project (implementing capacity improvements within a downtown core versus constructing a new roadway within environmentally sensitive lands to divert traffic from the downtown core). As such, several alternatives were developed over the time frame and, subsequently, screened through the ETDM Process. Five project alternatives were reviewed through the series of screening events. Alternatives #1 and #2 were reviewed as part of screening event #1, Alternative #3 as part of screening event #2, Alternative #4 as part of screening event #3, and Alternative #5 as part of screening event #4.

Given the number of screening events, two Advance Notifications (ANs) or AN Packages were distributed. The first AN was issued on August 9, 2007; the second AN was distributed on July 11, 2008. The AN Packages were submitted to the FDOT District One Environmental Technical Advisory Team (ETAT) for review and comment separately from the ETDM Programming Screen. A full list of the agencies that received the AN Packages is provided in the May 2020 *Comments and Coordination Report*. Comments were received on the AN Package from the Florida State Clearinghouse, FAA, FDEP South District Office, Florida Department of Community Affairs (FDCA), Florida Division of Historical Resources/Bureau of Historic Preservation, STOF, SFWMD, Southwest Florida Regional Planning Council (SWFRPC), and USACE. The comments received were related to each agency's permitting requirements and stressed avoidance and minimization of impacts to environmental and cultural resources. Other comments noted that the project is regionally significant and is consistent with planning goals for the area. There were no adverse comments regarding the proposed roadway improvements. All

comments have been considered and substantive comments have been addressed in the appropriate sections of this report.

During the various screening events, the ETAT identified potential impacts to environmental and cultural resources. In the screening event for Alternative #3, FWS assigned a Dispute Resolution Degree of Effect to two issues: Wildlife and Habitat and Secondary and Cumulative Effects. FWS indicated that due to the location of Alternative #3 within FWS Panther Consultation Area as well as both Primary and Secondary Panther Habitat Zones, the project will adversely impact the Florida panther as a result of lost habitat and an increase in the probability of vehicle collisions. In addition to these direct impacts, the FWS also stated that the project will result in indirect effects to the Florida panther by promoting additional development of panther habitat within the project area that would not go forward without the presence of transportation infrastructure.

Several meetings were convened with representatives from various agencies (including FWS, Collier MPO, the Immokalee Focus Group, private property owners, Collier County, and FWC) to discuss the dispute of (concerns with) Alternative #3. Based on coordination between FDOT and the noted agencies, the dispute regarding the project was resolved. An ETDM Dispute Resolution Log, documenting activities of the dispute resolution process, may be reviewed in the EST as part of the project's record.

Overall, concerns raised by ETAT members, local organizations, and the public as part of the ETDM Process were resolved through additional environmental analysis and outreach/coordination as documented throughout Sections 2.0 and 3.0 of this document. Comments received from these stakeholders helped to identify feasible alternatives that are being advanced for consideration as part of this PD&E Study. Specific agency comments and FDOT District One's responses to these comments are documented in the ETDM *Programming Screen Summary Reports*, prepared under separate cover.

4.2 COORDINATION AND CONSULTATION

Throughout the duration of the SR 29 Immokalee PD&E Study to present, the FDOT attended community events and participated in numerous coordination meetings and consulted with FHWA, FAA, Collier County Growth Management, Collier MPO and its Committees, the Immokalee Community Redevelopment Agency (CRA), a Stakeholders Advisory Committee (SAC), government and non-government agencies, and the public to solicit input on the project.

Additional coordination took place between the FAA and FDOT OEM based on comments received in October 2018 from the FAA that questioned the designation of the Immokalee Airport Conservation Easement as a "significant resource" under Section 4(f). FDOT OEM concurred with FAA's determination that Section 4(f) does not apply as the primary purpose of the land is airport use.

Table 4-1 provides a list of public meetings conducted to date/scheduled for the project. Spanish translators were present at the milestone meetings; Creole translators were available upon request. Brief summaries of the milestone public meetings and workshops, including comments received, are provided below. Full documentation of the public meetings and outreach activities are included

in the *Comments and Coordination Report* (May 2020) and *Comments and Coordination Report Addendum* (June 2024).

**TABLE 4-1
PUBLIC MEETINGS**

Meeting/Presentation	Date
Agency and Public Purpose and Need Scoping Meetings*	October 18, 2007
Stakeholder Advisory Committee No. 1	November 1, 2007
Stakeholder Advisory Committee No. 2	July 24, 2008
Corridor Public Workshop*	August 7, 2008
Stakeholder Advisory Committee No. 3	April 23, 2009
Large Property Owners Meeting	June 23, 2009
Alignments Public Workshop*	June 23, 2009
Public Alternatives Scoping Meeting*	February 17, 2010
Agency Alternatives Scoping Meeting (WebEx)**	February 18, 2010
Stakeholder Advisory Committee No. 4	August 5, 2010
Immokalee Community Redevelopment Agency	September 15, 2010
Large Property Owners Meeting	December 7, 2011
Eastern Collier Chamber of Commerce	December 11, 2011
Immokalee Community Redevelopment Agency	December 21, 2011
Large Property Owners Meeting	August 16, 2013
Immokalee Community Redevelopment Agency	August 21, 2013
Collier MPO Technical Advisory Committee & Citizens Advisory Committee	August 26, 2013
Collier MPO Board	September 13, 2013
Stakeholder Advisory Committee No. 5	September 16, 2013
Stakeholder Advisory Committee No. 6	January 23, 2014
Immokalee Community Redevelopment Agency	January 23, 2014
Collier MPO Technical Advisory Committee & Citizens Advisory Committee	February 24, 2014
Collier MPO Board	March 14, 2014
Immokalee Harvest Festival	March 29, 2014
Alternatives Public Workshop*	April 3, 2014
Collier MPO Board	April 11, 2014
Collier MPO Technical Advisory Committee & Citizens Advisory Committee	April 21, 2014
Immokalee Community Redevelopment Agency	August 16, 2017
Collier MPO Technical Advisory Committee & Citizens Advisory Committee	August 28, 2017
Collier MPO Board	September 3, 2017
Alternatives Public Workshop #2*	November 9, 2017
Immokalee Cattle Drive and Jamboree	March 10, 2018
Collier County Airport Authority staff	April 19, 2018
Collier County Parks and Recreation Division staff	April 19, 2018
Public Hearing*	November 15, 2018

* Milestone Meeting with Spanish translator(s) present. ** Milestone Meeting with Spanish translator(s) available upon request.

**TABLE 4-1
PUBLIC MEETINGS (CONTINUED)**

Meeting/Presentation	Date
Collier MPO Technical Advisory Committee & Citizens Advisory Committee	April 29, 2019
Collier MPO Board	May 10, 2019
Immokalee Community Redevelopment Agency	April 17, 2024
Project Update: FDOT In-Person Office Hours*	April 18, 2024
Collier MPO Technical Advisory Committee & Citizens Advisory Committee	April 22, 2024
Project Update: FDOT Live Online Office Hour**	April 23, 2024
Collier MPO Board	May 10, 2024

* Milestone Meeting with Spanish translator(s) present. ** Milestone Meeting with Spanish translator(s) available upon request.

PURPOSE AND NEED SCOPING MEETINGS

Two Purpose and Need Scoping Meetings were held at the beginning of the project. The Agency Purpose and Need Scoping Meeting was held on October 18, 2007 from 2:00 p.m. to 4:00 p.m. at the Immokalee One-Stop Career Center, Immokalee. The Public Purpose and Need Scoping Meeting was held the same day at the same location from 5:30 p.m. to 7:30 p.m. The purpose of these scoping meetings was to review and receive comments on the draft purpose and need statement developed for the project.

CORRIDOR PUBLIC WORKSHOP

A Corridor Public Workshop was held on August 7, 2008 at the Immokalee One-Stop Career Center, Immokalee, where four corridors (Existing SR 29 Corridor, West Corridor, Central Corridor, and East Corridor) were presented for consideration at the workshop. The workshop was attended by 55 people. A total of 24 comments were received as a result of the Corridor Public Workshop. The majority stated a preference for the East Corridor, one individual each preferred the Existing Corridor and Central Corridor, and none preferred the West Corridor. Other concerns cited were the need for access to the industrial zone near the airport; the need to minimize impacts to residential properties, churches, and stores; the need to keep trucks/freight traffic out of downtown; the need to include bicycle/pedestrian facilities; and the need to avoid environmental impacts. All of the comments received were taken into consideration in the development of the corridors. Stand-alone Spanish language versions of all handouts and meeting materials were made available at this workshop and at all other public meetings associated with this study effort. Bilingual (English and Spanish) staff were present at all public meetings for translation services, as needed, given the large number of Spanish speaking individuals present within the project study area.

ALIGNMENTS PUBLIC WORKSHOP

An Alignments Public Workshop was held on June 23, 2009 at the Immokalee One-Stop Career Center, Immokalee, where five “representative alignments” [Alignment A (Existing Corridor), Alignment E (West Corridor), Alignment L (Central Corridor), Alignment S (East Corridor), and Alignment U (East Corridor)] were presented based on coordination with and input from FHWA,

the Stakeholder Advisory Committee (SAC), resource agencies, and the public. The workshop was attended by 22 people. All attendees were given the opportunity to provide written comments at the workshop or within a 10-day comment period. A total of eight comments were received at the Alignments Public Workshop from participants, and two additional comments were received as a result of the workshop, one via the project website and one via email. Additional comments were received from a meeting that was held on the same day as the workshop with a group of large property owners in the project area. Based on the comments: four favored Alignment S, one favored Alignment A, and two favored Alignment E. Other concerns/suggestions relayed were impacts on private properties, concerns that a bypass would harm downtown businesses, the need to minimize impacts to the human and natural environments, and suggestions of ways to revise/modify the representative alignments. All of the comments received were taken into consideration in the development of the alignments. FDOT continued to utilize the previously stated accommodations to enhance public outreach efforts to the Limited English Proficiency (LEP) populations within the SR 29 study area.

PUBLIC AND AGENCY ALTERNATIVES SCOPING MEETINGS

The Public Alternatives Scoping Meeting was held on February 17, 2010 at the Immokalee One-Stop Career Center, Immokalee. An Agency Alternatives Scoping Meeting was held the following day on February 18, 2010 as a WebEx meeting. At both meetings, four preliminary alternatives (Existing SR 29 Alternative, West Preliminary Alternative, Central Preliminary Alternative, and East Preliminary Alternative) were presented. The No-Build Alternative, which remained a viable alternative through the PD&E process, was also presented. The purpose of the scoping meetings was to:

- Review the process used to get to the alternatives stage and discuss progress made to date.
- Identify the range of alternatives which were to be carried forward for analysis from the corridor and alignment stages.
- Determine the potential impacts to be evaluated, including the scope and degree of analysis required to evaluate the alternatives to be considered in the environmental document.
- Identify issues which were identified during the ETDM process as not needing further study, or which needed only minor analysis. This would narrow discussion in the environmental document to a brief description of why they will not have a significant effect on the human or natural environment or providing a reference to their coverage elsewhere.
- Identify other Environmental Assessments or Environmental Impact Statements which are being prepared in the vicinity of the project that are related to, but are not part of, the scope of the environmental document under consideration.
- Identify other environmental review and consultation requirements so the lead and cooperating agencies may prepare other required analyses and studies concurrently with, and integrated with, the environmental document.

At the Public Alternatives Scoping Meeting, aerial photographs and other project information were available for public viewing in an informal open house format. The meeting was attended by 15 citizens. All attendees were given the opportunity to provide written comments at the workshop or within a 10-day comment period. A total of 12 comments were received at the meeting. Of those

comments, 11 supported the East Preliminary Alternative, and one supported the Existing SR 29 Alternative. The West and Central Preliminary Alternatives did not receive any support. The comments generally supported the East Preliminary Alternative because it took traffic out of downtown Immokalee and directed trucks to the industrial area. The support for the Existing SR 29 Alternative was to keep traffic in downtown Immokalee.

The Agency Alternatives Scoping Meeting was conducted as a WebEx Meeting with alternatives presented via shared computer screen and 12 representatives participated from a range of agencies. Issues relating to each alternative were discussed to determine if any alternatives could be dropped at the time or if other alternatives needed to be developed. Issues with the West Alternative, including the social and natural environment, were discussed as being major and unavoidable. The agencies stated that any panther habitat between any new road and Immokalee would be considered a loss. Adjustments to the Central and East Alternatives to move them closer to town and take more direct paths were discussed.

The Public and Agency Alternatives Scoping Meetings, and subsequent coordination, resulted in the following actions:

- No-Build Alternative: Moved forward for further evaluation;
- Existing SR 29 Alternative: Moved forward for further evaluation;
- West Preliminary Alternative: Eliminated by FHWA on June 1, 2010;
- Central Preliminary Alternative: Revised to become Central Preliminary Alternative #1, which was advanced for further evaluation; and
- East Preliminary Alternative: Revised to become East Preliminary Alternative #1 and East Preliminary Alternative #2, both of which were advanced for further evaluation.

FDOT representatives were available at the meetings to answer questions and continued to utilize the previously stated accommodations to enhance public outreach efforts to the LEP populations within the SR 29 study area. All of the comments received were taken into consideration in the development of the alternatives.

ALTERNATIVES PUBLIC WORKSHOP – APRIL 3, 2014

An Alternatives Public Workshop was held on April 3, 2014 at the Immokalee One-Stop Career Center, Immokalee, where four alternatives (No-Build Alternative, Existing SR 29 Alternative, Central Alternative #1 Revised, and Central Alternative #2) were presented. The workshop was attended by 40 people. All attendees were given the opportunity to provide written comments at the workshop or within a 10-day comment period. A total of 17 comments were received: one favored the No-Build Alternative, three favored the Existing SR 29 Alternative, and thirteen favored Central Alternative #2; the majority of responders were against Central Alternative #1 Revised. An additional 26 comments were received following the workshop, which were in opposition to roundabouts. Other concerns expressed from stakeholders and the public regarding the Existing SR 29 Alternative and Central Alternative #1 Revised included bicycle and pedestrian safety issues and the funneling of traffic through key portions of Immokalee, which would bisect portions of the town and result in impacts to key structures and limitations on future

redevelopment. FDOT representatives were available at the workshop to answer questions and continued to utilize the previously stated accommodations to enhance public outreach efforts to the LEP populations within the SR 29 study area. All of the comments received were taken into consideration in the development of the alternatives.

ALTERNATIVES PUBLIC WORKSHOP #2 – NOVEMBER 9, 2017

A second Alternatives Public Workshop was held on November 9, 2017 at the University of Florida, Institute of Food and Agricultural Sciences (UF/IFAS) Extension, Southwest Florida Research and Education Center in Immokalee. Four alternatives were presented at this workshop: No-Build Alternative, Central Alternative #1 Revised, Central Alternative #2, and Central Alternative #2 Revised. This workshop was attended by 28 people. All attendees were given the opportunity to provide written comments at the workshop or within a 10-day comment period. A total of 16 comments were received during the meeting. Attendees were asked to rank the alternatives from one through four in order of preference, with one being their most preferred. Of the comments received, five people supported Central Alternative #1 Revised, five people supported Central Alternative #2, three people supported Central Alternative #2 Revised, three people supported the No Build Alternative, five people preferred a traffic signal, and five people preferred a roundabout. FDOT representatives were available at the workshop to answer questions and continued to utilize the previously stated accommodations to enhance public outreach efforts to the LEP populations within the SR 29 study area. All of the comments received were taken into consideration in the development of the alternatives.

After the workshop, the Conservancy of Southwest Florida and Collier Enterprises responded with comments. A letter signed by Alison Wescott was sent by Susan Scott of the Conservancy of Southwest Florida on November 20, 2017. The letter expressed support for the Central Alternative #1 Revised. An email was received from Pat Utter of Collier Enterprises on December 21, 2017 in support of Central Alternative #2 Revised. None of the letters ranked the additional alternatives. Besides the No-Build Alternative, Central Alternative #2 Revised was the least supported of the three Build Alternatives.

4.3 PUBLIC HEARING

A Public Hearing for this project was held from 5:00 p.m. to 7:00 p.m. on November 15, 2018 at CareerSource Southwest Florida (formerly Immokalee One-Stop Career Center) in Immokalee to present the viable Build Alternative (Central Alternative #2) along with the No-Build Alternative. The hearing began with an open house from 5 p.m. to 6 p.m. followed by a formal presentation (beginning at 6 p.m.), which included an audiovisual presentation. At the conclusion of the audiovisual presentation, members of the public had an opportunity to voice comments regarding the project that were documented in the official public hearing record. The Public Hearing Transcript Certification Package (February 2019), which includes the public hearing transcript, and all received oral public comments, was prepared under separate cover and is included in **Appendix O** and in the *Comments and Coordination Report*. FDOT representatives were available at the hearing to answer questions and continued to utilize the previously stated accommodations to enhance public outreach efforts to the LEP populations within the SR 29 study area. A total of

64 people signed-in at the Public Hearing including two local agency staff. Five persons spoke for the public record at the hearing, and FDOT received 20 written comment forms at the hearing, on the project website, by email, and regular mail during the 10-day comment period ending on November 26, 2018 that followed the hearing. Four comments were received in support of Central Alternative #2 with only one comment opposing. The majority of the comments received (14) were relative to the bicycle and pedestrian accommodations along the corridor: one was pleased with the proposed pedestrian and bicycle facilities, one was opposed to the proposed facilities believing they will create an unnecessary safety issue, and the remaining 12 comments addressed additional or different bicycle and/or pedestrian facilities at various locations along the corridor. Eleven comments were received concerning the proposed roundabout at SR 29 and Westclox Street/New Market Road W: two comments were in support of the proposed roundabout, four were in opposition to the proposed roundabout, and the remaining five expressed concern about pedestrian safety at the intersection and/or the need for immediate improvements at the intersection. All comments received have been taken into consideration and responses have been provided which are included in the *Comments and Coordination Report*, prepared under separate cover.

4.4 PROJECT UPDATE: FDOT OFFICE HOUR EVENTS

Subsequent to the Public Hearing, design refinements were made to the PD&E Study Preferred Alternative to meet the FDM requirements and included the identification of proposed SFMs necessary to accommodate stormwater runoff from CR 846 to SR 82. To inform the community and answer questions about the design refinements and associated proposed SMFs, the FDOT hosted two Project Update: FDOT Office Hour Events (an in-person event and a live online event). Citizens were informed of the events through a newsletter that was mailed and emailed, a press release, a Florida Administrative Register notification, a display ad in the Immokalee Bulletin, social media posts, and flyers displayed at eight venues around Immokalee frequented by the community (such as restaurants, retail establishments, community centers, etc.).

The in-person office hours event took place from 12:00 p.m. to 4:00 p.m. at the Collier County Public Library-Immokalee Branch Library on April 18, 2024. A total of 32 people attended the event. A board displaying the Preferred Alternative along with concept plan sheets of the Preferred Alternative were available for viewing. An audiovisual presentation (approximately seven minutes in length) was pre-loaded on iPads and additionally available for viewing. The presentation provided an overview of the project, briefly explained the design refinements, outlined previous public engagement, and discussed next steps in the FDOT project delivery process. All attendees were given the opportunity to provide written comments at the in-person office hours event or to submit comments through mail, email, or the project website by May 3, 2024. A total of ten comments were received from attendees at the event.

The live online office hour event occurred from 6:00 p.m. to 7:00 p.m. via the GoTo Webinar platform on April 23, 2024. The event opened with the audiovisual presentation, as described above, beginning shortly after 6:00 p.m. At the conclusion of the audiovisual presentation, members of the public had an opportunity to ask questions and voice comments. Once all participants wishing to comment or ask a question were addressed by FDOT staff, the audiovisual presentation was played a second time followed by a question and comment period. This cycle

continued until the office hour event ended at 7:00 p.m. A total of 22 people participated in the live online office hour event. A total of two comments were received from attendees at the event. All attendees were given the opportunity to submit comments during the live online office hour event or through mail, email, or the project website by May 3, 2024.

Comments received outside of the office hours included three provided via the project website and six provided via email. Two additional comments were submitted via email after May 3, 2024. Questions and comments generally cited during the two events and received through the project website and email pertained to access, safety, proximity of the new roadway to existing and planned development, concept plans, drainage, schedule, and correct project contacts.

Bilingual (English and Spanish) staff were present/available upon request for translation services at both office hour events given the large number of Spanish speaking individuals present within the project study area. Details regarding the office hour events, as well as questions and comments received during the office hour events and comment period, are documented in the *Comments and Coordination Report Addendum*, prepared under separate cover.

4.5 LOCATION DESIGN AND CONCEPT ACCEPTANCE

A final project notification will be sent to property owners, business owners, and residences within 300 feet of the project as well as public officials; federal, state, and local agencies; stakeholders; and interested citizens and parties to announce the final approval (i.e., Location and Design Concept Acceptance) of this environmental document. This notification will also be published in the Naples Daily News. In addition, this notification and the final project documents will be made available on the project website.

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SECTION 5.0

COMMITMENTS

The FDOT is committed to the following measures to minimize impacts to the human and natural environment:

- The most recent version of the FWS’ *Standard Protection Measures for the Eastern Indigo Snake* will be adhered to during the construction of the proposed project.
- The FDOT will follow the FDOT *Supplemental Standard Specification 7-1.4.1 Additional Requirements for the Florida Black Bear* to minimize human-bear interactions associated with construction sites during project construction.
- To comply with Section 7 of the ESA, as amended, the FDOT will re-initiate consultation during design and permitting for the following species: Florida scrub-jay, Florida panther, Florida bonneted bat, and Audubon’s crested caracara. The FDOT will provide additional information, as needed, that will allow the FWS to complete their analysis of the project’s effects on documented species and complete Section 7 ESA consultation for the project.
- The FDOT will implement best management practices consistent with the FDOT Conservation Plan for the Florida Panther.
- FDOT will construct the wildlife crossing between Oil Well Road and CR 846. This crossing was listed at the 2024 annual prioritization meeting (held January 17, 2024) of the FDOT Conservation Plan for the Florida Panther to determine priority for available funding. As part of the preferred recommendation, directional fencing associated with the proposed crossing would be consistent with the Florida Panther Conservation Plan and, as appropriate, the Wildlife Crossing Memorandum (June 2022).
- To mitigate at a ratio of two acres per one acre of impact for the loss of 52.14 total acres of occupied Florida scrub-jay territory on the Collier property (private property) and a ratio of four acres per one acre of impact for the loss of 15.75 acres of the Immokalee Regional Airport Upland Management Area (UMA). FDOT will provide a total of 167.28 acres of occupied scrub-jay habitat (104.28 acres associated with the loss of two scrub-jay territories within the Collier property + an additional 63 acres associated with potential habitat loss within the UMA = 167.28 acres) as a conservation measure to compensate for the loss of scrub-jay habitat resulting from the project.
- The FDOT will contribute \$10,000 to the FWS Florida Bonneted Bat Fund.
- Audubon’s crested caracara conservation measures will be implemented. Land clearing activities for the project will be conducted outside of the caracara nesting season (December 1 through April 30) to the greatest extent practicable. Since caracara nesting season is from December 1 through April 30, clearing should be completed between May 1 and November 30. Should it be necessary to conduct land clearing activities within the nesting season, the FDOT or their designated agent will survey suitable caracara nesting habitat to determine if an active nest occurs within or adjacent to the project area. If an active nesting is observed within

300 meters (985 feet) of the project area, land clearing within 300 meters (985 feet) of the nest will not occur until monitoring has determined that the nest has either been abandoned, or chicks within the nest have fledged and left the nest site.

- The FDOT will complete a cumulative effects analysis for impacts to threatened and endangered species.
- Based on coordination with the FWC, the FDOT will provide compensatory land acquisition for the determined required use of the FWC-held Immokalee Regional Airport Upland Management Area (UMA).
- A land use review will be conducted during the design phase to identify noise sensitive sites that may have received a building permit subsequent to the noise study but prior to the Date of Public Knowledge (i.e., the date that the environmental document has been approved by the FDOT Office of Environmental Management). If the review identifies noise sensitive sites that have been permitted prior to the Date of Public Knowledge, then those sensitive sites will be evaluated for traffic noise and abatement considerations.
- Given the proposed use of property owned by the Immokalee Regional Airport and proximity to a runway threshold, the FDOT will continue to coordinate with Collier County and the FAA throughout future project phases. This may include, but not necessarily be limited to, evaluating Runway 36 Protection Zone (RPZ) compatibility for the CR 846 improvements; airfield security fence relocation; evaluation of potential airspace obstructions in proximity to Runway 36 (e.g., new or relocated light and utility poles); and the release of federally-obligated land for use as public road right-of-way.
- The FDOT will coordinate with Collier County and the FAA during future project phases in order to incorporate hazardous wildlife control measures recommended in *FAA Advisory Circular 150/5200-33B, Hazardous Wildlife Attractants On or Near Airports*.

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APPENDICES

Pre-Public Hearing

APPENDIX A

Planning Consistency Documentation



COLLIER MPO

2045

LONG RANGE TRANSPORTATION PLAN

Approved by the Collier MPO Board
December 11, 2020

DECEMBER 2020



Tables

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Table 6-1. Collier MPO FY 2021 - FY 2025 TIP Summary

(in millions \$)

Facility	Limits From	Limits To	Description	Lead Agency	Financial Project Number (FPN)	Total TIP Funding 2021-2025 (YOE)	Plan Period 1 (TIP): 2021-2025		
							PRE-ENG	ROW	CST
PLAN PERIOD 1 TRANSPORTATION IMPROVEMENT PROJECTS (TIP)									
HIGHWAY PROJECTS									
SR 29	Oil Well Rd.	Sunniland Nursery Rd.	Add Lanes and Reconstruct	FDOT	4175402	\$8.33	\$8.33		
SR 29	Sunniland Nursery Rd.	S. of Agricultural Way	Widen from 2-Lanes to 4-Lanes	FDOT	4175403	\$0.50	\$0.50		
SR 29	S. of Agricultural Way	CR 846 E.	Add lanes and Reconstruct	FDOT	4175404	\$0.27	\$0.27		
SR 29	CR 846 E.	New Markey Rd. N.	New Road Construction	FDOT	4175405	\$6.74	\$0.06	\$6.68	
SR 29	N. of New Market Rd.	SR 82	Add Lanes and Reconstruct	FDOT	4175406	\$1.47	\$0.38	\$1.09	
SR 29	SR 82	Hendry County Line	Add Lanes and Reconstruct	FDOT	4178784	\$1.36	\$0.07	\$1.30	
I-75	SR 951		Ultimate Interchange Improvement	FDOT	4258432	\$104.19	\$1.02	\$6.90	\$96.27
SR 82	Hendry C/L	Gator Slough Ln.	Add Lanes and Reconstruct	FDOT	4308481	\$44.73	\$0.07	\$2.12	\$42.54
SR 951	Manatee Rd.	N. of Tower Rd.	Add Lanes and Reconstruct	FDOT	4351112	\$17.34		\$1.96	\$15.39
Airport Pulling Rd.	Vanderbilt Beach Rd.	Immokalee Rd.	Add Thru Lanes	Collier	4404411	\$12.86	\$3.00		\$9.86
SR 90 (US 41)	at Oasis Visitor Center		Add Left Turn Lane(s)	FDOT	4419751	\$0.58	\$0.05		\$0.54
I-75	Pine Ridge Rd.		Interchange Improvement	FDOT	4452962	\$5.45			\$5.45
Corkscrew Rd. N.	S. of Wildcat Dr.	E. of Wildcat Dr.	Widen/Resurface	Collier	4463231	\$1.48			\$1.48
Corkscrew Rd. S.	Lee County Curve	Collier County Curve	Widen/Resurface	Collier	4463232	\$1.32			\$1.32
Vanderbilt Beach Rd.	US 41	E. of Goodlette-Frank Rd.	Add Lanes and Reconstruct	Collier	4463381	\$8.43			\$8.43
Goodlette Frank Rd.	Vanderbilt Beach Rd.	Immokalee Rd.	Add Lanes and Reconstruct	Collier	4463411	\$5.50			\$5.50
CR 951 (Collier Blvd.)	Golden Gate Canal	Green Blvd.	Widen/Resurface	Collier	4464121	\$3.20	\$3.20		
BRIDGE PROJECTS									
16th St. Bridge N.E.	Golden Gate Blvd.	Randall Blvd.	New Bridge Construction	Collier	4318953	\$4.93			\$4.93
SR 951	Over Big Marco Pass		Bridge Repair/Rehabilitation	FDOT	4348571	\$1.68			\$1.68
Scour Countermeasure	Various Locations		Bridge Repair/Rehabilitation	FDOT	4350431	\$1.89	\$0.20		\$1.69
CR 846	Over Drainage Canal		Bridge Replacement	FDOT	4441851	\$2.61	\$0.05		\$2.56
CONGESTION MANAGEMENT SYSTEMS/INTELLIGENT TRANSPORTATION SYSTEMS (CMS/ITS) PROJECTS									
Bicycle Detection	City of Naples		ITS Surveillance System	Naples	4462531	\$0.07			\$0.07
TMC Operations Funding	Collier County		Other ITS	Collier	4371031	\$0.32			\$0.32
Traffic Signal Timing Optimization	Various Locations		Traffic Signal Update	Collier	4404351	\$0.40	\$0.35		\$0.05
TMC Operations Funding	City of Naples		Other ITS	Naples	4371041	\$0.12			\$0.12
Traffic Signal Reimbursement	City of Naples		Traffic Signals	Naples	4136271	\$0.68			\$0.68
Signal Timing County Roads	Various Locations		Traffic Signal Update	Collier	4379251	\$0.45			\$0.45
Signal Timing US 41	SR 951 (Collier Blvd.)	Old US 41	Traffic Signal Update	Collier	4379261	\$0.52			\$0.52
Travel Time Data Collection	Collier County		Other ITS	Collier	4379241	\$0.44			\$0.44
Collier MPO Identified Operational Improvements Funding			Traffic Ops. Improvements	FDOT	4051061	\$7.15			\$7.15
Traffic Signals Reimbursement	Collier County		Traffic Signals	Collier	4126661	\$1.73			\$1.73
Fiber Optic & FPL	Collier County		ITS Communication System	Collier	4462501	\$0.27			\$0.27
Travel Time Data	Collier County		ITS Communication System	Collier	4462511	\$0.70			\$0.70
School Flasher ITS	Collier County		ITS Surveillance System	Collier	4462521	\$0.35			\$0.35
Vehicle Count Stations ITS	Collier County		Traffic Control Devices/Systems	Collier	4462541	\$0.31			\$0.31
Traffic Control ITS	Collier County		Traffic Control Devices/Systems	Collier	4463421	\$0.89	\$0.12		\$0.78
Harbour Dr.	at Crayton Rd.		Roundabout	Naples	4463171	\$0.89			\$0.89
Mooring Line Dr.	Crayton Rd.		Roundabout	Naples	4463172	\$0.13	\$0.13		
US 41	Golden Gate Parkway		Intersection Improvement	FDOT	4464511	\$0.50	\$0.27	\$0.23	



COLLIER METROPOLITAN PLANNING ORGANIZATION

TRANSPORTATION IMPROVEMENT PROGRAM

FY2024 - FY2028

Adopted June 9, 2023



The preparation of this report has been financed in part through grants from the Federal Highway Administration and Federal Transit Administration, U.S. Department of Transportation, under the Metropolitan Planning Program, Sections 134 and 135 of Title 23 U.S. Code. The contents of this report do not necessarily reflect the official views or policy of the U.S. Department of Transportation.

COLLIER MPO FY 2024 - 2028 TIP



417540-2	SR 29 FROM OIL WELL ROAD TO SUNNILAND NURSERY ROAD
Project Description	Widen from 2 lanes to 4, segment of larger project
Type of Work Description	ADD LANES & RECONSTRUCT
Responsible Agency	MANAGED BY FDOT
Project Length	4.762
SIS	Yes
2045 LRTP	P6-2, Table 6-1

<u>Fund</u>	<u>Phase</u>	<u>2024</u>	<u>2025</u>	<u>2026</u>	<u>2027</u>	<u>2028</u>	<u>Totals</u>
ACNP	PE	\$1,300,000.00	\$0.00	\$0.00	\$0.00	\$0.00	\$1,300,000.00
DI	PE	\$6,140,000.00	\$0.00	\$0.00	\$0.00	\$0.00	\$6,140,000.00
		\$7,440,000.00	\$0.00	\$0.00	\$0.00	\$0.00	\$7,440,000.00

COLLIER MPO FY 2024 - 2028 TIP



417540-5	SR 29 FROM CR 846 E TO N OF NEW MARKET ROAD W
Project Description	Immokalee Loop Rd, Freight Priority
Type of Work Description	NEW ROAD CONSTRUCTION
Responsible Agency	MANAGED BY FDOT
Project Length	3.484
SIS	Yes
2045 LRTP	P6-2, Table 6-1

<u>Fund</u>	<u>Phase</u>	<u>2024</u>	<u>2025</u>	<u>2026</u>	<u>2027</u>	<u>2028</u>	<u>Totals</u>
TALT	ENV	\$250,000.00	\$60,000.00	\$0.00	\$0.00	\$0.00	\$310,000.00
ACNP	ROW	\$250,950.00	\$6,541,994.00	\$0.00	\$0.00	\$0.00	\$6,792,944.00
BNIR	ROW	\$98,543.00	\$521,563.00	\$0.00	\$0.00	\$0.00	\$620,106.00
		\$599,493.00	\$7,123,557.00	\$0.00	\$0.00	\$0.00	\$7,723,050.00

COLLIER MPO FY 2024 - 2028 TIP



417540-6	SR 29 FROM N OF NEW MARKET RD TO SR 82
Project Description	Widen from 2 lanes to 4, segment of larger project, Freight Priority
Type of Work Description	ADD LANES & RECONSTRUCT
Responsible Agency	MANAGED BY FDOT
Project Length	2.991
SIS	Yes
2045 LRTP	P6-2, Table 6-1

<u>Fund</u>	<u>Phase</u>	<u>2024</u>	<u>2025</u>	<u>2026</u>	<u>2027</u>	<u>2028</u>	<u>Totals</u>
ACNP	ROW	\$0.00	\$318,956.00	\$0.00	\$0.00	\$0.00	\$318,956.00
TALT	ENV	\$0.00	\$75,000.00	\$225,000.00	\$0.00	\$0.00	\$300,000.00
DI	CST	\$0.00	\$0.00	\$0.00	\$32,128,568.00	\$0.00	\$32,128,568.00
ACNP	CST	\$0.00	\$0.00	\$0.00	\$4,504,002.00	\$0.00	\$4,504,002.00
DI	RRU	\$0.00	\$576,000.00	\$0.00	\$0.00	\$0.00	\$576,000.00
DI	ROW	\$0.00	\$803,000.00	\$1,253,897.00	\$0.00	\$0.00	\$2,056,897.00
		\$0.00	\$1,772,956.00	\$1,478,897.00	\$36,632,570.00	\$0.00	\$39,884,423.00



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Web Application

Federal Aid Management David Williams - Manager

STIP Project Detail and Summaries Online Report

**** Repayment Phases are not included in the Totals ****

Selection Criteria	
Current STIP Financial Project: 417540 As Of: 3/3/2024	Detail Related Items Shown

HIGHWAYS							
Item Number: 417540 1		Project Description: SR 29 FROM OIL WELL ROAD TO SR 82				*SIS*	
District: 01	County: COLLIER	Type of Work: PD&E/EMO STUDY			Project Length: 16.961MI		
		Fiscal Year					
Phase / Responsible Agency	<2024	2024	2025	2026	2027	>2027	All Years
P D & E / MANAGED BY FDOT							
Fund Code:	DDR-DISTRICT DEDICATED REVENUE	54,015					54,015
	DIH-STATE IN-HOUSE PRODUCT SUPPORT	37,618					37,618
	DS-STATE PRIMARY HIGHWAYS & PTO	135,799					135,799
	SU-STP, URBAN AREAS > 200K	4,975,503	51,165				5,026,668
	Phase: P D & E Totals	5,202,935	51,165				5,254,100
	Item: 417540 1 Totals	5,202,935	51,165				5,254,100
<hr/>							
Item Number: 417540 2		Project Description: SR 29 FROM OIL WELL ROAD TO SUNNILAND NURSERY ROAD				*SIS*	
District: 01	County: COLLIER	Type of Work: ADD LANES & RECONSTRUCT			Project Length: 4.762MI		
		Fiscal Year					
Phase / Responsible Agency	<2024	2024	2025	2026	2027	>2027	All Years
PRELIMINARY ENGINEERING / MANAGED BY FDOT							

Fund Code:	ACNP-ADVANCE CONSTRUCTION NHPP		1,300,000						1,300,000
	DI-ST. - S/W INTER/INTRASTATE HWY		6,140,000						6,140,000
	Phase: PRELIMINARY ENGINEERING Totals		7,440,000						7,440,000
	Item: 417540 2 Totals		7,440,000						7,440,000

Item Number: 417540 3 **Project Description:** SR 29 FROM SUNNILAND NURSERY ROAD TO S OF AGRICULTURE WAY *SIS*
District: 01 **County:** COLLIER **Type of Work:** ADD LANES & RECONSTRUCT **Project Length:** 2.548MI

		Fiscal Year						
Phase / Responsible Agency		<2024	2024	2025	2026	2027	>2027	All Years
PRELIMINARY ENGINEERING / MANAGED BY FDOT								
Fund Code:	ACSA-ADVANCE CONSTRUCTION (SA)	2,780,406						2,780,406
	GFSU-GF STPBG ANY AREA	609,339						609,339
	GFSU-GF STPBG >200 (URBAN)	313,131						313,131
	SA-STP, ANY AREA	1,572,987	9,177					1,582,164
	SU-STP, URBAN AREAS > 200K	686,869						686,869
	Phase: PRELIMINARY ENGINEERING Totals	5,962,732	9,177					5,971,909
	Item: 417540 3 Totals	5,962,732	9,177					5,971,909

Item Number: 417540 4 **Project Description:** SR 29 FROM S OF AGRICULTURE WAY TO CR 846 E *SIS*
District: 01 **County:** COLLIER **Type of Work:** ADD LANES & RECONSTRUCT **Project Length:** 2.251MI

		Fiscal Year						
Phase / Responsible Agency		<2024	2024	2025	2026	2027	>2027	All Years
PRELIMINARY ENGINEERING / MANAGED BY FDOT								
Fund Code:	ACSA-ADVANCE CONSTRUCTION (SA)	1,984,290						1,984,290
	DDR-DISTRICT DEDICATED REVENUE	3,372						3,372
	DS-STATE PRIMARY HIGHWAYS & PTO	2,430						2,430
	GFSU-GF STPBG >200 (URBAN)	833,449						833,449
	SA-STP, ANY AREA	1,187,240	7,570					1,194,810
	Phase: PRELIMINARY ENGINEERING Totals	4,010,781	7,570					4,018,351
	Item: 417540 4 Totals	4,010,781	7,570					4,018,351

Item Number: 417540 5 **Project Description:** SR 29 FROM CR 846 E TO N OF NEW MARKET ROAD W *SIS*
District: 01 **County:** COLLIER **Type of Work:** NEW ROAD CONSTRUCTION **Project Length:** 3.484MI

		Fiscal Year						
Phase / Responsible Agency		<2024	2024	2025	2026	2027	>2027	All Years
PRELIMINARY ENGINEERING / MANAGED BY FDOT								
Fund Code:	DDR-DISTRICT DEDICATED REVENUE	6,003,461						6,003,461
	DIH-STATE IN-HOUSE PRODUCT SUPPORT	57,324	10,000					67,324
	DS-STATE PRIMARY HIGHWAYS & PTO	1,226						1,226

	FINC-FINANCING CORP		550,000						550,000
Phase: PRELIMINARY ENGINEERING Totals		6,062,011	560,000						6,622,011
RIGHT OF WAY / MANAGED BY FDOT									
Fund Code:	ACNP-ADVANCE CONSTRUCTION NHPP		250,950	6,541,994					6,792,944
	BNIR-INTRASTATE R/W & BRIDGE BONDS		98,543	521,563					620,106
	FINC-FINANCING CORP		823,285						823,285
Phase: RIGHT OF WAY Totals			1,172,778	7,063,557					8,236,335
ENVIRONMENTAL / MANAGED BY FDOT									
Fund Code:	ACSA-ADVANCE CONSTRUCTION (SA)		250,000						250,000
	FINC-FINANCING CORP		520,000						520,000
	TALT-TRANSPORTATION ALTS- ANY AREA			60,000					60,000
Phase: ENVIRONMENTAL Totals			770,000	60,000					830,000
Item: 417540 5 Totals		6,062,011	2,502,778	7,123,557					15,688,346
<p>Item Number: 417540 6 Project Description: SR 29 FROM N OF NEW MARKET RD TO SR 82 *SIS*</p> <p>District: 01 County: COLLIER Type of Work: ADD LANES & RECONSTRUCT Project Length: 2.991MI</p>									
		Fiscal Year							
Phase / Responsible Agency		<2024	2024	2025	2026	2027	>2027	All Years	
PRELIMINARY ENGINEERING / MANAGED BY FDOT									
Fund Code:	ACSA-ADVANCE CONSTRUCTION (SA)	393,677	37,537						431,214
	CM-CONGESTION MITIGATION - AQ	522,705							522,705
	DDR-DISTRICT DEDICATED REVENUE	8,984							8,984
	FINC-FINANCING CORP		4,560,000						4,560,000
	REPE-REPURPOSED FEDERAL EARMARKS	3,656,698							3,656,698
Phase: PRELIMINARY ENGINEERING Totals		4,582,064	4,597,537						9,179,601
RIGHT OF WAY / MANAGED BY FDOT									
Fund Code:	DI-ST. - S/W INTER/INTRASTATE HWY			803,000	1,253,897				2,056,897
	DIH-STATE IN-HOUSE PRODUCT SUPPORT			72,000					72,000
	FINC-FINANCING CORP		247,956						247,956
Phase: RIGHT OF WAY Totals			247,956	875,000	1,253,897				2,376,853
RAILROAD & UTILITIES / MANAGED BY FDOT									
Fund Code:	DI-ST. - S/W INTER/INTRASTATE HWY			576,000					576,000
CONSTRUCTION / MANAGED BY FDOT									
Fund Code:	ACNP-ADVANCE CONSTRUCTION NHPP					4,219,731			4,219,731
	DDR-DISTRICT DEDICATED REVENUE	5,733							5,733
	DI-ST. - S/W INTER/INTRASTATE HWY					38,819,614			38,819,614
	DIH-STATE IN-HOUSE PRODUCT SUPPORT					163,950			163,950

DS-STATE PRIMARY HIGHWAYS & PTO		10					10
Phase: CONSTRUCTION Totals		5,743			43,203,295		43,209,038
ENVIRONMENTAL / MANAGED BY FDOT							
Fund Code:	FINC-FINANCING CORP		75,000				75,000
	TALT-TRANSPORTATION ALTS- ANY AREA		380,000	75,000	225,000		680,000
Phase: ENVIRONMENTAL Totals			455,000	75,000	225,000		755,000
Item: 417540 6 Totals		4,587,807	5,300,493	1,526,000	1,478,897	43,203,295	56,096,492
Project Totals		25,826,266	15,311,183	8,649,557	1,478,897	43,203,295	94,469,198
Grand Total		25,826,266	15,311,183	8,649,557	1,478,897	43,203,295	94,469,198

This site is maintained by the Office of Work Program and Budget, located at 605 Suwannee Street, MS 21, Tallahassee, Florida 32399.

For additional information please e-mail questions or comments to:
Federal Aid Management

David Williams: David.Williams@dot.state.fl.us Or call 850-414-4449

Or

Denise Strickland: Denise.Strickland@dot.state.fl.us Or call 850-414-4491

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Consistent, Predictable, Repeatable

APPENDIX B

Central Alternative #2 Typical Sections

CENTRAL ALTERNATIVE #2 TYPICAL SECTIONS

Central Alternative #2 has been selected as the Preferred Alternative. It follows the existing alignment of SR 29 from the start of the project at Oil Well Road to north of Seminole Crossing Trail. From this point, the bypass portion of the Central Alternative #2 travels north from SR 29 on new alignment along the west side of the Immokalee Regional Airport to avoid the commercial/industrial areas of Immokalee and the State Farmers Market to the west. The bypass portion of Central Alternative #2 then turns to the northwest just past Gopher Ridge Road to parallel Madison Avenue and New Market Road. It then travels along the east side of Collier Health Services Medical Center and the Florida State University College of Medicine before reconnecting to SR 29 north of Westclox Street/New Market Road W. Finally, Central Alternative #2 travels from north of Westclox Street/New Market Road W to the project terminus near SR 82. A partial two-lane roundabout is proposed at SR 29 and Westclox Street/New Market Road W.

1.1 Typical Sections

1.1.1 SR 29

Within the project limits, SR 29 has been divided into the following six typical sections:

From Oil Well Road to South of Kaicasa Entrance

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 40-foot median). There is an open drainage system, and the design speed is 65 mph.

The existing right-of-way (ROW) varies from 173.75 feet to 181 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.1** depicts this typical section.

From South of Kaicasa Entrance to North of Seminole Crossing Trail

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 30-foot median), with a 10-foot shared use path on the west side of the corridor from Farm Worker Way to Seminole Crossing Trail. There is an open drainage system, and the design speed is 55 mph.

The existing ROW varies from 173.75 feet to 181 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits, except for the canal relocation near Seminole Crossing Trail. **Figure 1.2** depicts this typical section.

Figure 1.1
SR 29 Typical Section from Oil Well Road to South of Kaicasa Entrance

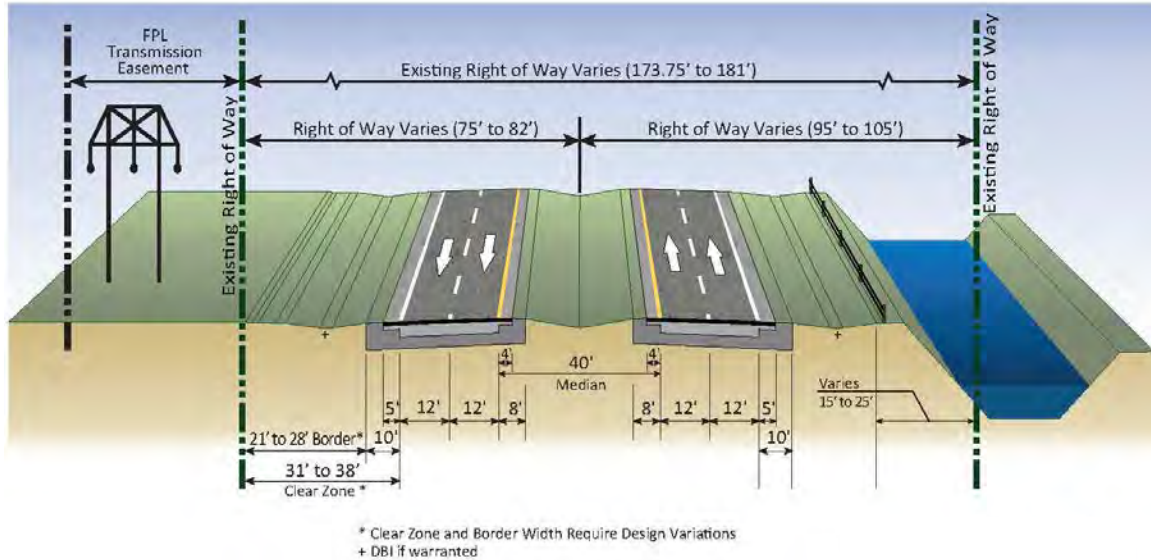
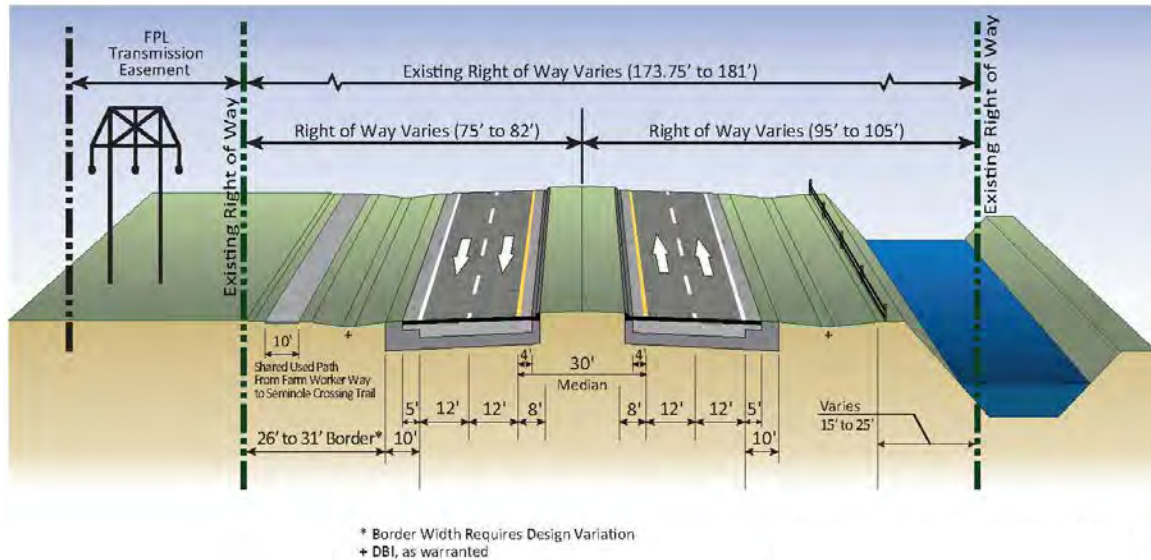


Figure 1.2
SR 29 Typical Section from South of Kaicasa Entrance to North of Seminole Crossing Trail

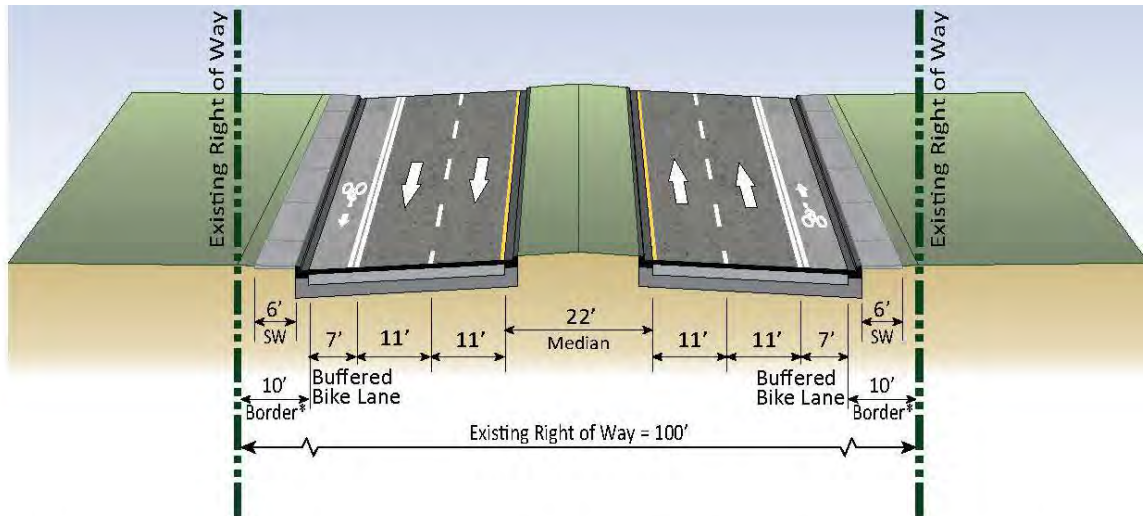


From North of Seminole Crossing Trail to CR 846

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 11-foot lanes in each direction and a 22-foot median), with 7-foot buffered bicycle lanes and 6-foot sidewalks in each direction. There is a closed drainage system with curb and gutter, and the design speed is 45 mph.

The existing ROW is 100 feet. The ROW width needed for this typical section can mostly be accommodated within the existing ROW limits, except for some additional ROW needed for a turn lane near 13th Street. **Figure 1.3** depicts this typical section.

Figure 1.3
SR 29 Typical Section from North of Seminole Crossing Trail to CR 846



* 10' Border Width Requires Design Variation Where Constrained by 100' Existing ROW

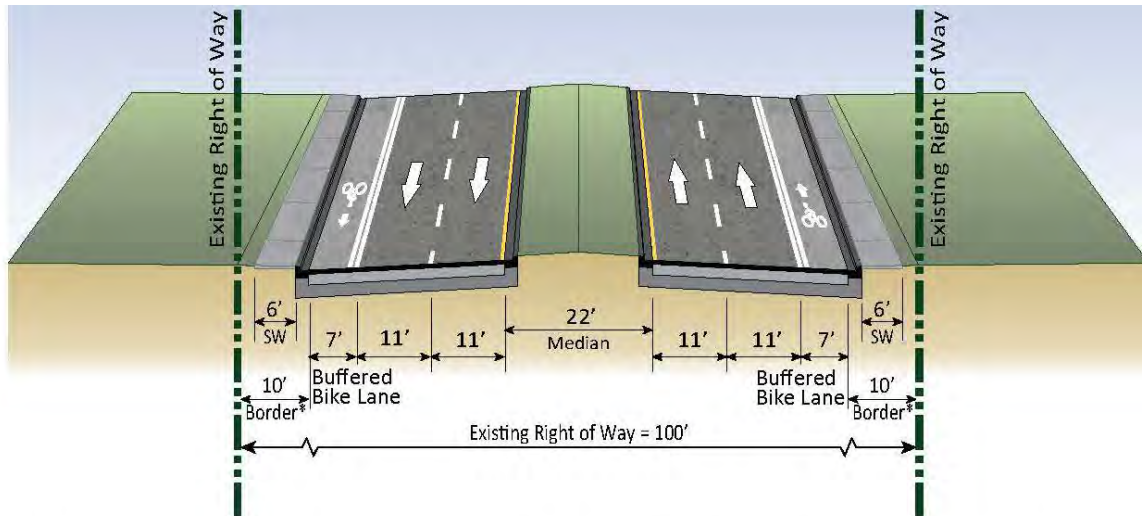
From North of Westclox Street to the SR 29 Bypass Junction

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 30-foot median), with a 10-foot shared use path on the west side of the corridor. There is an open drainage system, and the design speed will be 50 mph when the SR 29 Bypass is constructed.

The existing ROW is 200 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.4** depicts this typical section.

The existing ROW is 100 feet. The ROW width needed for this typical section can mostly be accommodated within the existing ROW limits, except for some additional ROW needed for a turn lane near 13th Street. **Figure 1.3** depicts this typical section.

Figure 1.3
SR 29 Typical Section from North of Seminole Crossing Trail to CR 846



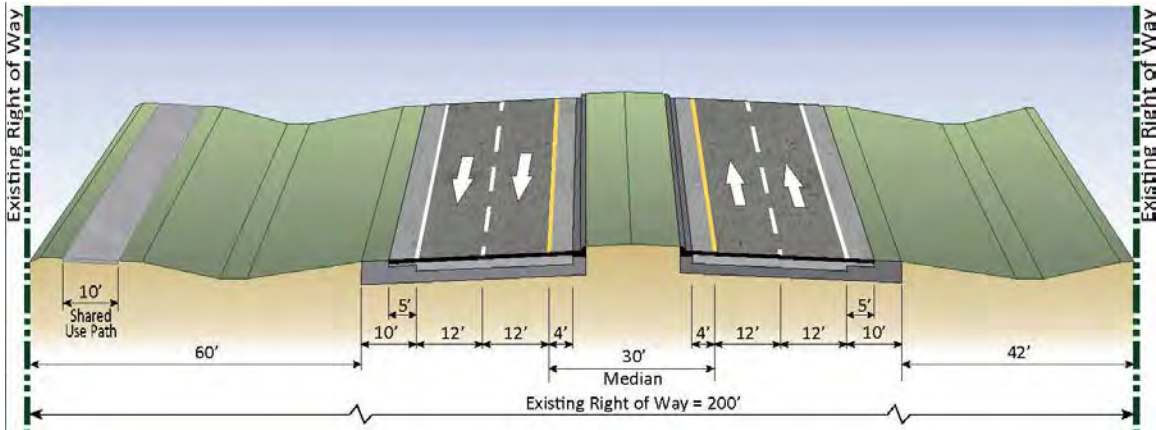
* 10' Border Width Requires Design Variation Where Constrained by 100' Existing ROW

From North of Westclox Street to the SR 29 Bypass Junction

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 30-foot median), with a 10-foot shared use path on the west side of the corridor. There is an open drainage system, and the design speed will be 50 mph when the SR 29 Bypass is constructed.

The existing ROW is 200 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.4** depicts this typical section.

Figure 1.4
SR 29 Typical Section from North of Westclox Street to the SR 29 Bypass Junction

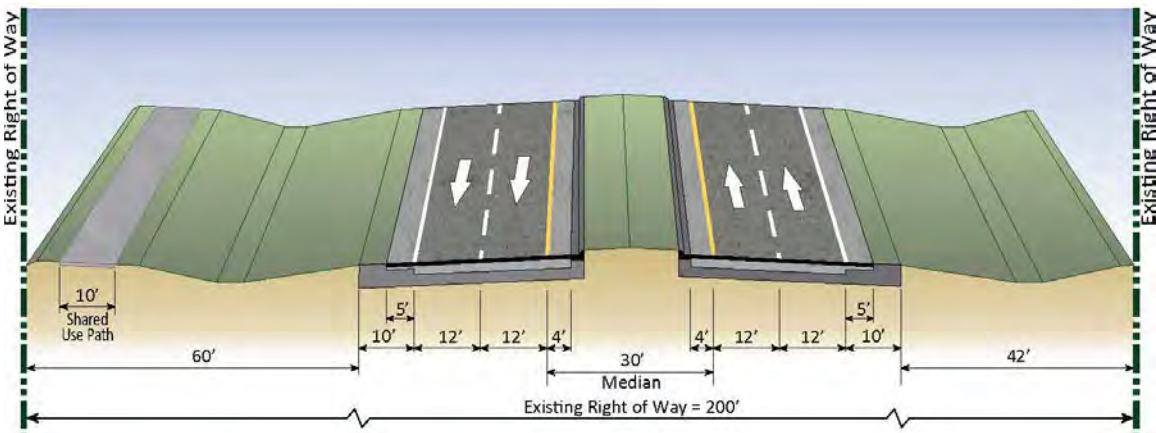


From the SR 29 Bypass Junction to Experimental Road

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 30-foot median), with a 10-foot shared use path on the west side of the corridor. There is an open drainage system, and the design speed is 55 mph.

The existing ROW is 200 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.5** depicts this typical section.

Figure 1.5
SR 29 Typical Section from the SR 29 Bypass Junction to Experimental Road

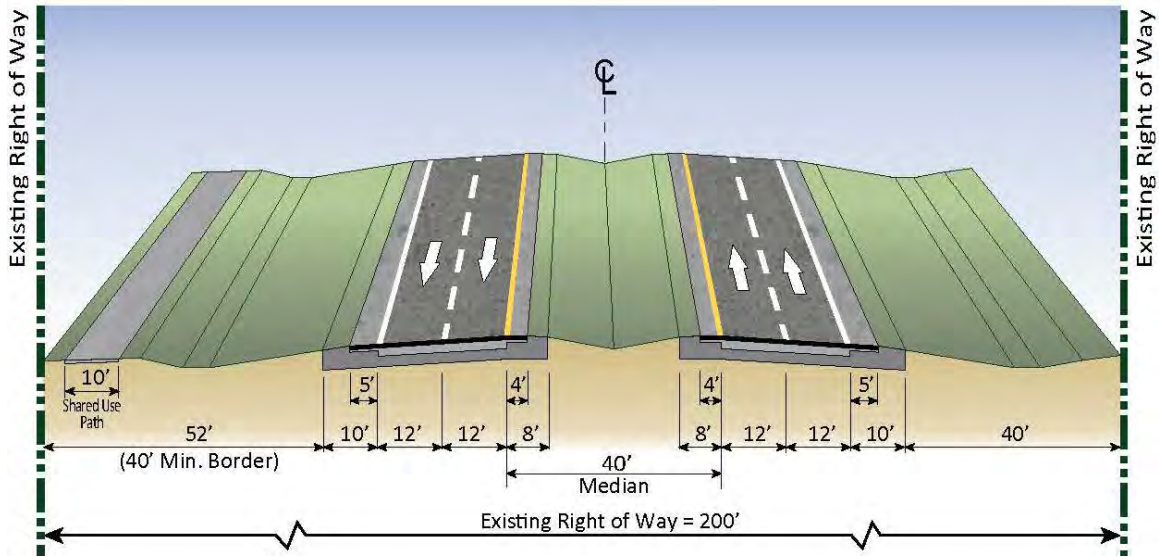


From Experimental Road to South of SR 82

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 40-foot median), with a 10-foot shared use path on the west side of the corridor. There is an open drainage system, and the design speed is 60 mph.

The existing ROW is 200 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.6** depicts this typical section.

Figure 1.6
SR 29 Typical Section from Experimental Road to South of SR 82



1.1.2 SR 29 Bypass Portion

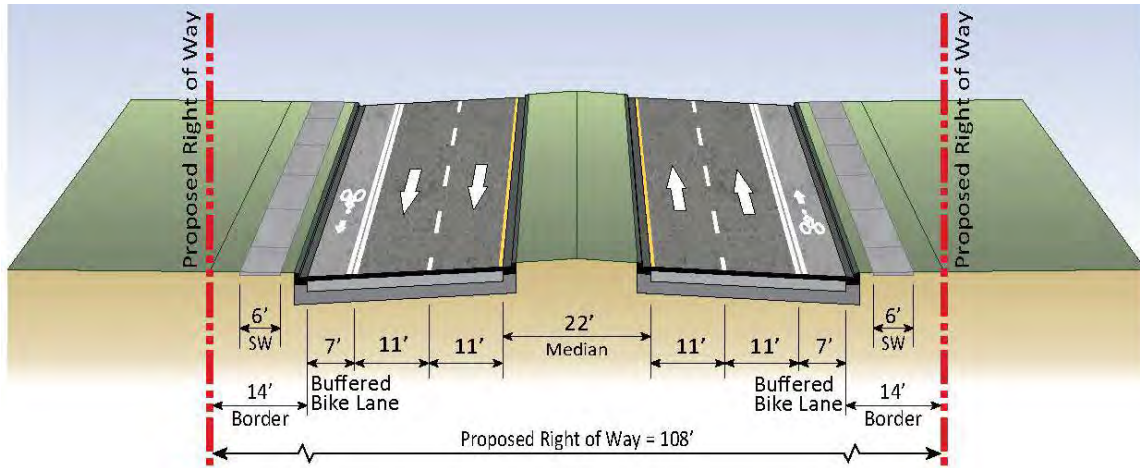
Within the project limits, the proposed SR 29 Bypass portion of Central Alternative #2 from CR 846 to the Bypass Junction with SR 29 north of Westclox Street/New Market Road W can be divided into the following two typical sections:

From CR 846 to Gopher Ridge Road

A 4-lane divided typical section (two (2) 11-foot travel lanes in each direction and a 22-foot median) is proposed, with 7-foot buffered bicycle lanes and 6-foot sidewalks in each direction. There is a closed drainage system with curb and gutter, and the design speed is 45 mph.

The ROW width needed for this typical section is 108 feet. **Figure 1.7** depicts this typical section.

Figure 1.7
SR 29 Bypass Typical Section from CR 846 to Gopher Ridge Road

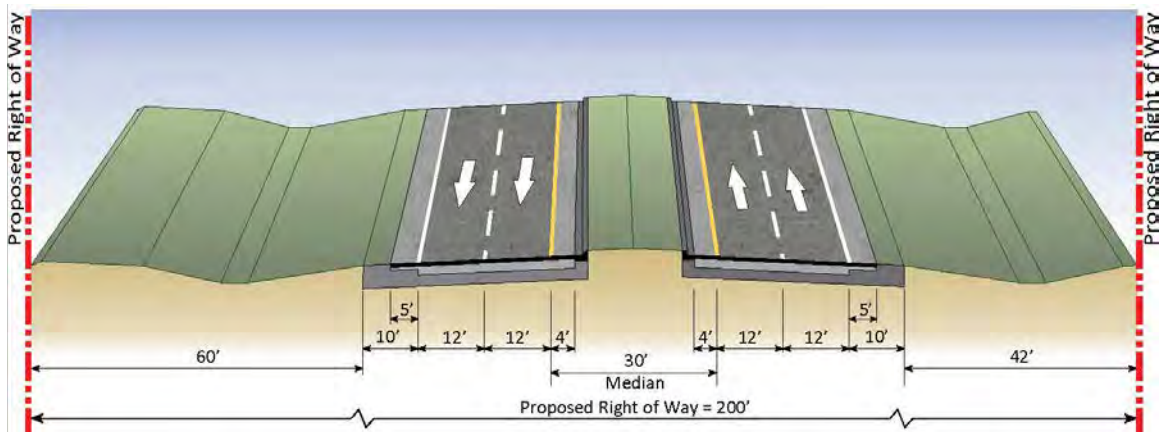


From Gopher Ridge Road to SR 29 Bypass Junction

A 4-lane divided typical section (two (2) 12-foot travel lanes in each direction and a 30-foot median) is proposed. There is an open drainage system, and the design speed is 50 mph.

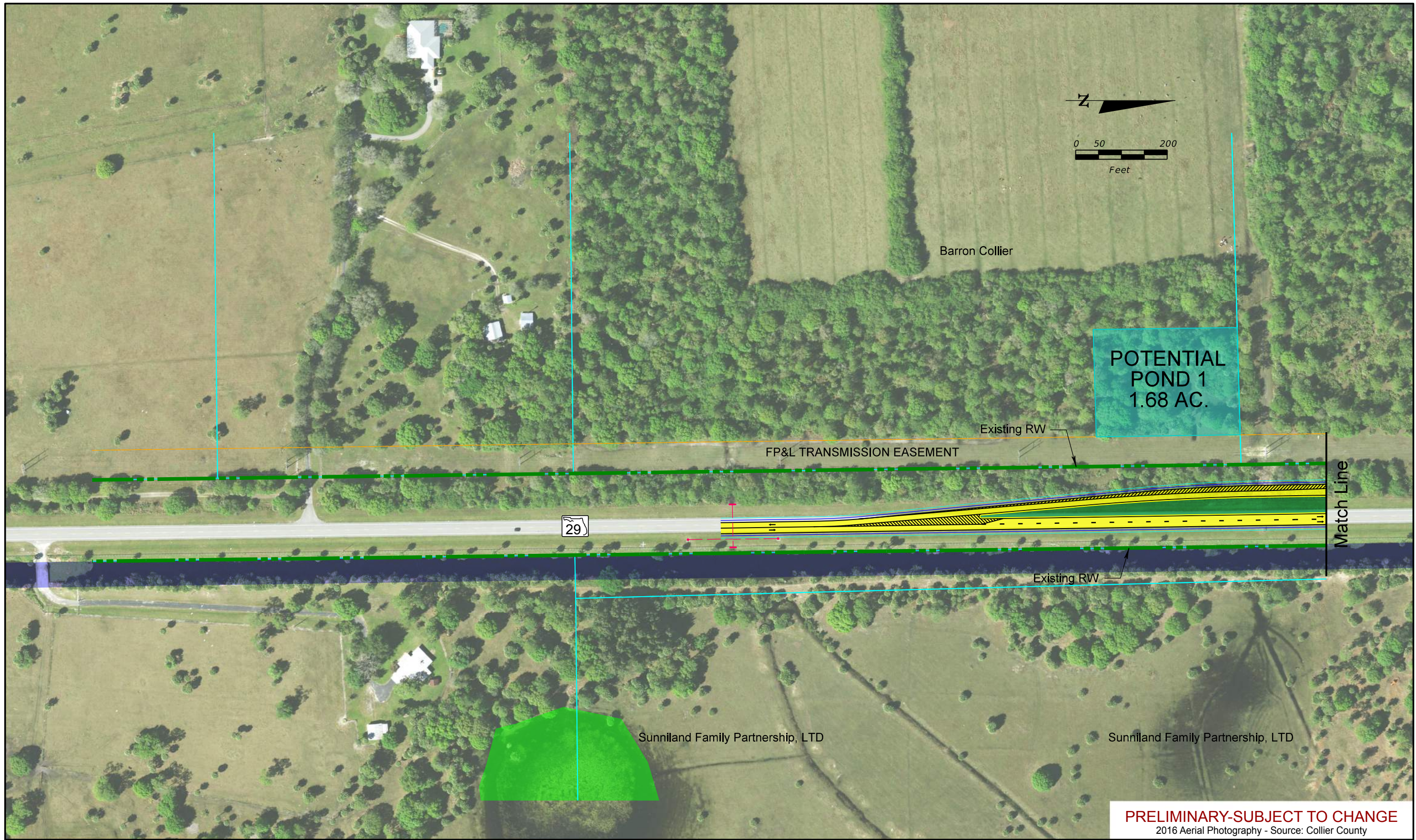
The ROW width needed for this typical section is 200 feet. **Figure 1.8** depicts this typical section.

Figure 1.8
SR 29 Bypass Typical Section from Gopher Ridge Road to SR 29



APPENDIX C

***Preferred Alternative Concept Plans
with Relocation Impacts***



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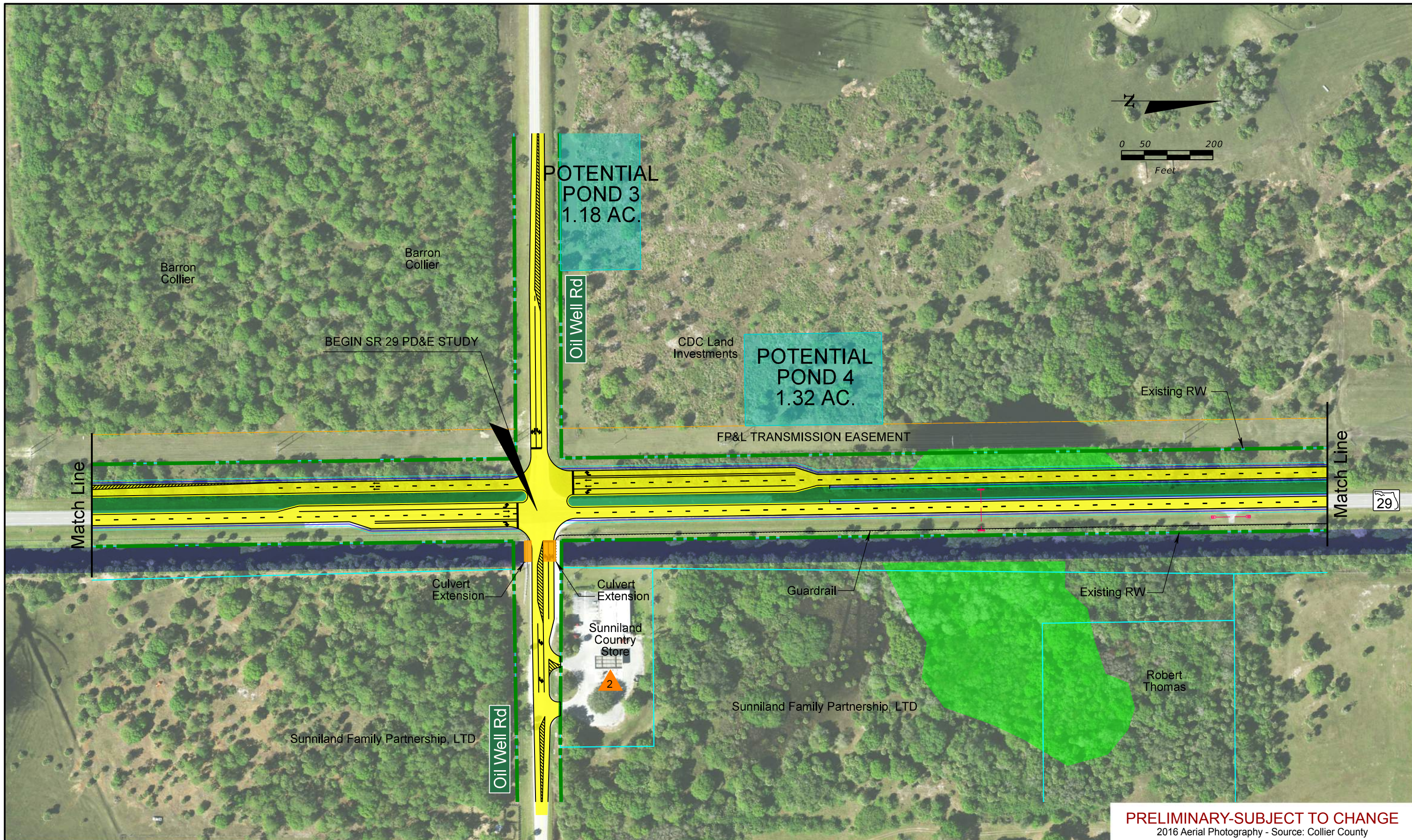
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend			
Existing Right-of-Way	Wetland	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Flood Plain Compensation	Proposed Sidewalks	Potential Contamination (Low)
Water/Canal	Traffic Signal	Proposed Structure	Potential Contamination (Medium or High)
Seminole Land		Existing Drainage Structure	

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Central Alternative #2

Sheet No.
1



BEGIN SR 29 PD&E STUDY

POTENTIAL POND 3
1.18 AC.

POTENTIAL POND 4
1.32 AC.

FP&L TRANSMISSION EASEMENT

Match Line

Match Line

29

Culvert Extension

Culvert Extension

Guardrail

Existing RW

Sunniland Family Partnership, LTD

Sunniland Country Store

Sunniland Family Partnership, LTD

Robert Thomas

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2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
From Oil Well Road to SR 82
FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend	
Existing Right-of-Way	Wetland
Parcels	Potential Pond
Proposed Right-of-Way	Potential Flood Plain Compensation
Water/Canal	Traffic Signal
Seminole Land	Proposed Pavement
Proposed Median/Border	Proposed Guardrail
Proposed Sidewalks	Potential Business Relocation
Proposed Structure	Potential Contamination (Low)
Existing Drainage Structure	Potential Contamination (Medium or High)

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Central Alternative #2

Sheet No.
2



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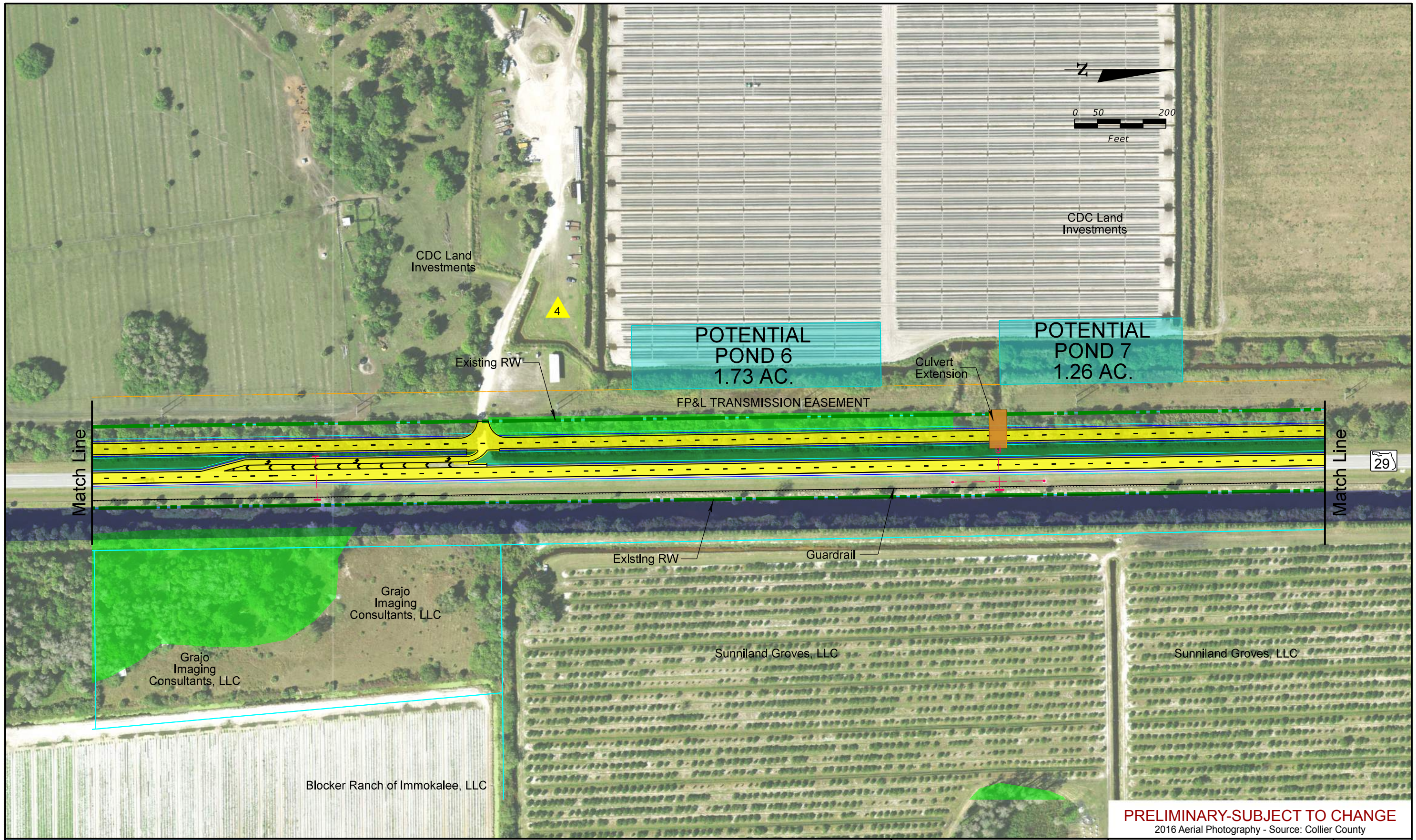
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks
			Proposed Structure
	Existing Drainage Structure		Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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3



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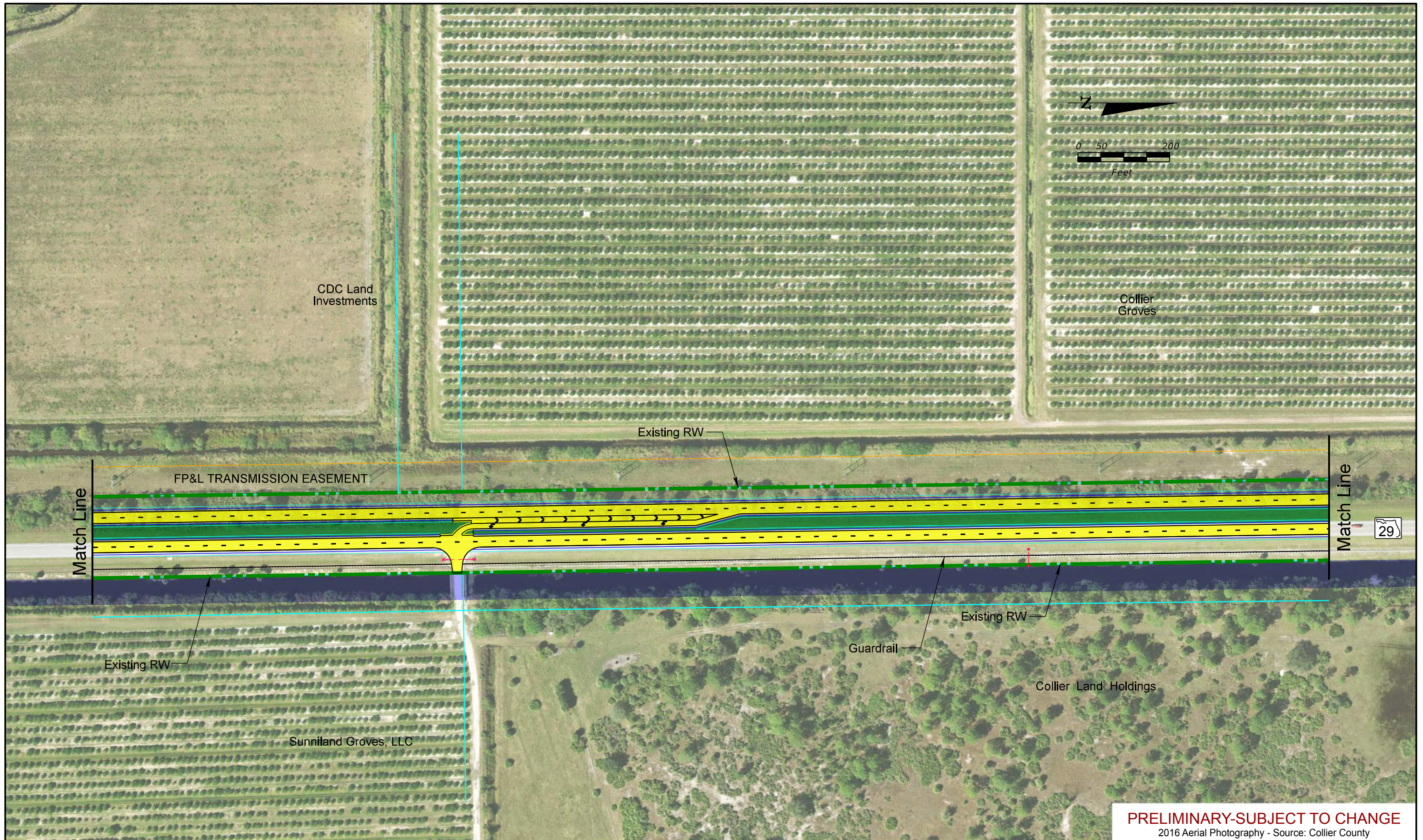
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks
	Existing Drainage Structure		Proposed Structure
			Proposed Guardrail
	Potential Business Relocation		Potential Contamination (Low)
	Potential Contamination (Low)		Potential Contamination (Medium or High)

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Central Alternative #2

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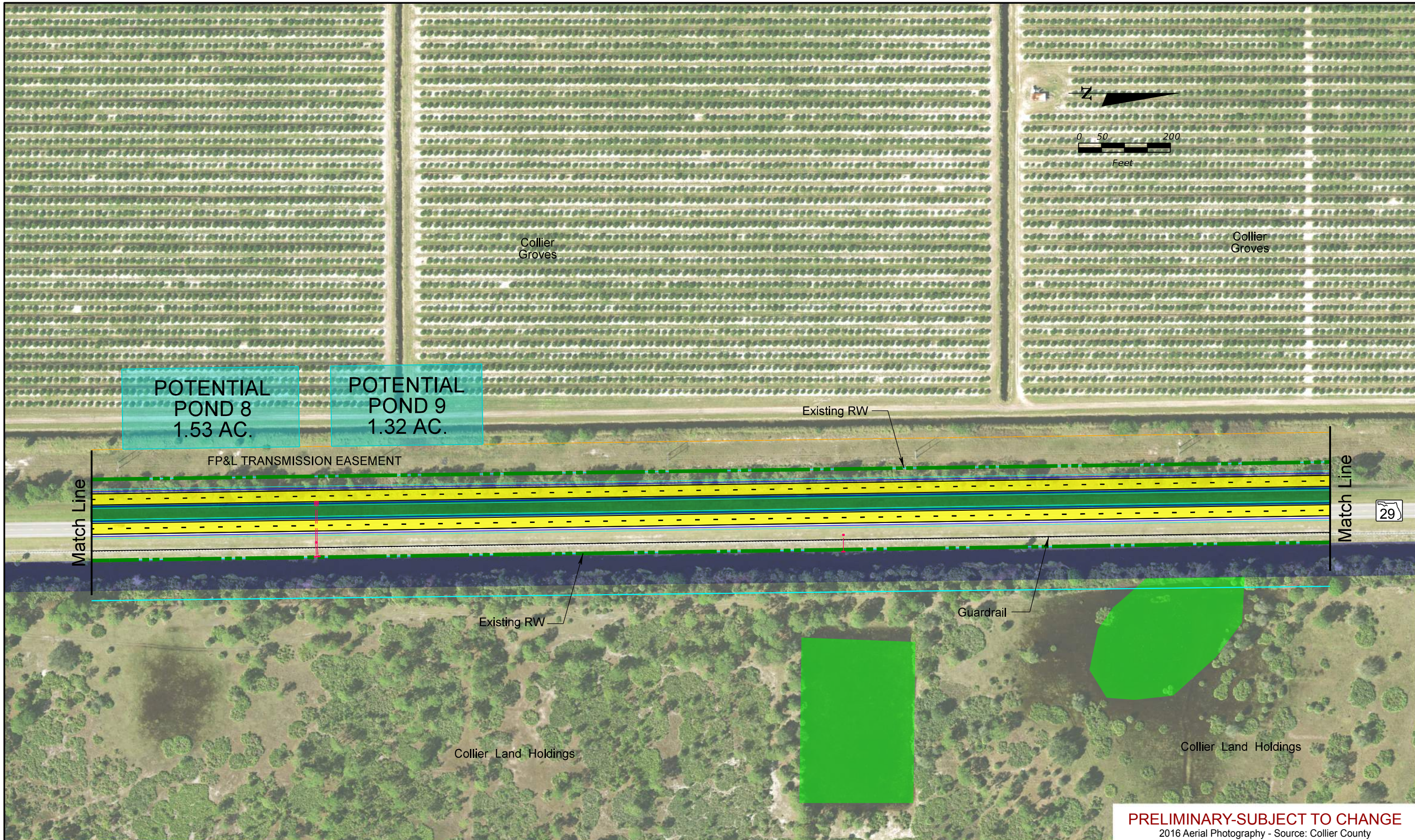
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 0227

Legend	
	Existing Right-of-Way
	Parcels
	Proposed Right-of-Way
	Water/Canal
	Seminole Land
	Wetland
	Potential Pond
	Potential Flood Plain Compensation
	Traffic Signal
	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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Central Alternative #2

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5



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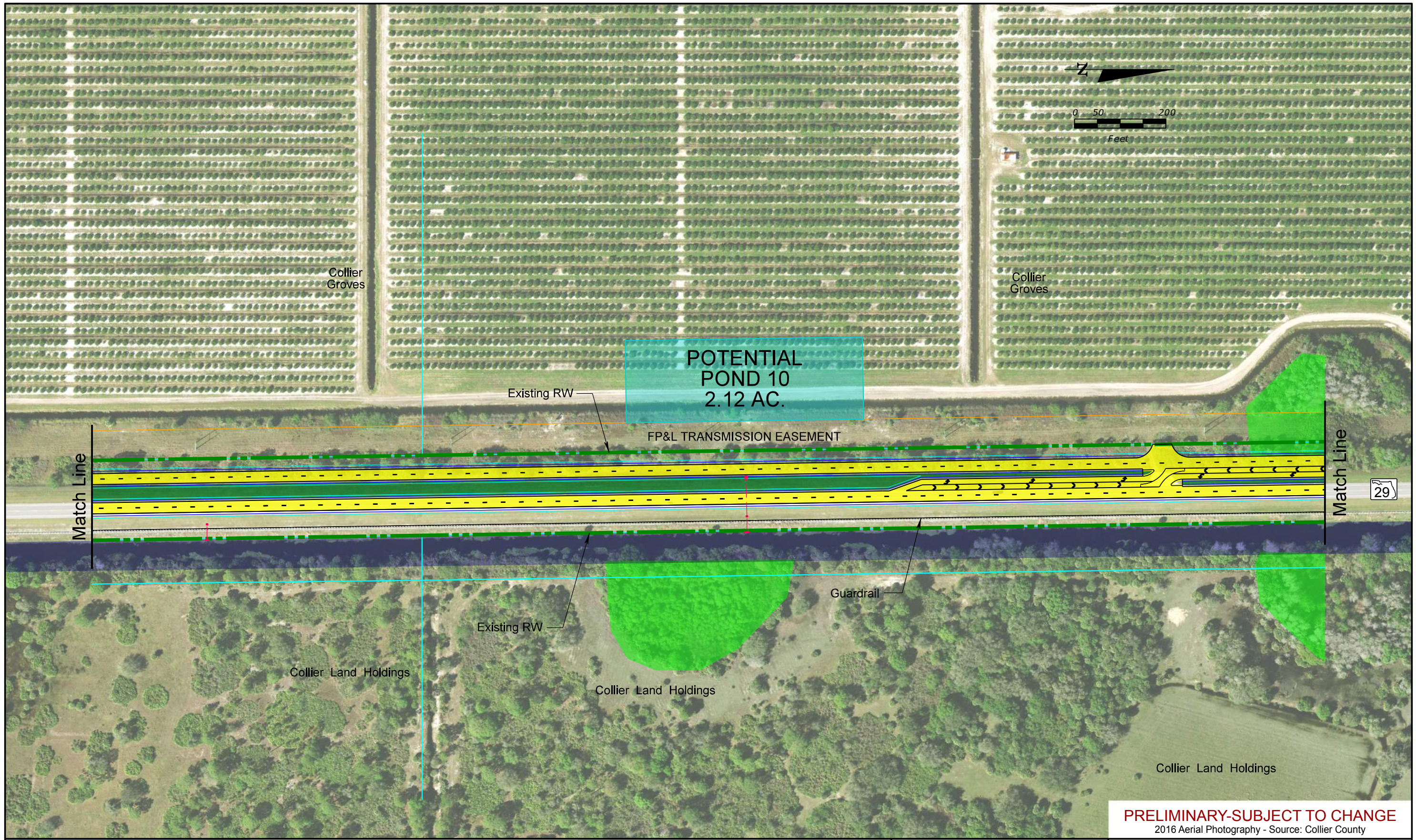
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend			
	Existing Right-of-Way		Proposed Pavement
	Parcels		Proposed Medlan/Border
	Proposed Right-of-Way		Proposed Sidewalks
	Water/Canal		Proposed Structure
	Seminole Land		Existing Drainage Structure
	Potential Pond		Proposed Guardrail
	Potential Flood Plain Compensation		Potential Business Relocation
	Wetland		Potential Contamination (Low)
	Potential Flood Plain Compensation		Potential Contamination (Medium or High)
	Traffic Signal		

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SR 29 PD&E Study
From Oil Well Road to SR 82
FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend	
	Existing Right-of-Way
	Parcels
	Proposed Right-of-Way
	Water/Canal
	Seminole Land
	Wetland
	Potential Pond
	Potential Flood Plain Compensation
	Traffic Signal
	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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7



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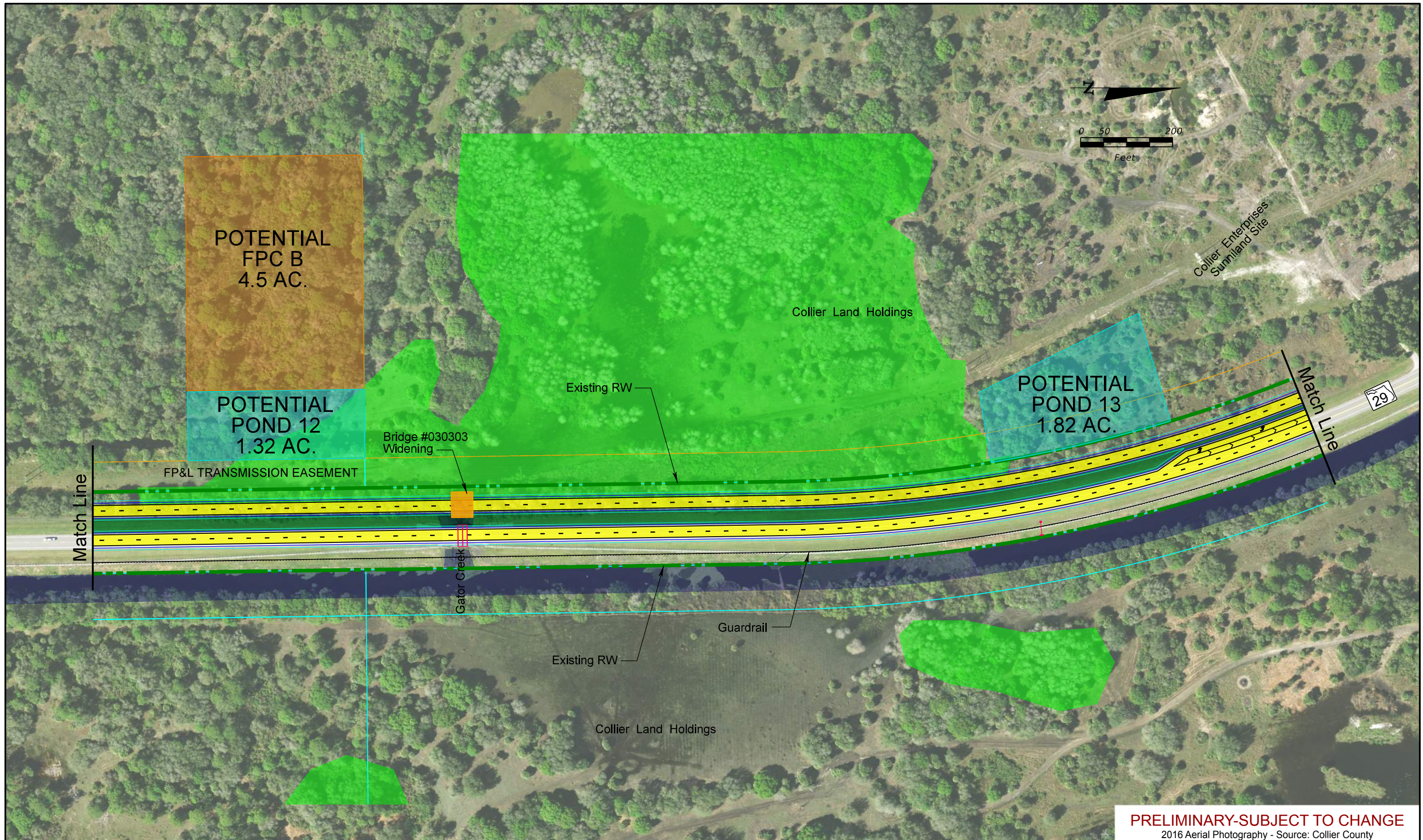
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend	
	Existing Right-of-Way
	Parcels
	Proposed Right-of-Way
	Water/Canal
	Seminole Land
	Wetland
	Potential Pond
	Potential Flood Plain Compensation
	Traffic Signal
	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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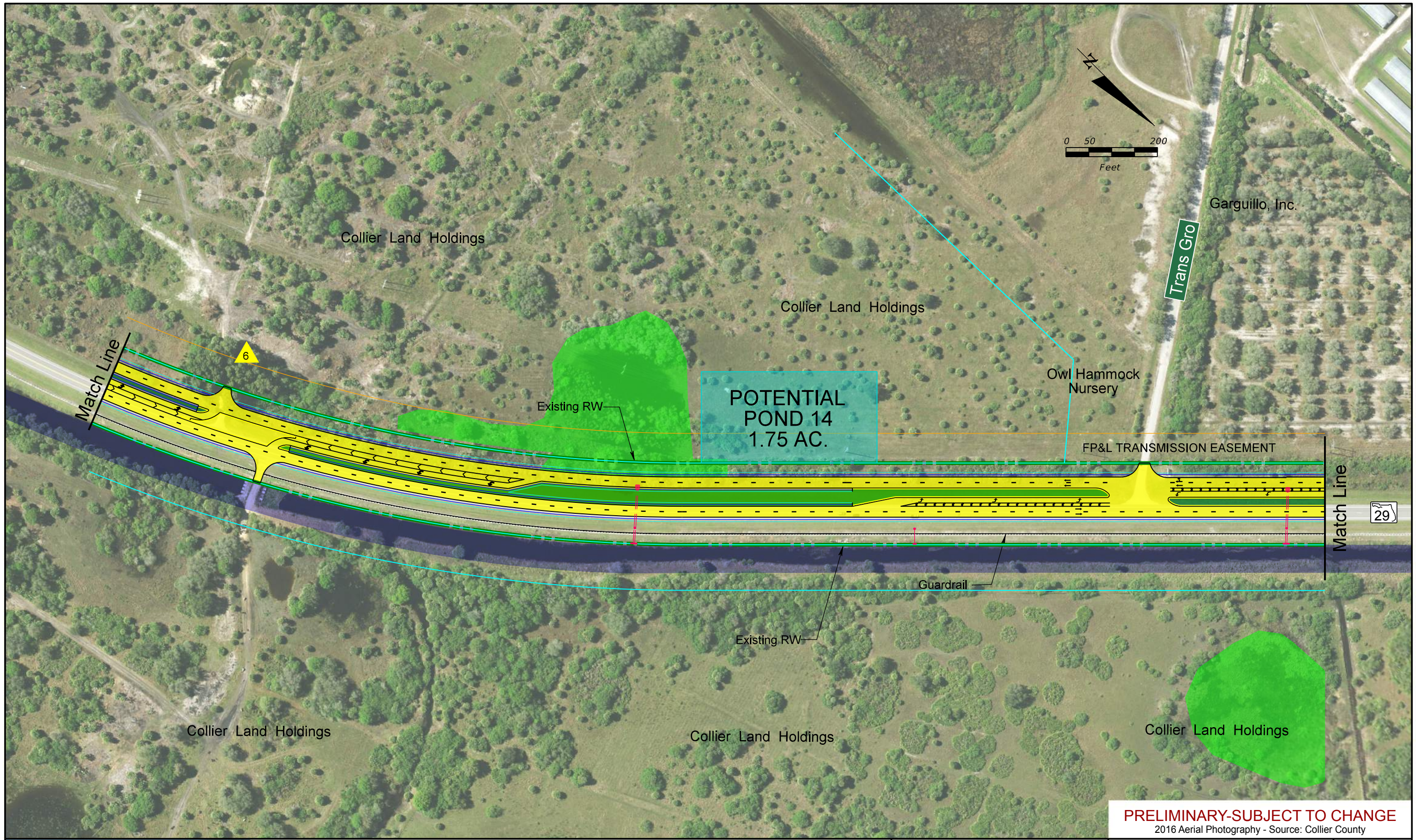
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend			
Existing Right-of-Way	Wetland	Proposed Pavement	Potential Business Relocation
Parcels	Potential Pond	Proposed Median/Border	Potential Contamination (Low)
Proposed Right-of-Way	Potential Flood Plain Compensation	Proposed Sidewalks	Potential Contamination (Medium or High)
Water/Canal	Traffic Signal	Proposed Structure	
Seminole Land		Existing Drainage Structure	
		Proposed Guardrail	

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Central Alternative #2

Sheet No.
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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Proposed Pavement
	Seminole Land		Proposed Median/Border
	Traffic Signal		Proposed Sidewalks
	Existing Drainage Structure		Proposed Structure
	Proposed Guardrail		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Medium or High)

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Central Alternative #2

Sheet No.
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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Potential Pond
	Parcels		Potential Flood Plain Compensation
	Proposed Right-of-Way		Traffic Signal
	Water/Canal		Proposed Pavement
	Seminole Land		Proposed Median/Border
	Existing Drainage Structure		Proposed Sidewalks
	Proposed Guardrail		Proposed Structure
	Potential Business Relocation		Potential Contamination (Low)
	Potential Contamination (Medium or High)		

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Central Alternative #2

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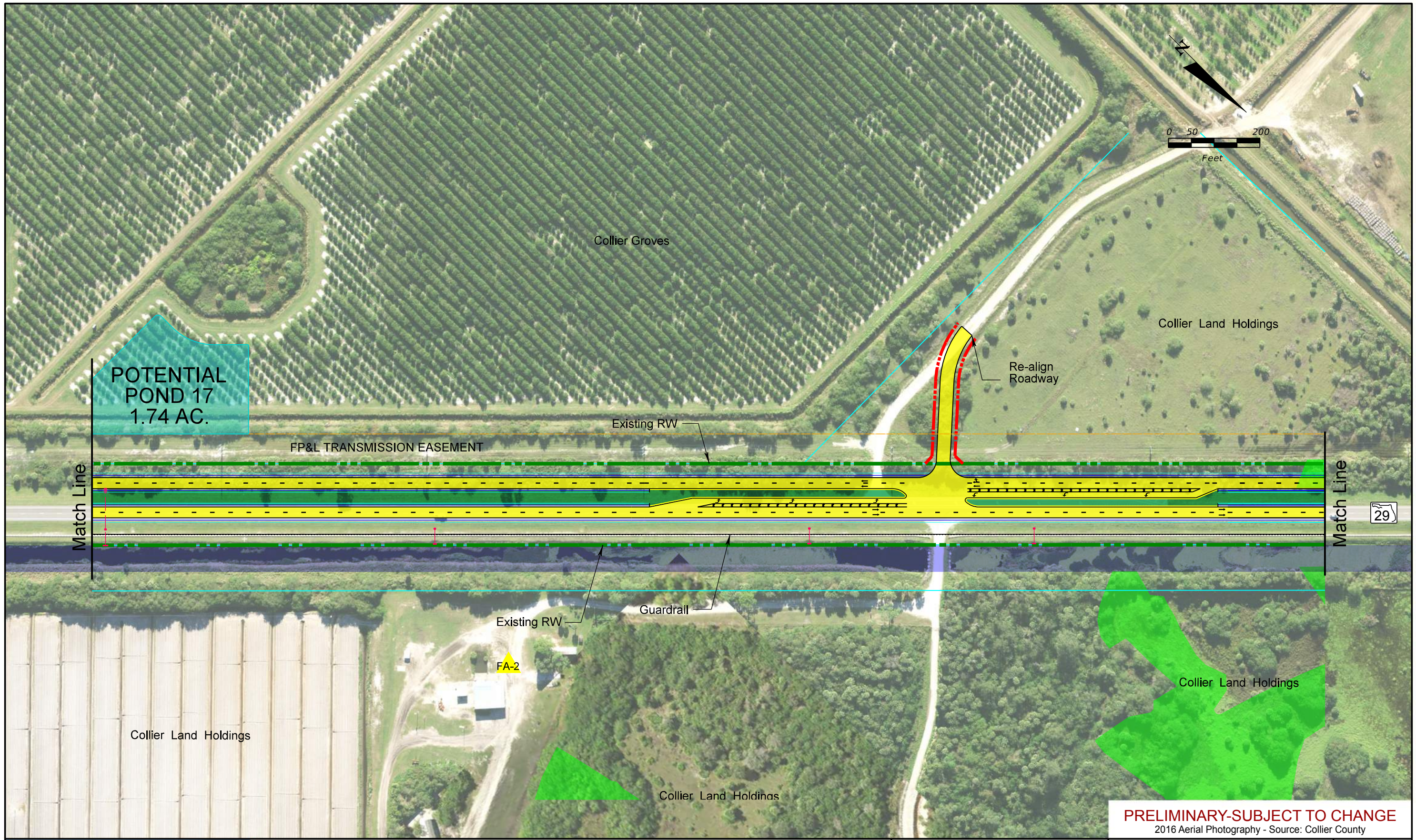
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
	Proposed Median/Border		Proposed Sidewalks
	Proposed Structure		Proposed Guardrail
	Existing Drainage Structure		Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

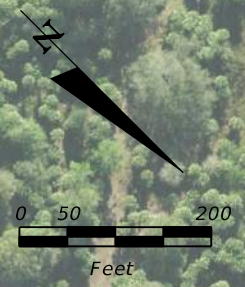
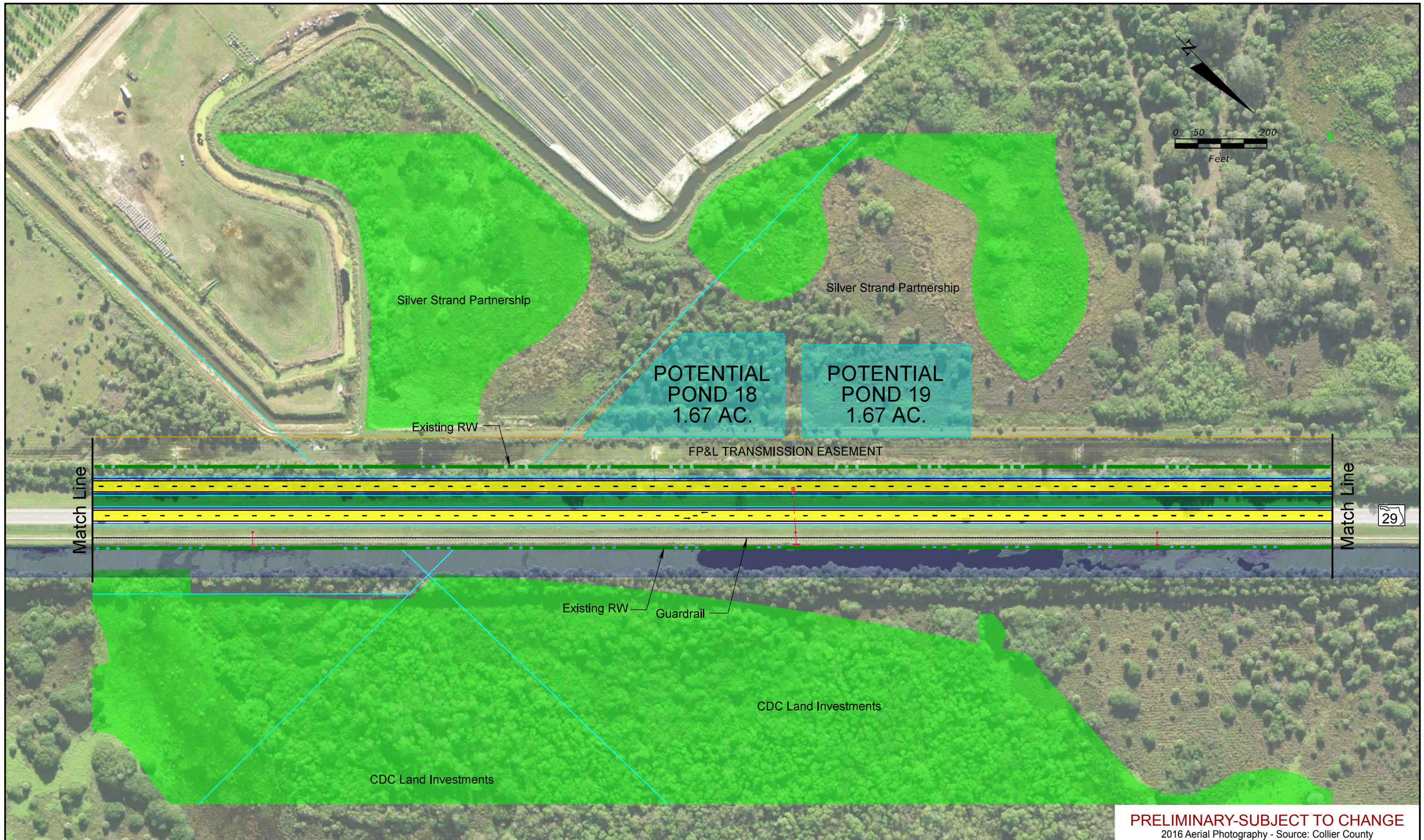
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks
			Proposed Structure
			Existing Drainage Structure
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

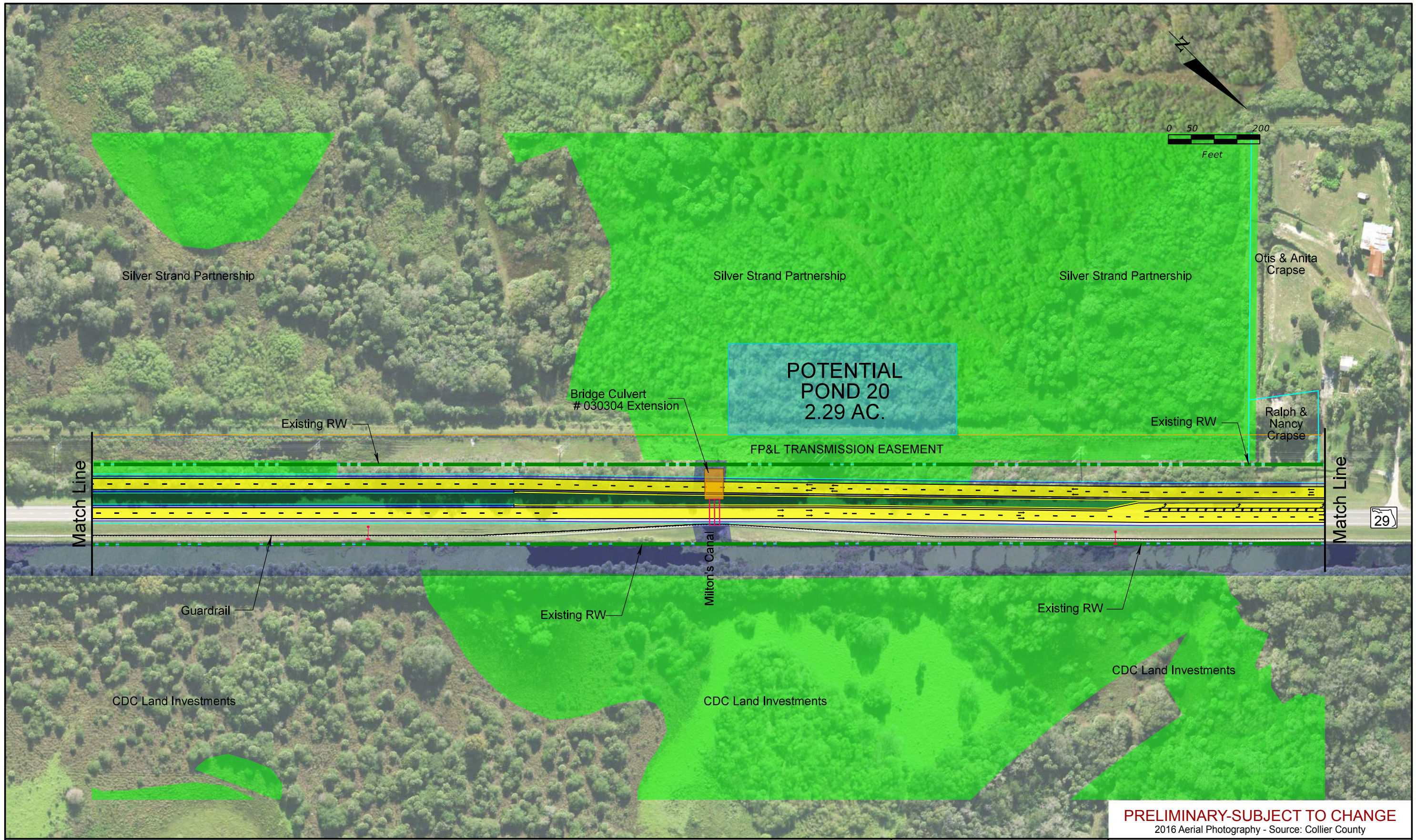
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
	Existing Drainage Structure		Proposed Median/Border
	Existing RW		Proposed Sidewalks
	Guardrail		Proposed Structure
	Proposed Guardrail		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
	Proposed Median/Border		Proposed Sidewalks
	Proposed Structure		Potential Business Relocation
	Existing Drainage Structure		Potential Contamination (Low)
	Proposed Guardrail		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

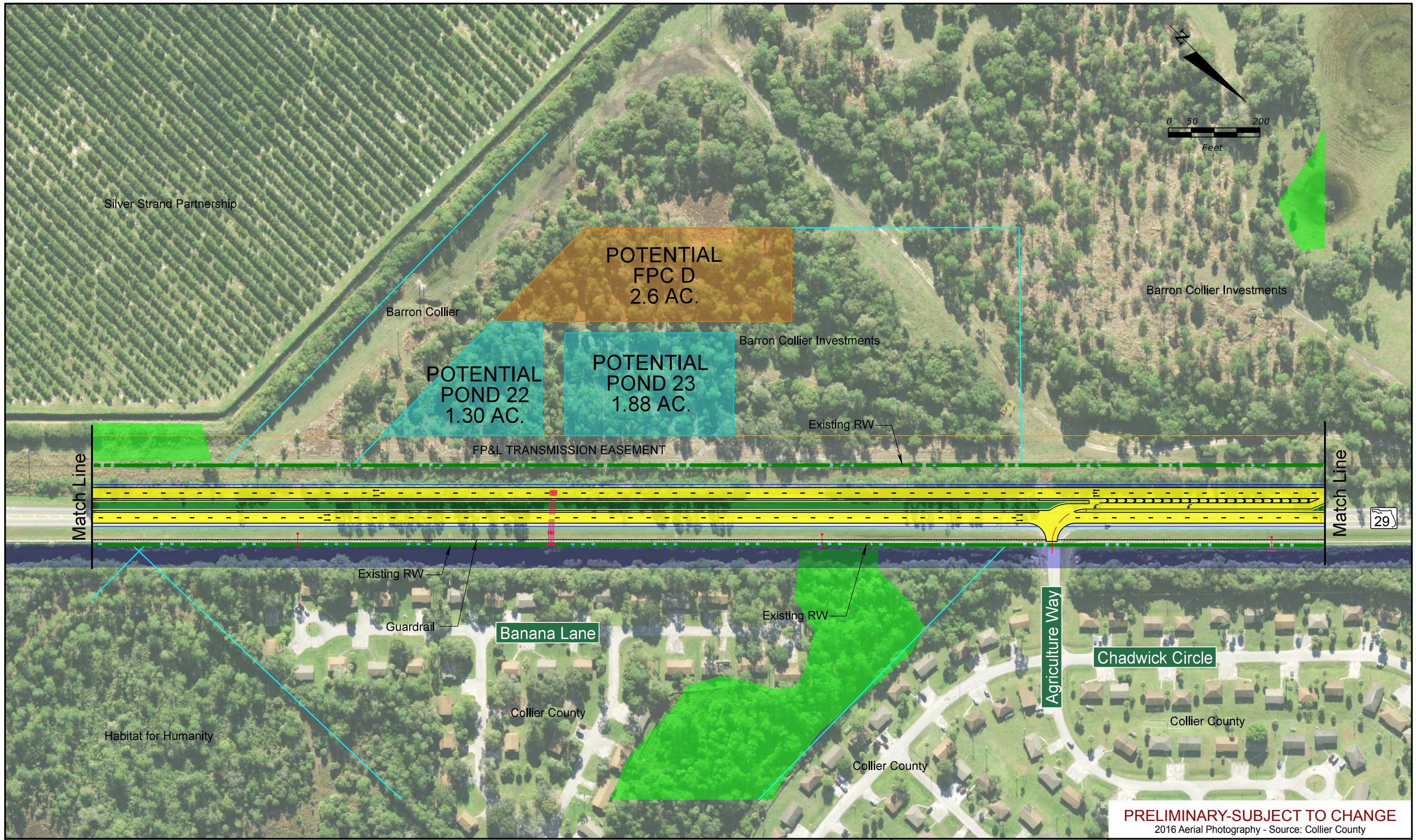
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
	Existing Drainage Structure		Proposed Median/Border
	Existing Drainage Structure		Proposed Sidewalks
	Existing Drainage Structure		Proposed Structure
	Existing Drainage Structure		Potential Business Relocation
	Existing Drainage Structure		Potential Contamination (Low)
	Existing Drainage Structure		Potential Contamination (Medium or High)
	Existing Drainage Structure		Proposed Guardrail

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

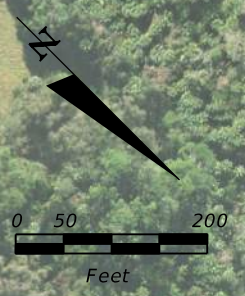
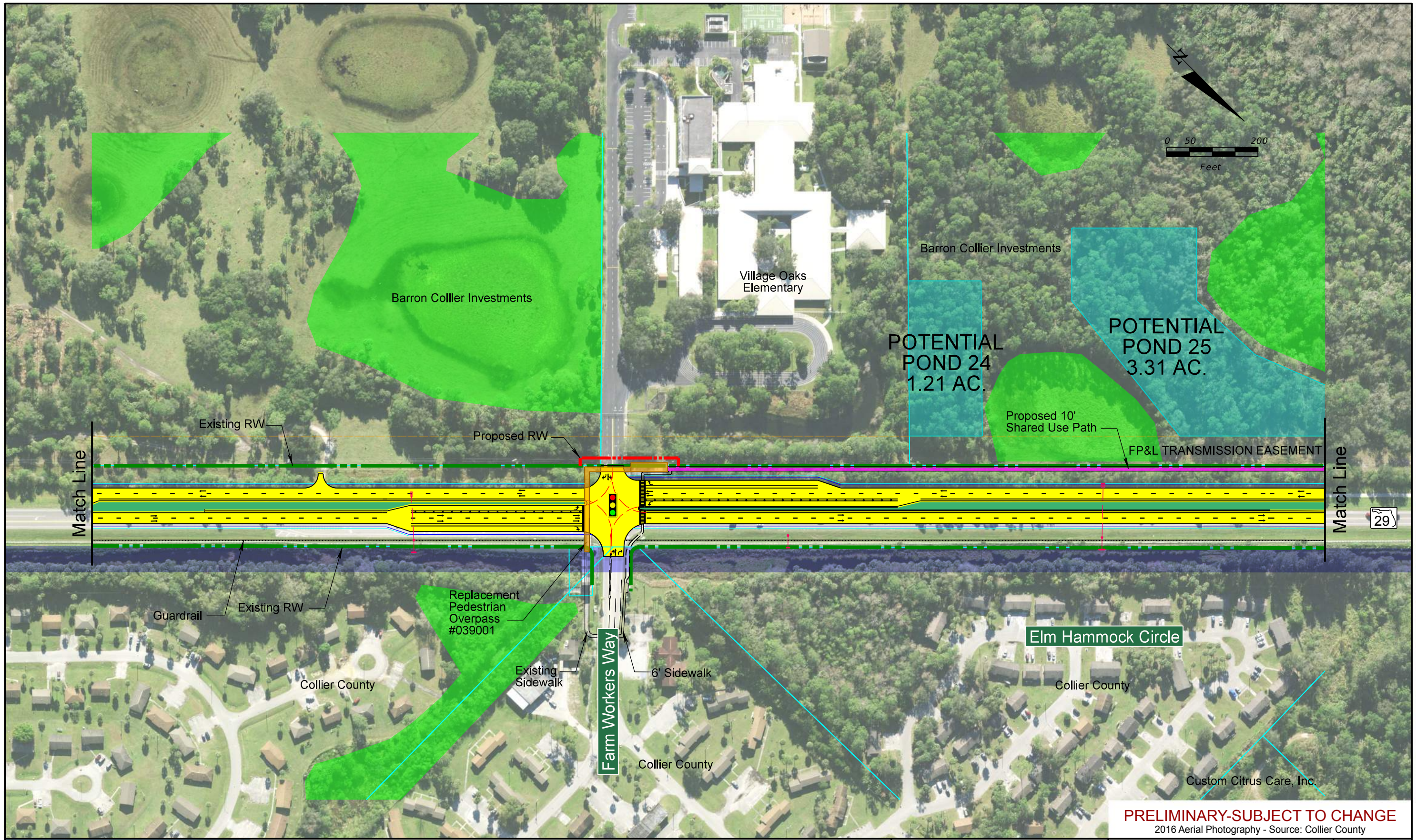
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend	
Existing Right-of-Way	Wetland
Parcels	Potential Pond
Proposed Right-of-Way	Potential Flood Plain Compensation
Water/Canal	Traffic Signal
Seminole Land	Proposed Pavement
Existing Drainage Structure	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

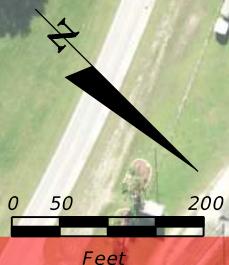
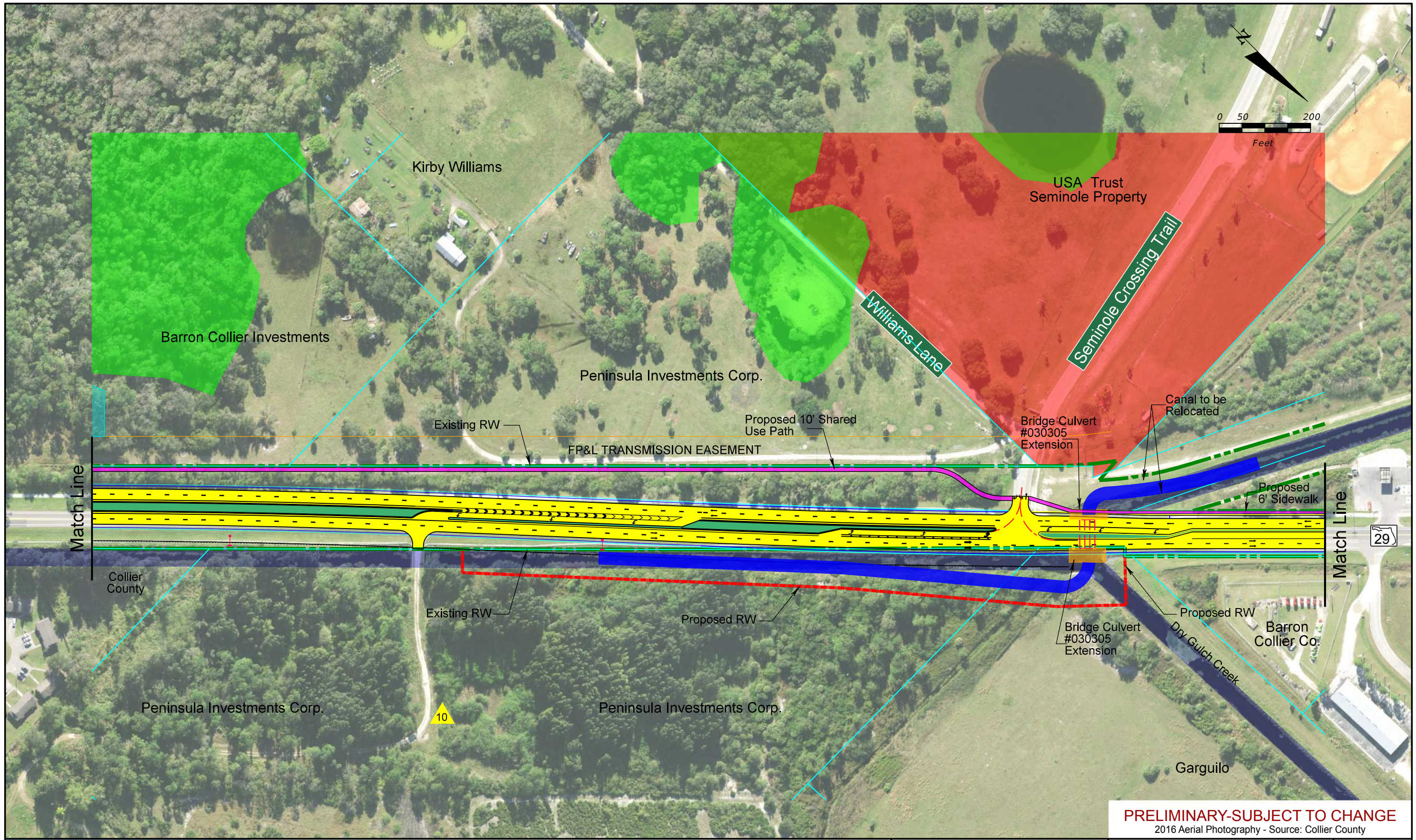
Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Proposed Structure
	Seminole Land		Proposed Pavement
	Traffic Signal		Proposed Median/Border
	Existing Drainage Structure		Proposed Sidewalks
	Existing Drainage Structure		Proposed Guardrail
	Potential Business Relocation		Potential Contamination (Low)
	Potential Contamination (Medium or High)		

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

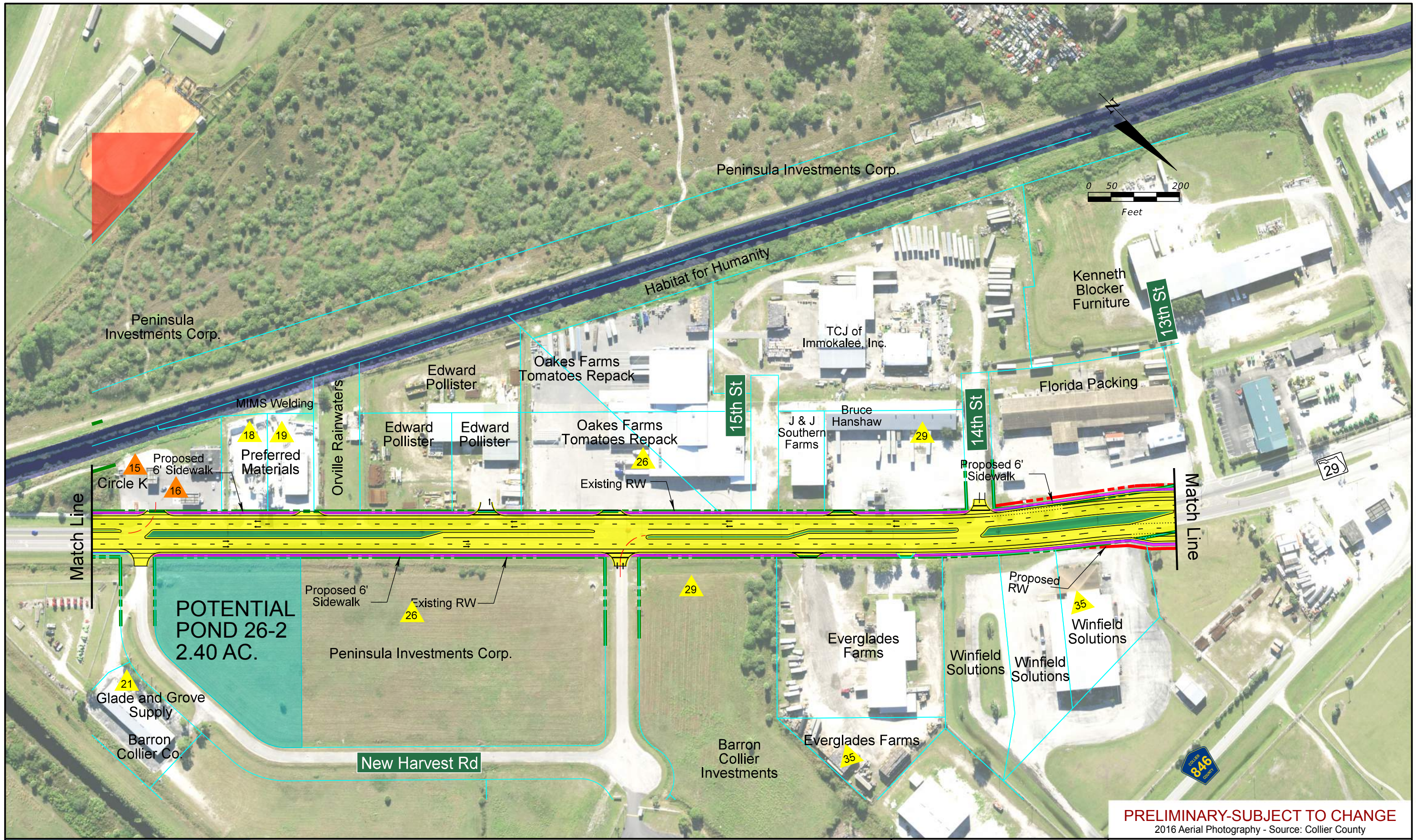
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
	Proposed Median/Border		Proposed Sidewalks
	Proposed Structure		Potential Business Relocation
	Existing Drainage Structure		Potential Contamination (Low)
	Proposed Guardrail		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

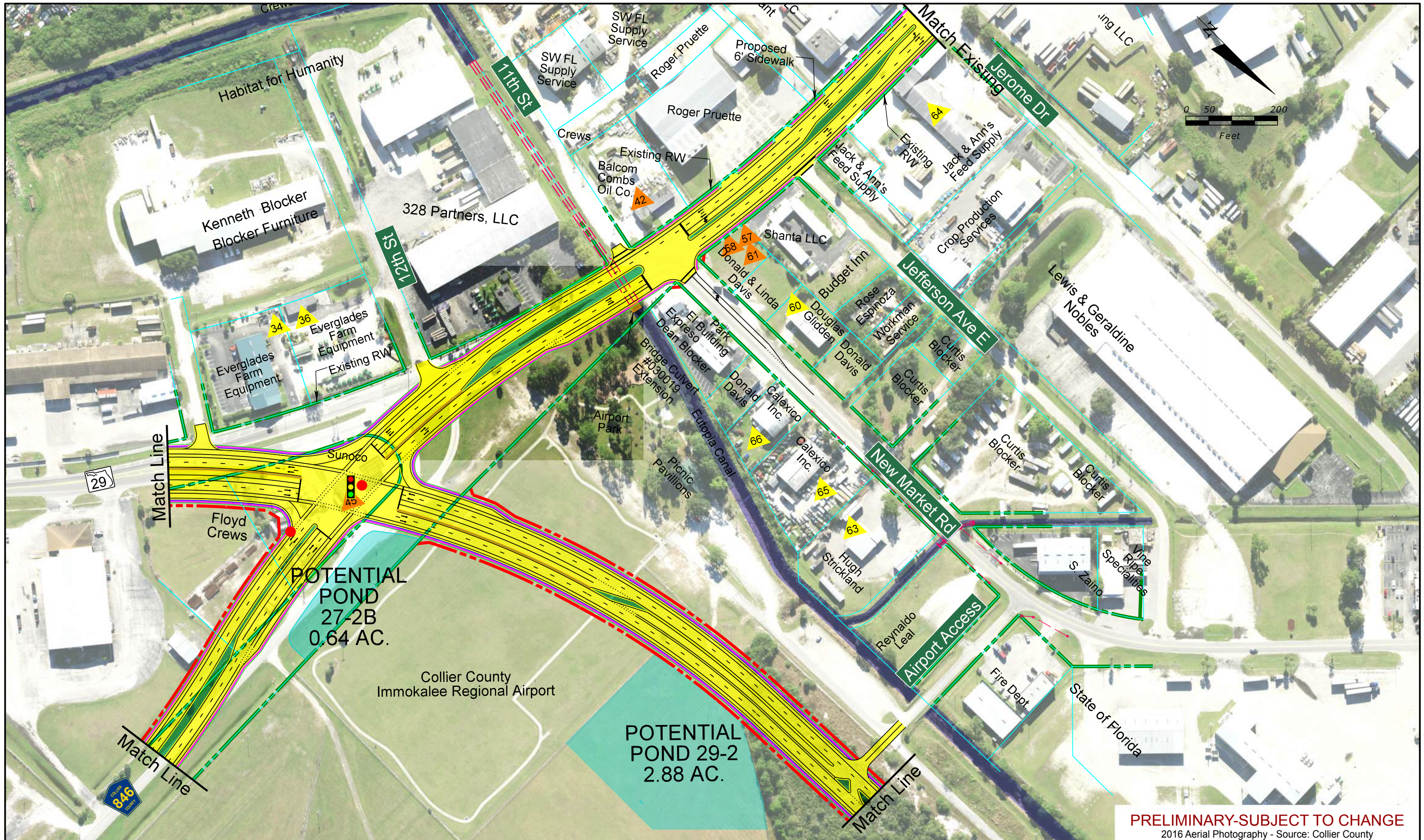
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks
			Proposed Structure
			Existing Drainage Structure
	Proposed Guardrail		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

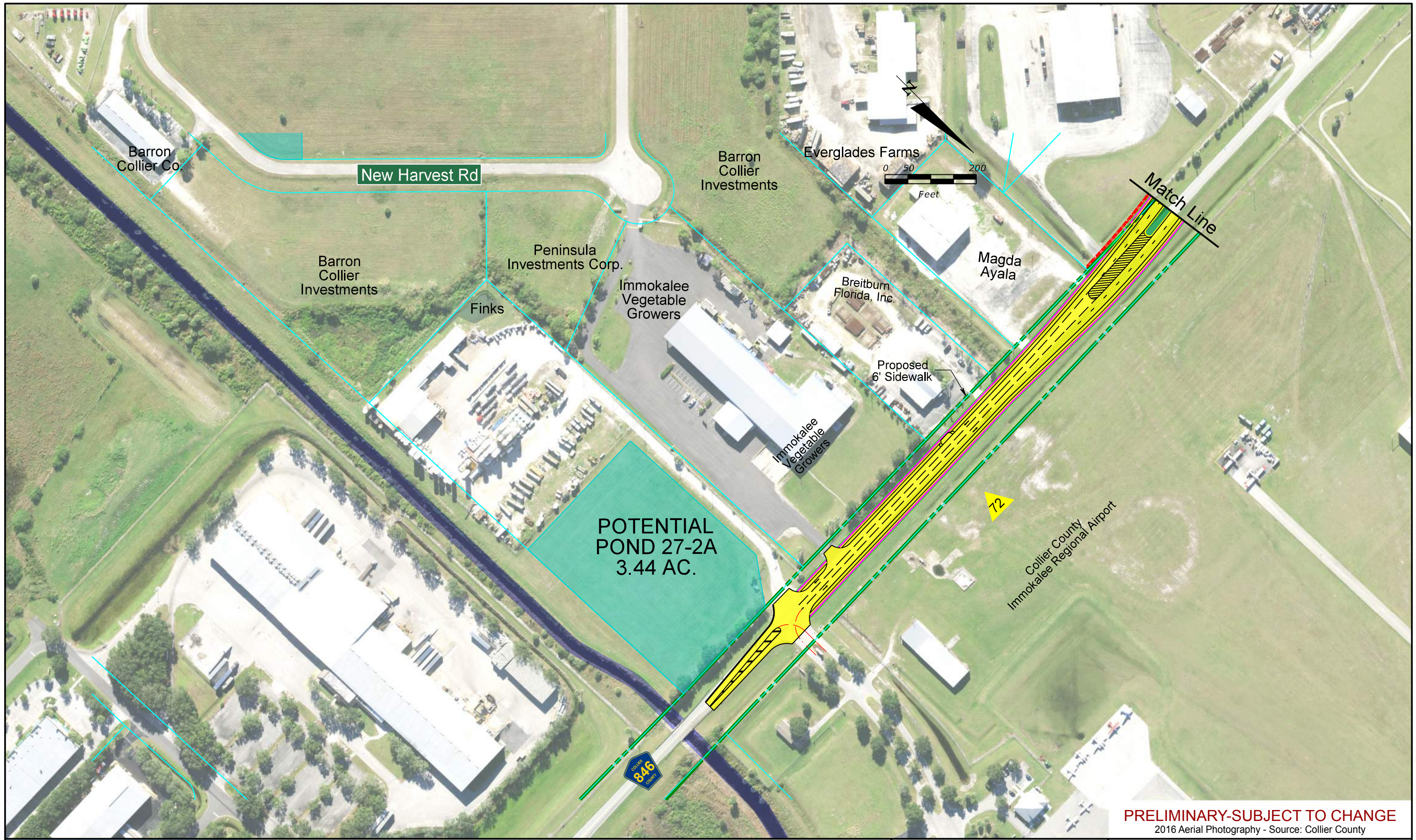
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend	
	Existing Right-of-Way
	Parcels
	Proposed Right-of-Way
	Water/Canal
	Seminole Land
	Wetland
	Potential Pond
	Potential Flood Plain Compensation
	Traffic Signal
	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks
			Proposed Structure
	Existing Drainage Structure		Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

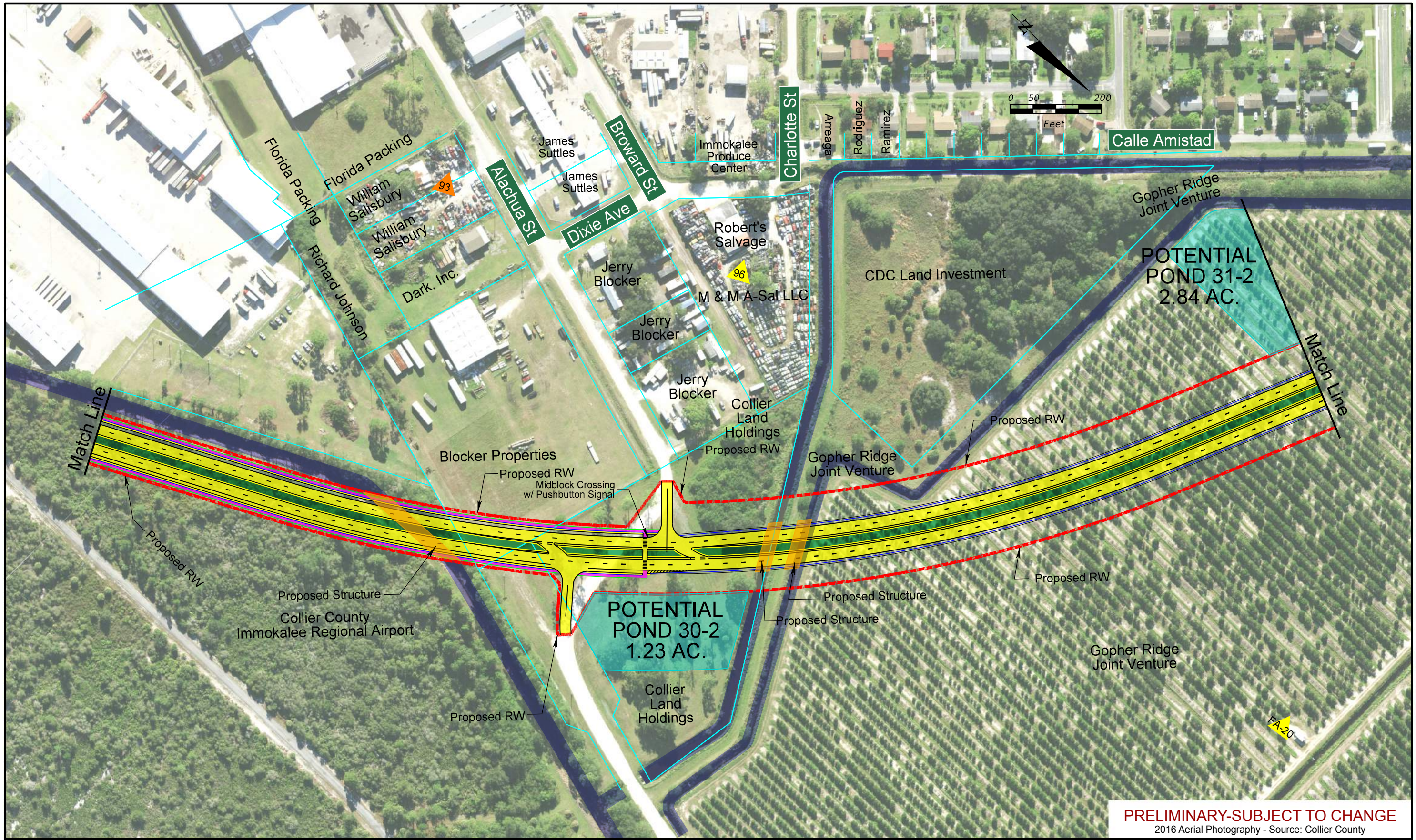
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Proposed Pavement
	Parcels		Proposed Median/Border
	Proposed Right-of-Way		Proposed Sidewalks
	Water/Canal		Proposed Structure
	Seminole Land		Existing Drainage Structure
	Wetland		Proposed Guardrail
	Potential Pond		Potential Business Relocation
	Potential Flood Plain Compensation		Potential Contamination (Low)
	Traffic Signal		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

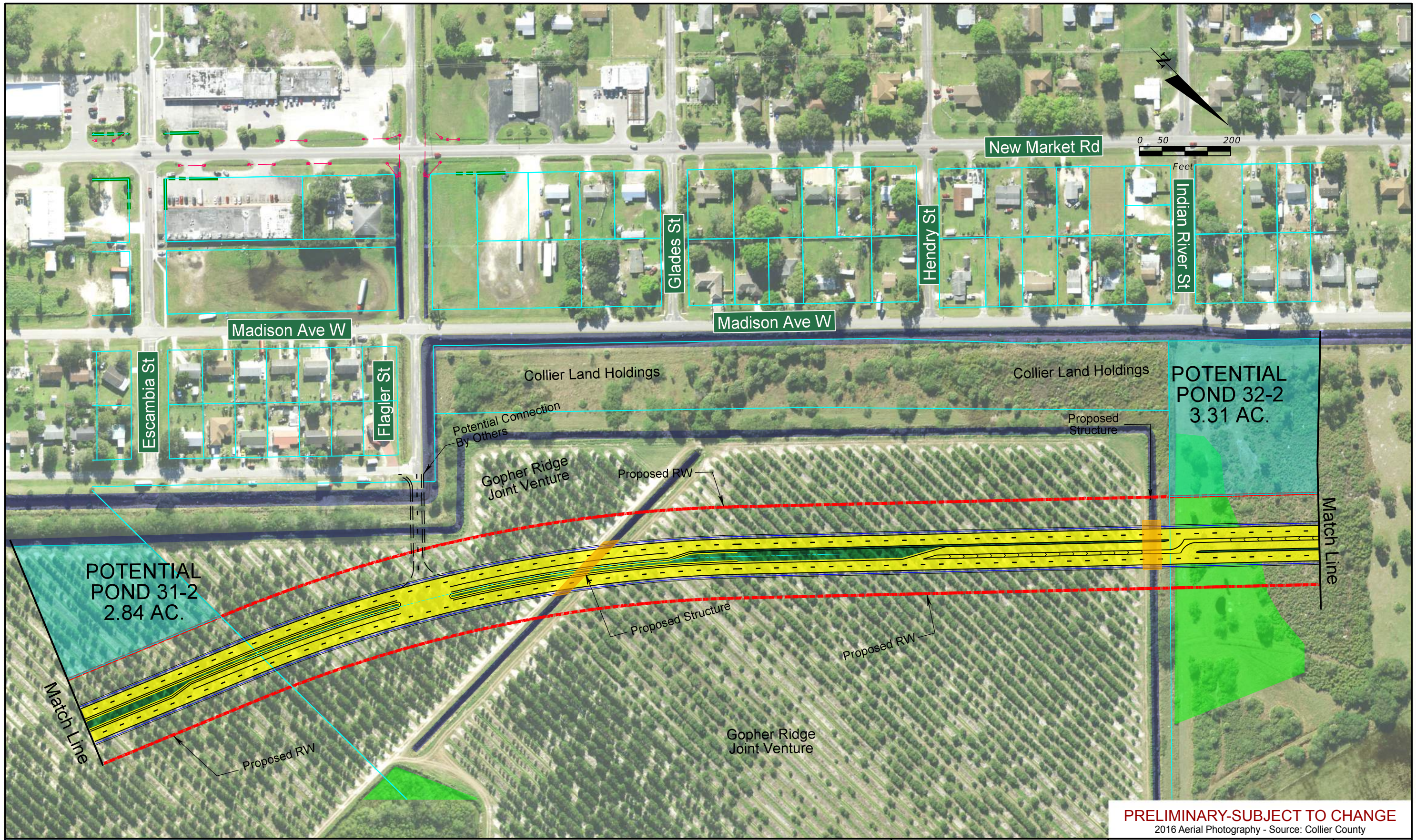
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks
			Proposed Structure
	Existing Drainage Structure		Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)
			Proposed Guardrail

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

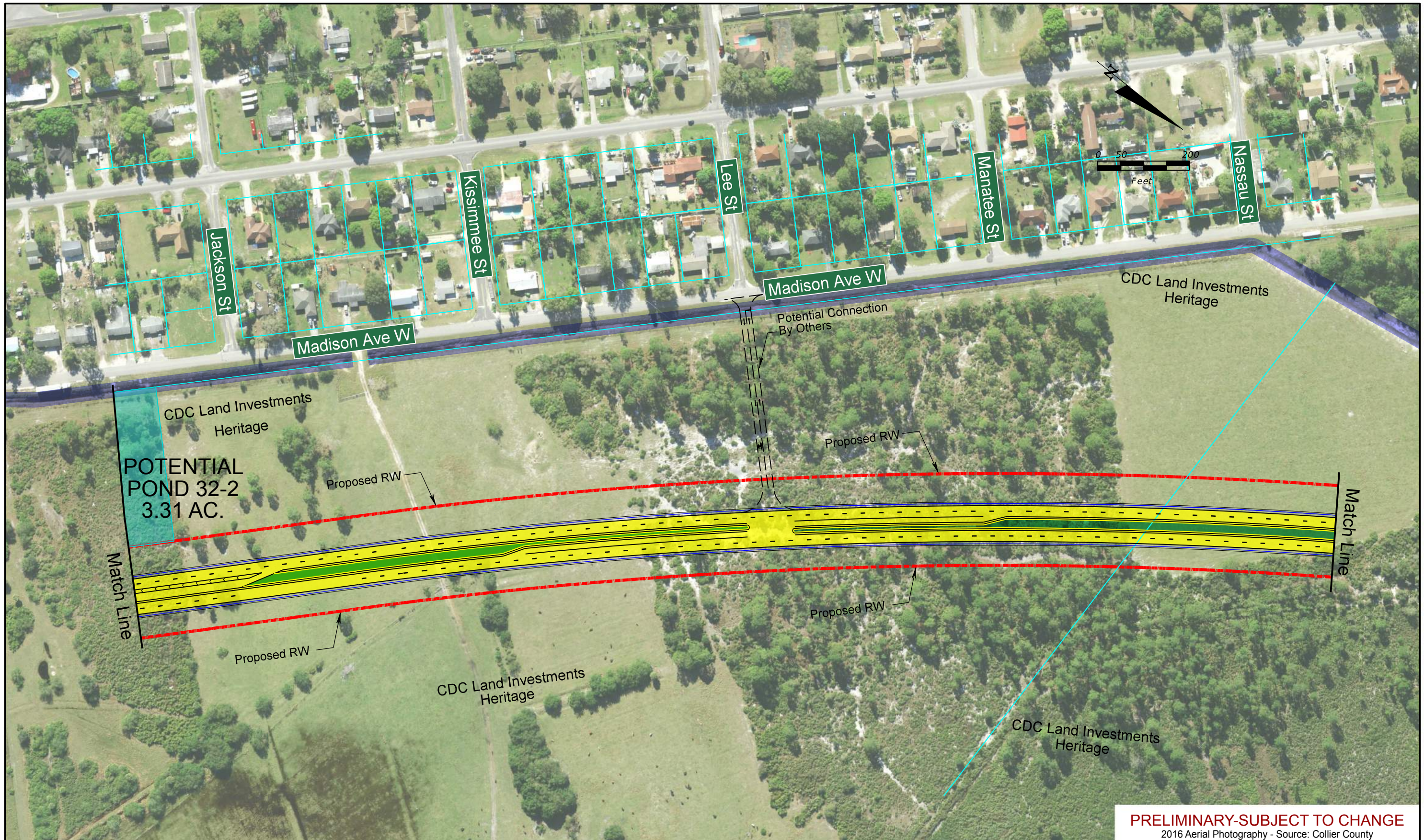
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend	
	Existing Right-of-Way
	Parcels
	Proposed Right-of-Way
	Water/Canal
	Seminole Land
	Wetland
	Potential Pond
	Potential Flood Plain Compensation
	Traffic Signal
	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

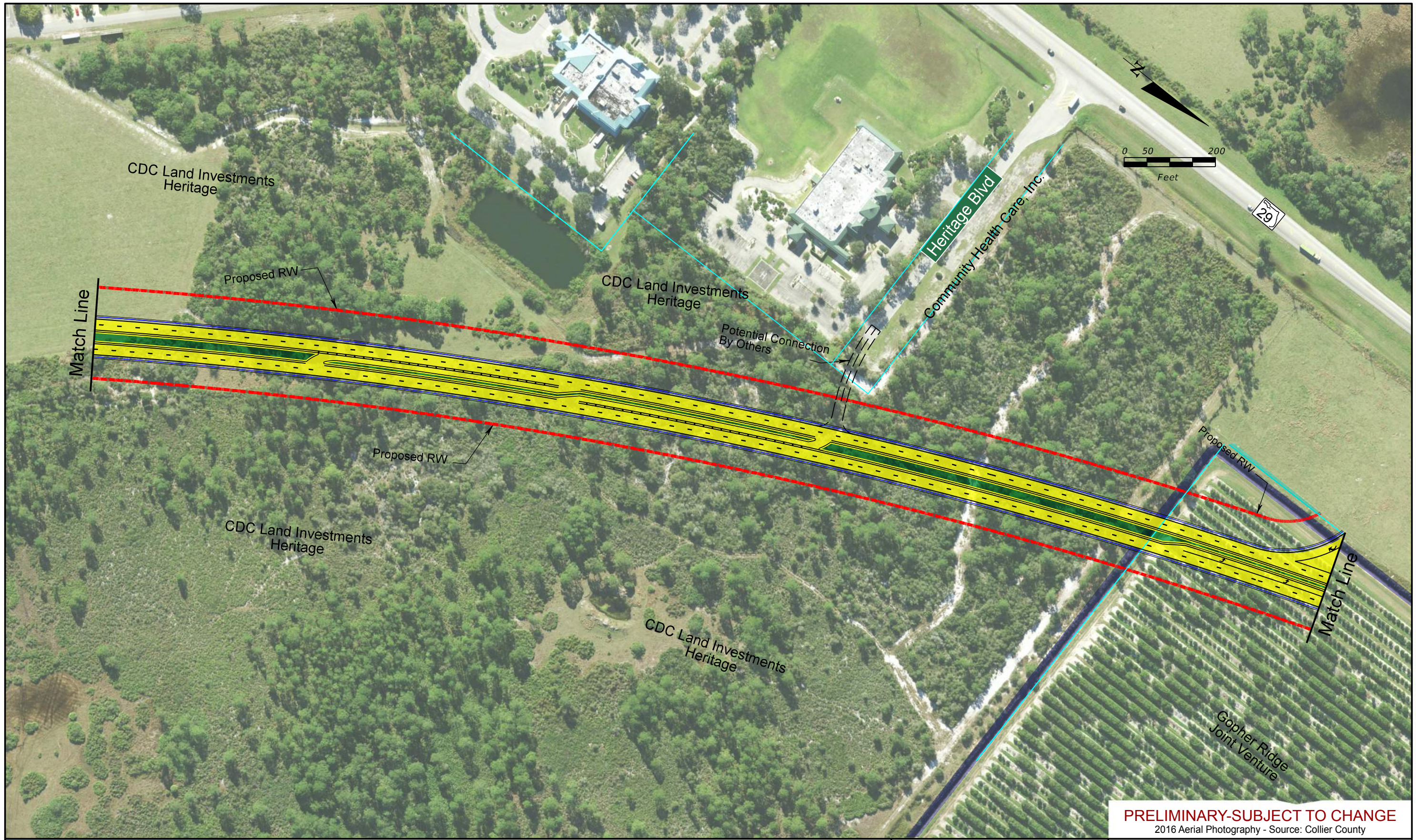
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
	Proposed Median/Border		Proposed Sidewalks
	Proposed Guardrail		Proposed Structure
	Potential Business Relocation		Existing Drainage Structure
	Potential Contamination (Low)		
	Potential Contamination (Medium or High)		

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

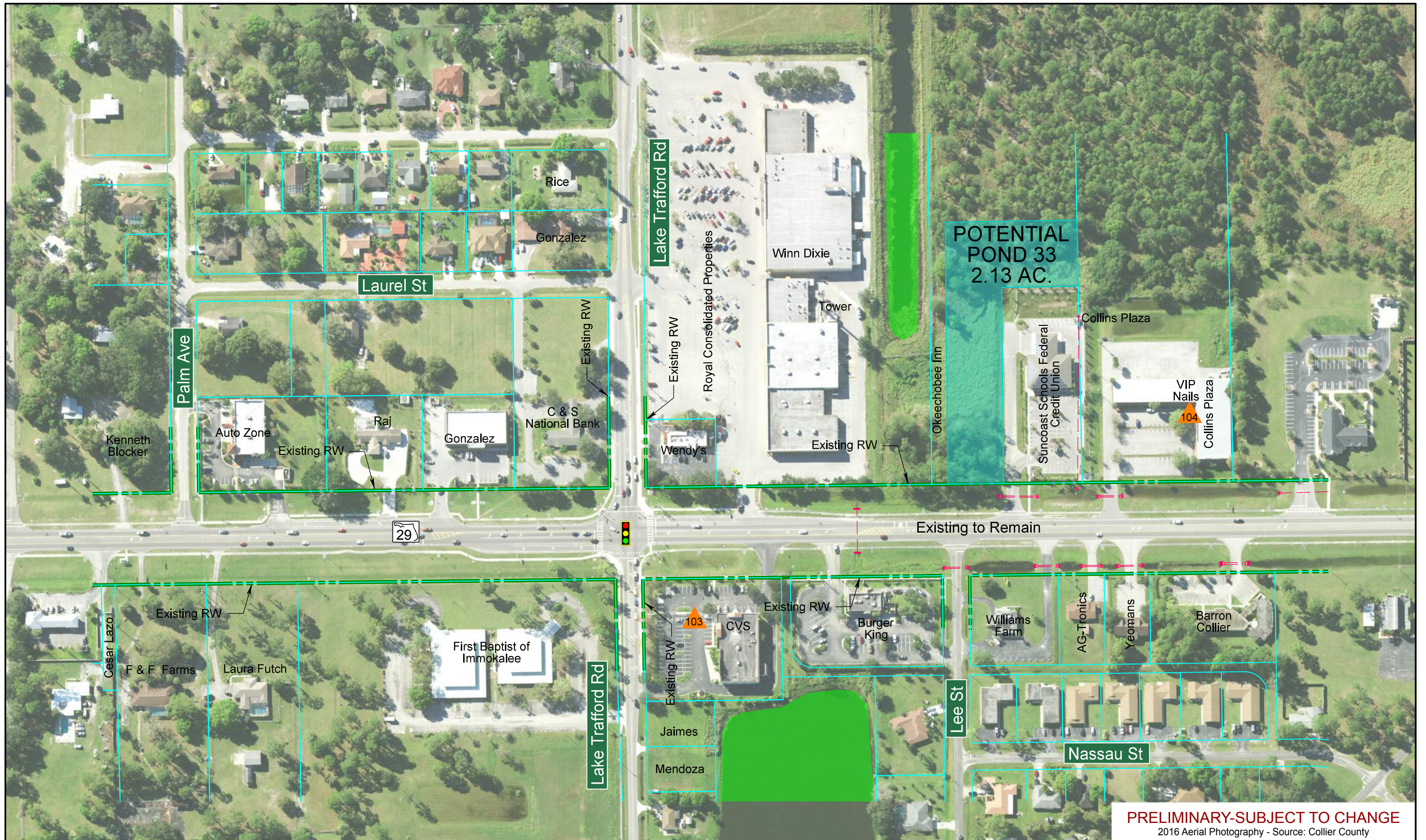
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
Existing Right-of-Way	Wetland	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Flood Plain Compensation	Proposed Sidewalks	Potential Contamination (Low)
Water/Canal	Traffic Signal	Proposed Structure	Potential Contamination (Medium or High)
Seminole Land		Existing Drainage Structure	

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

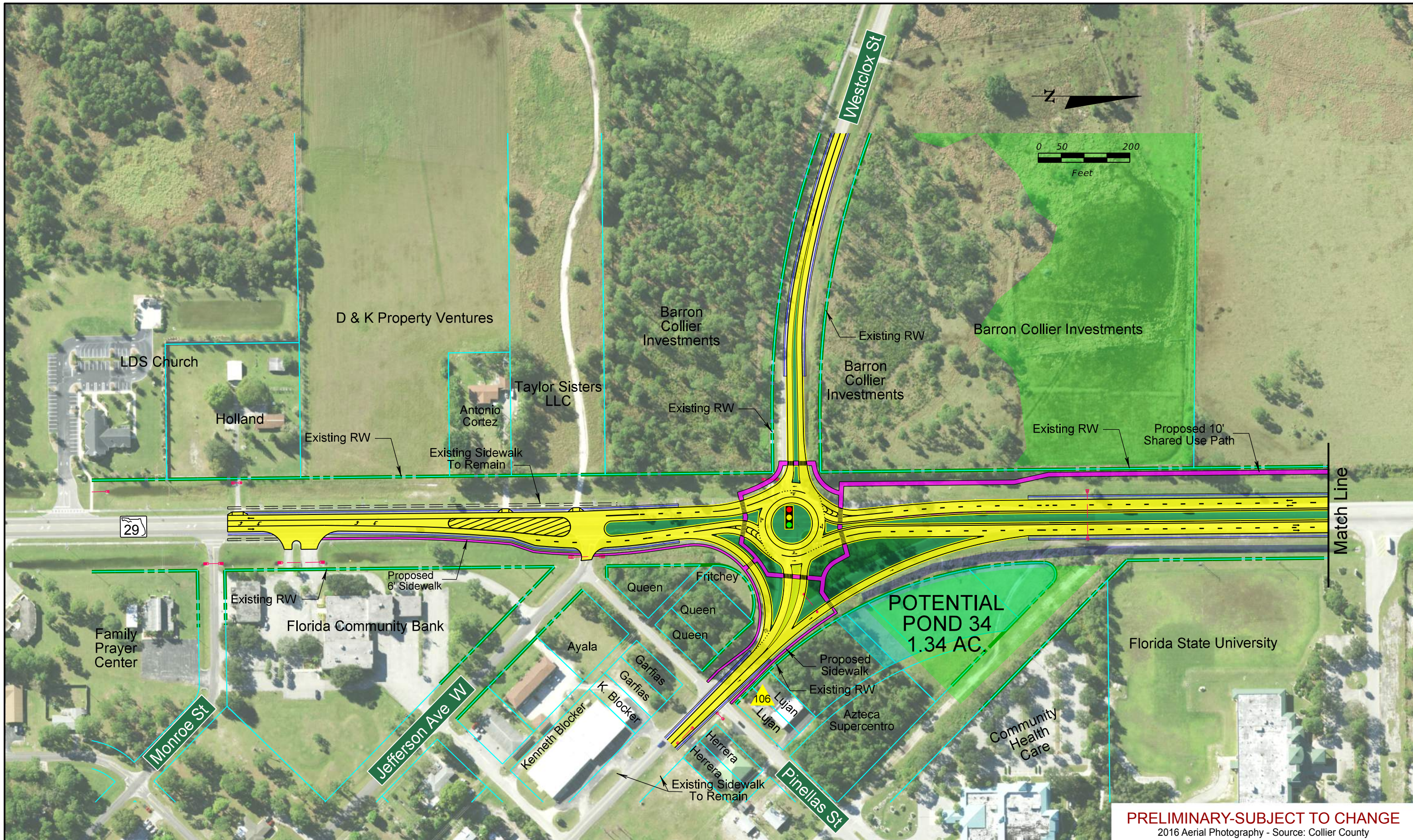
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend	
Existing Right-of-Way	Wetland
Parcels	Potential Pond
Proposed Right-of-Way	Potential Flood Plain Compensation
Water/Canal	Traffic Signal
Seminole Land	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

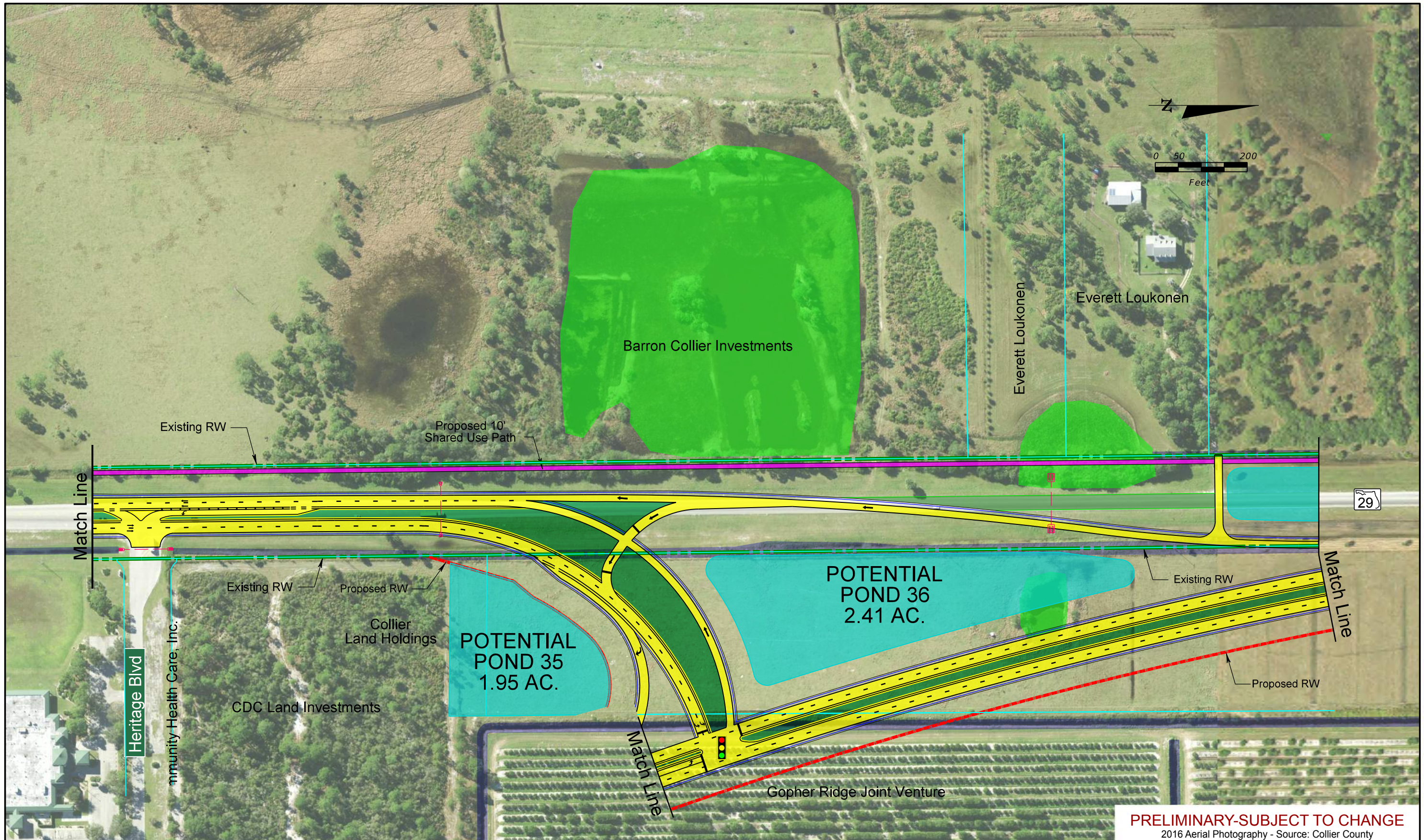
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Proposed Pavement
	Seminole Land		Proposed Median/Border
	Existing Sidewalk To Remain		Proposed Sidewalks
	Proposed 6' Sidewalk		Proposed Structure
	Existing RW		Existing Drainage Structure
	Existing RW		Proposed Guardrail
	Existing RW		Potential Business Relocation
	Existing RW		Potential Contamination (Low)
	Existing RW		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

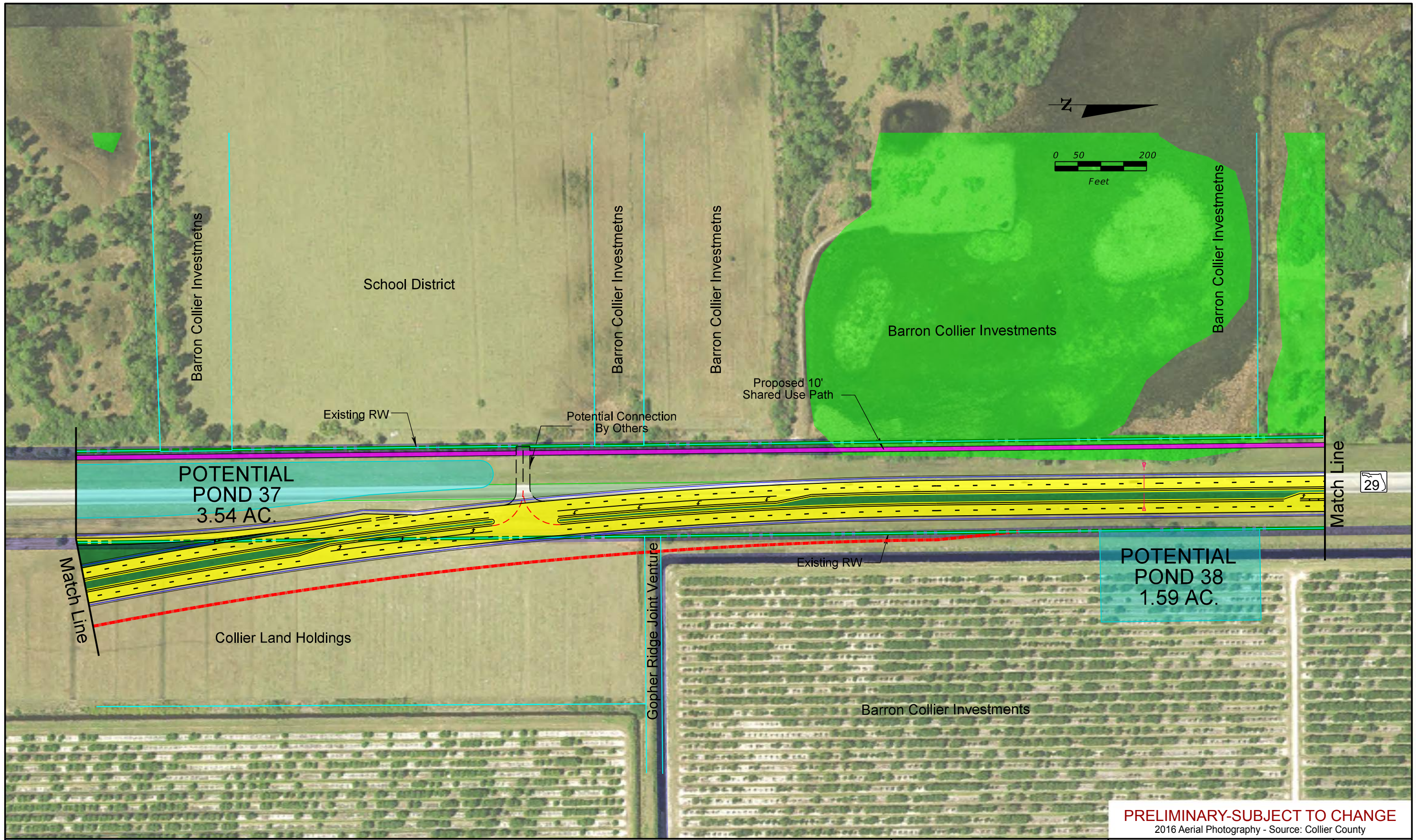
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend	
Existing Right-of-Way	Wetland
Parcels	Potential Pond
Proposed Right-of-Way	Potential Flood Plain Compensation
Water/Canal	Traffic Signal
Seminole Land	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks
	Proposed Structure
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Traffic Signal
	Seminole Land		Proposed Pavement
	Proposed Median/Border		Proposed Sidewalks
	Proposed Structure		Existing Drainage Structure
	Proposed Guardrail		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

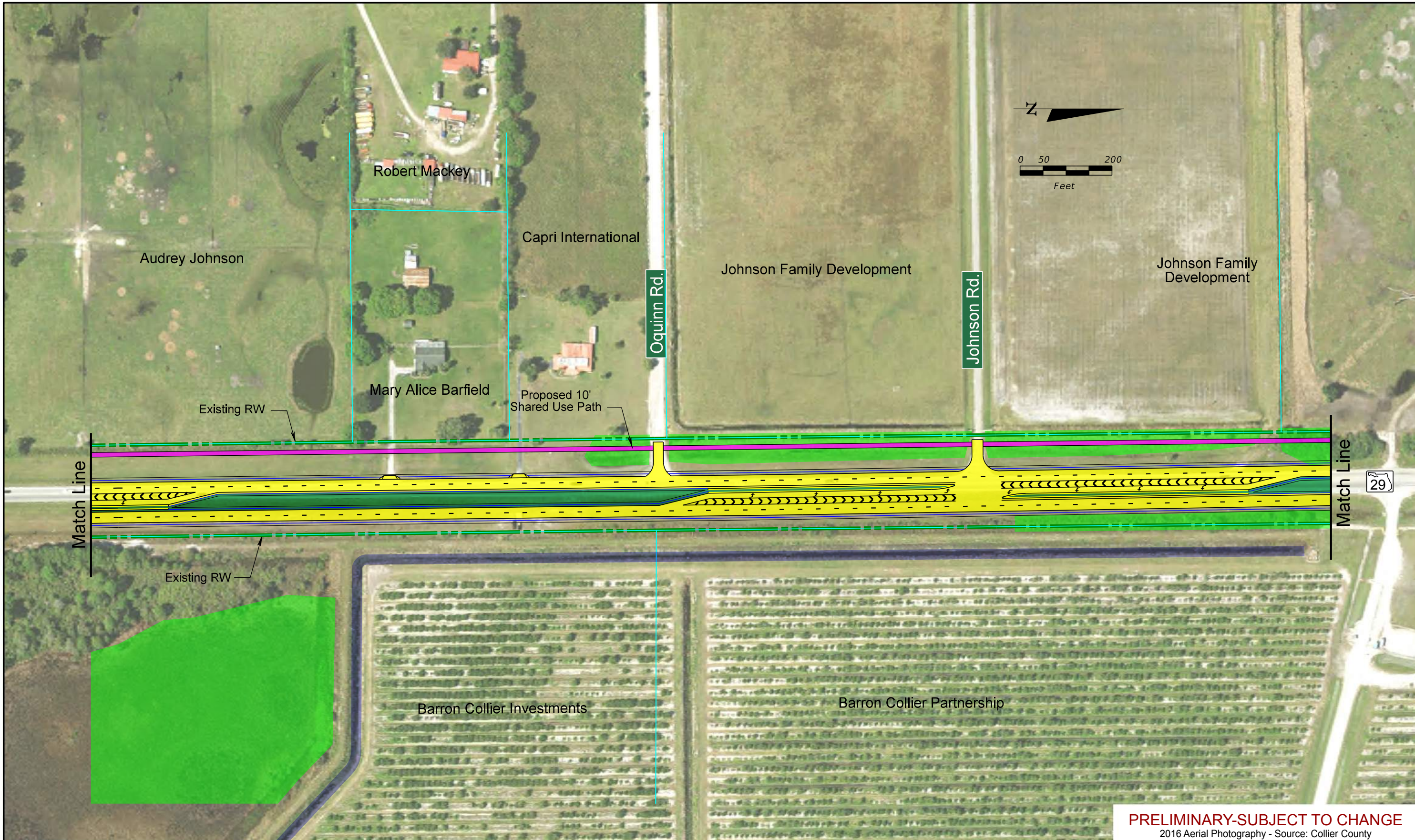
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Proposed Pavement
	Seminole Land		Proposed Medlan/Border
	Existing Drainage Structure		Proposed Sidewalks
	Traffic Signal		Proposed Structure
	Proposed Guardrail		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

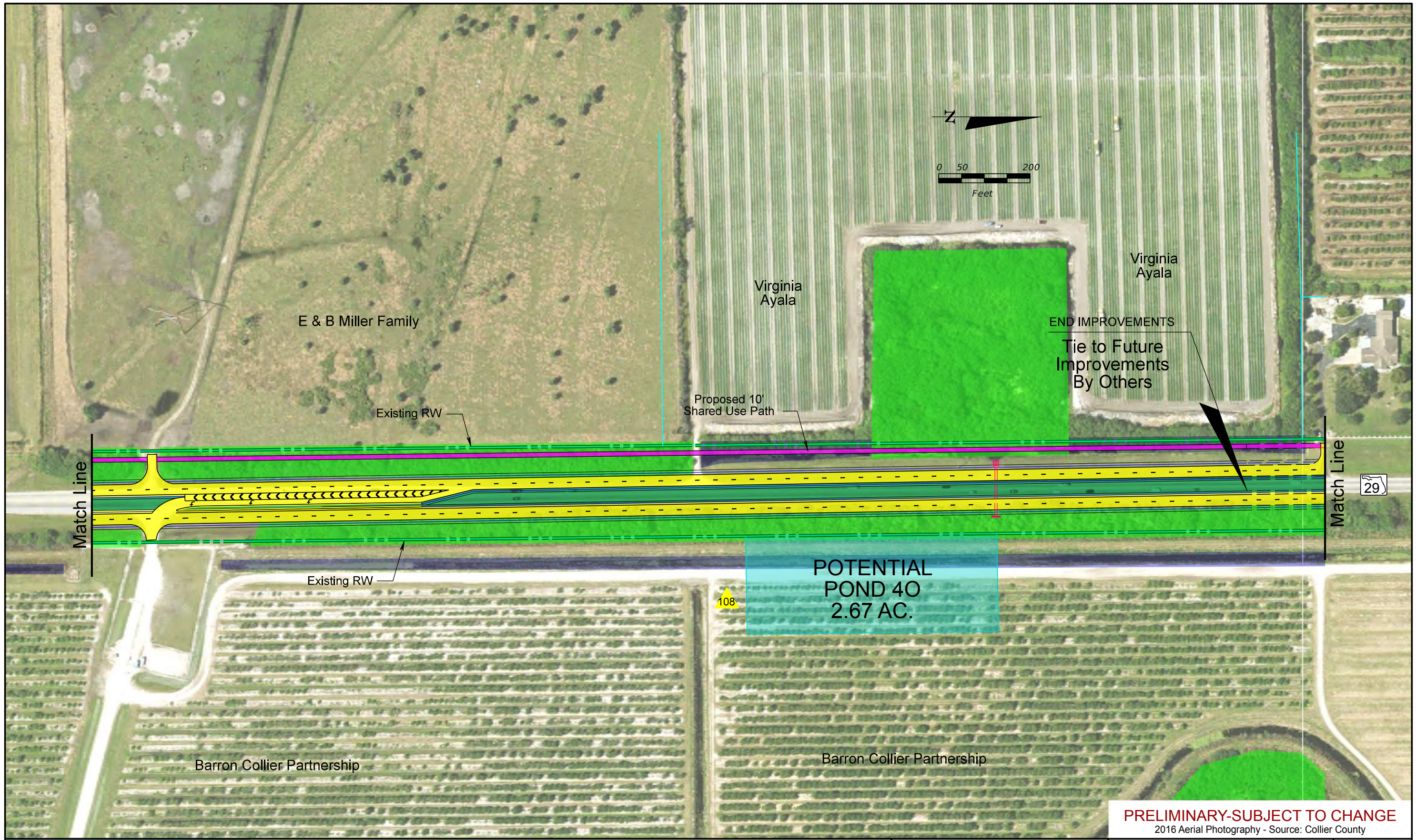
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022P

Legend			
	Existing Right-of-Way		Proposed Pavement
	Parcels		Proposed Median/Border
	Proposed Right-of-Way		Proposed Sidewalks
	Water/Canal		Proposed Structure
	Seminole Land		Existing Drainage Structure
	Wetland		Proposed Guardrail
	Potential Pond		Potential Business Relocation
	Potential Flood Plain Compensation		Potential Contamination (Low)
	Traffic Signal		Potential Contamination (Medium or High)

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend			
	Existing Right-of-Way		Wetland
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Flood Plain Compensation
	Water/Canal		Proposed Pavement
	Seminole Land		Proposed Median/Border
	Traffic Signal		Proposed Structure
	Existing Drainage Structure		Proposed Guardrail
	Potential Business Relocation		Potential Contamination (Low)
	Potential Contamination (Medium or High)		

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PRELIMINARY-SUBJECT TO CHANGE
 2016 Aerial Photography - Source: Collier County

SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3811 022P

Legend	
Existing Right-of-Way	Wetland
Parcels	Potential Pond
Proposed Right-of-Way	Potential Flood Plain Compensation
Water/Canal	Traffic Signal
Seminole Land	Proposed Pavement
Proposed Median/Border	Proposed Sidewalks
Proposed Structure	Existing Drainage Structure
Proposed Guardrail	Potential Business Relocation
Potential Contamination (Low)	Potential Contamination (Medium or High)

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APPENDIX D

NRCS Farmlands Determination

**FARMLAND CONVERSION IMPACT RATING
FOR CORRIDOR TYPE PROJECTS**

PART I (To be completed by Federal Agency)		3. Date of Land Evaluation Request 7/12/18	4. Sheet 1 of 1		
1. Name of Project SR 29 Immokalee PD&E Study, 417540-1-22		5. Federal Agency Involved FDOT OEM (pursuant to MOU with FHWA)			
2. Type of Project Widen 2-4 Lanes & New 4-Lane Bypass		6. County and State Collier County, Florida			
PART II (To be completed by NRCS)		1. Date Request Received by NRCS 7/16/18	2. Person Completing Form LeRoy Crockett		
3. Does the corridor contain prime, unique statewide or local important farmland? (If no, the FPPA does not apply - Do not complete additional parts of this form). YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>		4. Acres Irrigated Average Farm Size 26412 387			
5. Major Crop(s) Citrus, vegetables	6. Farmable Land in Government Jurisdiction Acres: 93360 % 7.2		7. Amount of Farmland As Defined in FPPA Acres: 51,951 % .401		
8. Name Of Land Evaluation System Used Soil Potential Rating	9. Name of Local Site Assessment System None		10. Date Land Evaluation Returned by NRCS 7/24/18		
PART III (To be completed by Federal Agency)		Alternative Corridor For Segment			
		Corridor A	Corridor B	Corridor C	Corridor D
A. Total Acres To Be Converted Directly		135.9	160.5	N/A	N/A
B. Total Acres To Be Converted Indirectly, Or To Receive Services		0.0	0.0		
C. Total Acres In Corridor		445.8	459.5		
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland		444	530		
B. Total Acres Statewide And Local Important Farmland		0.0	0.0		
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted		0.0047	0.0057		
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value		21.9	14.3		
PART V (To be completed by NRCS) Land Evaluation Information Criterion Relative value of Farmland to Be Serviced or Converted (Scale of 0 - 100 Points)		54.9	54.9		
PART VI (To be completed by Federal Agency) Corridor Assessment Criteria (These criteria are explained in 7 CFR 658.5(c))		Maximum Points			
1. Area in Nonurban Use		15	12	12	
2. Perimeter in Nonurban Use		10	7	7	
3. Percent Of Corridor Being Farmed		20	10	10	
4. Protection Provided By State And Local Government		20	20	20	
5. Size of Present Farm Unit Compared To Average		10	10	10	
6. Creation Of Nonfarmable Farmland		25	2	2	
7. Availability Of Farm Support Services		5	5	5	
8. On-Farm Investments		20	8	10	
9. Effects Of Conversion On Farm Support Services		25	0	0	
10. Compatibility With Existing Agricultural Use		10	1	1	
TOTAL CORRIDOR ASSESSMENT POINTS		160	75	77	0 0
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		100	54.9	54.9	0 0
Total Corridor Assessment (From Part VI above or a local site assessment)		160	75	77	0 0
TOTAL POINTS (Total of above 2 lines)		260	129.9	131.9	0 0
1. Corridor Selected: Corridor B (ALT C2)	2. Total Acres of Farmlands to be Converted by Project: 160.5 Acres	3. Date Of Selection: 8/14/18		4. Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>	
5. Reason For Selection: Corridor B (Alt C2) incorporates slightly more farmlands, but avoids direct impacts to the State Farmer's Market located in Immokalee, FL, as well as multiple residential and commercial relocations that would result from the development of Corridor A (Alt R1).					
Signature of Person Completing this Part Gwen Pipkin, FDOT				DATE 8/14/18	

NOTE: Complete a form for each segment with more than one Alternate Corridor

APPENDIX E

Section 4(f) Resources

DOAs

From: Linda.Anderson@dot.gov [<mailto:Linda.Anderson@dot.gov>]
Sent: Thursday, June 06, 2013 5:34 PM
To: James, Jeffrey W; Schulz, Mark
Cc: Benito.Cunill@dot.gov; BSB.Murthy@dot.gov
Subject: FHWA's Determination re Section 4(f) Applicability for Properties Adjacent to Proposed Alternatives for SR 29 (Immokalee) EIS, FPID # 417540-1-22-01

FHWA has reviewed the Section 4(f) DOA for SR 29 (Immokalee) EIS, FPID # 417540-1-22-01, and made the determination that Immokalee Airport Park, 1st Street Plaza, and 9th Street Plaza are Section 4(f) properties.

Whether the Collier Rural Land Stewardship Sending Area #5 is a Section 4(f) property is a more complex question, given its designated use for both conservation and ranching, and the nature of the Stewardship Easement Agreement between Collier County, FDOT, FDACS, and the property owner.

There are two issues here:

1. Does the land have a designated function as a wildlife or waterfowl refuge. Page 2, #'s 3A and B of the Stewardship Easement Agreement (p. A-7 of DOA) state that the land may be used for "Conservation, Restoration, and Natural Resources Uses" and "Agriculture." The Land Use Matrix on P. A-19 of the DOA defines "Conservation, Restoration and Natural Resources" as "Wildlife management, plant and wildlife conservancies, refuges and sanctuaries." Page 2-1, #1 of the DOA states "those areas within SSAs designated exclusively for conservation use are the only areas considered to fall under the auspices of Section 4(f). Note: the limitation of applicability of Section 4(f) to the areas of the SSA supporting conservation is based on 23 CFR 774.11(d)." However, 23 CFR 774.11(d) does not state that lands have to be "designated exclusively for conservation," only that they have to be "designated in the plans of the administering agency as being for, significant park, recreation, or wildlife and waterfowl refuge purposes." The easement does not appear to designate specific areas within the western portion adjacent to East Alternative #1 for conservation or agriculture. The land may be used for either. Consequently, FHWA's opinion is that Eastern Alternative #1 may have a designated function as a wildlife or waterfowl refuge.
2. Does the easement make this public land? This depends on the nature of the easement as well as other factors (see Question 1B of the Section 4(f) Policy Paper) and is a difficult question that will require additional research.

FHWA's recommendation is that a Section 4(f) determination for Collier Rural Land Stewardship Sending Area #5 be postponed until it is apparent that East Alternative #1 will be retained as a viable alternative. If it is, then we can further explore the question of whether this is a Section 4(f) property.

Linda Anderson
Environmental Protection Specialist
Federal Highway Administration
545 John Knox Rd., Ste. 200
Tallahassee, FL 32303
P: 850-553-2226
F: 850-942-8308

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) DETERMINATION OF APPLICABILITY

650-050-45
Environmental
Management
06/17

Project Name:	State Road (SR) 29 from Oil Well Road to SR 82				
FM#:	417540-1-22-01	ETDM#:	3752	FAP#:	3911 022 P
Project Review Date:	6/14/2018				
FDOT District:	1				
County(ies):	Collier				

A DOA IS REQUIRED FOR EACH SECTION 4(f) PROPERTY AND PROPOSED ALTERNATIVE.

Project Description including Section 4(f) Specific Information:

SR 29 extends from south of Oil Well Road north to SR 82 in Collier County and is approximately 15.6 miles in length. Existing SR 29 will be widened from two lanes to four lanes from south of Oil Well Road to CR 846 (Airport Road) and from the central alignment connection north of Immokalee to SR 82. SR 29 is proposed to be on new alignment in the central segment from CR 846 (Airport Road) north to its reconnection at existing SR 29 north of Immokalee. One of the proposed alternatives, Central #2, will require approximately 2.44 acres from the Airport Viewing Area.

Type of Property

Check all that apply:

- Public Parks and Recreation Areas
- Wildlife and Waterfowl Refuges
- Historic Sites

Description of Property: The Immokalee Regional Airport is located northeast of the intersection of SR 29 and CR 846 (Airport Road). The Airport Viewing Area, owned by the Collier County Airport Authority, occupies the southwest corner of the airport property. See the exhibit included in Attachment 1. A Memorandum of Understanding (MOU) was entered into on April 26, 2011 between the Collier County Airport Authority and Collier County, operating through its Parks and Recreation Department. See Attachment 3. The MOU establishes the primary use of the property as one supporting airport operations and consents to the Collier County Parks and Recreation Department use of the Airport Viewing Area for passive recreational purposes and for attendance by large group activities, such as outdoor concerts, festivals, charitable functions, etc.. However, the MOU establishes that the Collier County Airport Authority maintains control and the regulated use occurs on an "as needed basis". The MOU establishes a process by which the Airport Viewing Area may be used and prohibits the placement or installation of any permanent building, trees, structure or fixtures. It does allow for sidewalks and/or bicycle pathways, park benches and picnic tables. It is also stated in the MOU that the Collier County Airport Authority may terminate the agreement upon thirty (30) days written notice and return the Airport Viewing Area to airport use.

Criteria of Selected Property Type(s):

Public Parks and Recreation Areas

- o Must be publicly owned which refers to ownership by local, state or federal government
 - Ownership can also include permanent easements and long-term lease agreements
- o Must be open to the public during normal hours of operation
- o The major purpose must be for park or recreation activities
- o Must be designated or function as a significant park or recreational area.
 - Applies to the entire park or recreation area not just a specific feature

Wildlife and Waterfowl Refuge

- o Must be publicly owned which refers to ownership by local, state or federal government;
 - Ownership can also include permanent easements and long-term lease agreements;

FLORIDA DEPARTMENT OF TRANSPORTATION
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650-050-45
Environmental
Management
06/17

- Must be open to the public but **refuges are able to restrict access for the protection of refuge habitat and species;**
- The major purpose must be for wildlife and waterfowl refuges;
- Must be designated or function as a significant as a wildlife and waterfowl refuges; -
 - Applies to the entire wildlife and waterfowl refuges not just a specific feature

Historic Sites- includes historic buildings, historic transportation facilities, archeological sites, traditional cultural places, historic & archeological districts and historic trails.

- Must be of national, state or local significance and it must be eligible for listing or is listed on the National Register of Historic Places (NRHP); or
- If a site is determined not to be eligible OEM may determine that the application of Section 4(f) is otherwise appropriate when an official (such as the Mayor, president of a local historic society) provides information to support that the historic site is of local importance.

Does the identified resource meet all of the criteria for the selected property type?

Yes, continue to complete the form

No, STOP Section 4(f) does not apply

Identify the Official(s) with Jurisdiction (OWJ) contacted: Justin Lobb, Airports Manager, Collier County Airport Authority. Statement of Significance concurrence provide in Attachment 2.

Date correspondence sent to the OWJ: 6/1/2018

Has the Official(s) with Jurisdiction (OWJ) responded?

Yes No

Has the 30 day response period passed since the initial OWJ correspondence was sent?

Yes No

Please answer the questions below about the resource:

Note: A potential source for this information can include the property management plan, resource website and/or communications with the OWJ (be sure to document these communications in writing).

What is the size and location of the property (include a map of the resource)?

██████████

Who/what organization owns/manages the property?

██████████

What is the primary function (activities, features and attributes) within the meaning of Section 4(f) of the facility or property?

██████████

Please describe the location of available appurtenances and facilities (e.g. tennis courts, pools, shelter houses, sports fields, beaches) on the property:

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) DETERMINATION OF APPLICABILITY

650-050-45
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What is the function of/or the available activities on the property?

Access and Usage of the property by the Public:

Relationship to other similarly used lands/facilities in the vicinity:

Are there any unusual characteristics of the property that either limit or enhance the value of the resource? If so please explain:

Describe project activities that could potentially "use" the resource:

If applicable, give a general description of the history of the Historic Site, Archaeological Site or Historic District:

Based on the above information the recommended level of Section 4(f) evaluation for this property is:

Select the level of Section 4(f) evaluation: Choose an Item

Reason the selected level is appropriate:

Supporting Documentation

The following items **must** be attached to this form:

1. A map of the resource based on the guidelines in the PD&E Manual Part 2, Chapter 7, including the proposed alternative being evaluated.
2. Statement of Significance from OWJ or FDOT's presumption of significance.
3. Determination of Eligibility or Listing in the National Register of Historic Places, Archaeological Site (*include criterion of eligibility*) or a Historic District if applicable.

Signatures

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 14, 2016, and executed by FHWA and FDOT.

Signature: Adam Purcell, AECOM : 6/14/2018
Preparer Date

Signature: Gwen G. Pipkin : 6/15/2018
Environmental Manager, or designee Date

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) DETERMINATION OF APPLICABILITY

650-050-45
Environmental
Management
06/17

OEM
Concurrence:



6/26/2018

Signature:



Director of OEM, or designee

6/26/18
Date

APPENDIX F

***Archaeological and Historic Resources
Miccosukee PSSR Response and SHPO
Concurrence***

Preservation Board. Early consultation with the Seminole and Miccosukee Tribal Historic Preservation Officers (THPOs) should also be conducted prior to the survey to assist in the identification of archaeological probability areas or areas that may have cultural importance to the tribes. Consultation with both tribes should continue throughout the cultural resource investigation. Confidential: Review will not be displayed on Public Access website.

Based on the foregoing, a Summary DOE of Substantial has been assigned to the Historic and Archaeological Sites issue.

Commitments and Responses: Preparation of a Cultural Resource Assessment Survey (CRAS), as per FDOT Guidance and in coordination with the Seminole and Miccosukee Tribes, will be included in the project scoping recommendations.

Degree of Effect: 4 *Substantial* assigned 02/15/2008 by Sherry Anderson, FL Department of State

Coordination Document: No Selection

Coordination Document Comments: Given the existence of an eligible property within the 200 foot buffer and the presence of unevaluated resources within 100 feet, it is highly probable that project activities will impact historic properties potentially eligible for listing in the National Register of Historic Places, or otherwise of historical, architectural or archaeological value. Our office recommends a cultural resource assessment survey. Resources that have not been evaluated by our office should be updated and evaluated for potential eligibility.

Direct Effects

Identified Resources and Level of Importance:

ONLY PREVIOUSLY RECORDED RESOURCES WITHIN 500 FEET NOTED BELOW

Florida Site File Historic Bridges

NONE PREVIOUSLY RECORDED WITHIN 500 FEET

Historic Standing Structures

Buffer distance: 100 feet

CR00901 POLE BARN, ineligible by SHPO

Florida Site File Archaeological or Historic Sites

Buffer distance: 100 feet

CR00704 WILLIAMSON SITE 2 GLADES, 1000 B.C.-A.D. 1700 PREHISTORIC MOUND(S) NOT EVALUATED BY SHPO LIKELY NRHP ELIGIBLE

Buffer distance: 200 feet

CR00828 ARROWHEAD MIDDEN GLADES, 1000 B.C.-A.D. 1700 CAMPSITE (PREHISTORIC) POTENTIALLY ELIGIBLE FOR NRHP ELIGIBLE FOR NRHP

Buffer distance: 500 feet

CR00703 WILLIAMSON MOUND 1 LAND-TERRESTRIAL GLADES, 1000 B.C.-A.D. 1700 ELIGIBLE FOR NRHP POTENTIALLY ELIGIBLE FOR NRHP

Resource Groups

Buffer distance: 100 feet

BIG CORKSCREW ISLAND TRAM LINE, insufficient information by SHPO

Comments on Effects to Resources:

A portion of this project corridor was subject to a cultural resource assessment survey in 1982 and 1995. Several general surveys also overlap the project area.

Within the 200 foot buffer zone is a potentially eligible midden. A prehistoric mound, not evaluated by SHPO, is located within 100 feet. SHPO has also reviewed the Big Corkscrew Island Tram Line (within 100 feet) but determined there was insufficient information to evaluate this resource.

Recommended Avoidance, Minimization, and Mitigation Opportunities:

Additional Comments (optional):

Given the existence of an eligible property within the 200 foot buffer and the presence of unevaluated resources within 100 feet, it is highly probable that project activities will impact historic properties potentially eligible for listing in the National Register of Historic Places, or otherwise of historical, architectural or archaeological value. Our office recommends a cultural resource assessment survey. Resources that have not been evaluated by our office should be updated and evaluated for potential eligibility.

CLC Recommendations:

Degree of Effect: 4 *Substantial* assigned 01/08/2008 by Steve Terry, Miccosukee Tribe of Indians of Florida

Coordination Document: No Selection

Coordination Document Comments: If the Cultural Resources Survey shows there are no archaeological sites that will be impacted by this project, then no further consultation is necessary. However, if the Cultural Resources Survey does show that archaeological sites will be impacted by this project, then further consultation with the Miccosukee Tribe should be done.

Direct Effects

Identified Resources and Level of Importance:

There are 4 prehistoric sites found within 1,320' of this alternative, two of which are mounds. One mound is found within 100' of this alternative. A Cultural Resources Survey needs to be conducted to determine the impacts, if any, to these sites.

Comments on Effects to Resources:

Once a Cultural Resources Survey has been done, then effects, if any, to archaeological sites can be ascertained.

Recommended Avoidance, Minimization, and Mitigation Opportunities:

Additional Comments (optional):

If the Cultural Resources Survey shows there are no archaeological sites that will be impacted by this project, then no further consultation is necessary. However, if the Cultural Resources Survey does show that archaeological sites will be impacted by this project, then further consultation with the Miccosukee Tribe should be done.

CLC Recommendations:

The following organization(s) were expected to but did not submit a review of the Historic and Archaeological Sites issue for this alternative: Federal Highway Administration, Seminole Tribe of Florida

Recreation Areas

Project Effects

Coordinator Summary Degree of Effect: 3 *Moderate* assigned 04/17/2008 by FDOT District 1

Comments:

The FDEP noted the presence of several conservation lands within the vicinity of the project. Interested in preserving the functions and natural communities of these lands, the FDEP recommended that an evaluation be conducted of the primary, secondary, and cumulative impacts of the proposed roadway construction/widening on the identified public lands and proposed acquisition sites.

Based on the foregoing, a Summary DOE of Moderate has been assigned to the Recreation Areas issue.

Commitments and Responses: A Section 4(f) Determination of Applicability (DOA) will be required for this project.

Degree of Effect: 2 *Minimal* assigned 02/20/2008 by Lauren P. Milligan, FL Department of Environmental Protection

Coordination Document: No Selection

Direct Effects

Identified Resources and Level of Importance:

The following South Florida Water Management District-managed conservation lands and Florida Forever project lands are located within a mile of the corridor study area: Lake Trafford Impoundment, Corkscrew Regional Ecosystem Watershed and Corkscrew Regional Ecosystem Watershed Florida Forever BOT Project.

Comments on Effects to Resources:

These lands contain significant natural communities and numerous element occurrences of listed species, as indicated by the Florida Natural Areas Inventory. The Department is interested in preserving the area's natural communities, wildlife corridor functions, natural flood control, stormwater runoff filtering capabilities, aquifer recharge potential, contributions to regional spring complexes, and recreational trail opportunities. Therefore, future environmental documentation should include an evaluation of the primary, secondary, and cumulative impacts of the proposed highway construction on the above public lands and proposed acquisition sites.

Recommended Avoidance, Minimization, and Mitigation Opportunities:

Additional Comments (optional):

CLC Recommendations:

The following organization(s) were expected to but did not submit a review of the Recreation Areas issue for this alternative: Federal Highway Administration, National Park Service, South Florida Water Management District, US Environmental Protection Agency

Section 4(f) Potential

Project Effects

Coordinator Summary Degree of Effect: 3 *Moderate* assigned 04/18/2008 by FDOT District 1



Florida Department of Transportation

RICK SCOTT
GOVERNOR

801 North Broadway Avenue
Bartow, FL 33830

MIKE DEW
SECRETARY

July 11, 2018

Timothy A. Parsons, Ph.D., Director
State Historic Preservation Officer
Florida Division of Historical Resources
Florida Department of State
R.A. Gray Building
500 South Bronough Street
Tallahassee, Florida 32399-0250

2018 JUL 13 A 8:12
FLORIDA DEPARTMENT OF
STATE
BUREAU OF
HISTORIC PRESERVATION

Attention: Ms. Alyssa McManus, Transportation Compliance Review Program

Re: **Cultural Resource Assessment Survey
State Road 29 Project Development and Environment Study from Oil Well Road
(County Road 858) to State Road 82
Collier County, Florida
Financial Project ID No.: 417540-1-22-01**

Dear Dr. Parsons,

The Florida Department of Transportation (FDOT), District One, is pleased to submit the *Cultural Resource Assessment Survey (CRAS) for the State Road (SR) 29 Project Development and Environment (PD&E) Study from Oil Well Road (County Road [CR] 858) to SR 82 in Collier County, Florida*. Please find enclosed the following:

- One unbound copy of the CRAS report;
- One CD containing a .pdf of the CRAS report, an electronic version of the survey log and site file forms, selected photos, and GIS shapefiles of the survey area;
- One unbound copy of all site file forms, and
- One unbound survey log.

Also included is the Cultural Resources Desktop Analysis of Proposed Ponds and Floodplain Compensation Sites associated with the alternatives included in the CRAS. Please note that the objective of this desktop analysis is to provide preliminary cultural resource information to assist in the avoidance of previously recorded resources listed in, determined eligible for, or considered eligible for listing in the *National Register of Historic Places* (National Register). Once final ponds are selected, a cultural resource assessment of those ponds will be conducted.

Timothy A. Parsons, Ph.D.
SR 29 PD&E from Oil Well Road (CR 858) to SR 82
Collier County, Florida
Financial Project ID No.: 417540-1-22-01
July 11, 2018
Page 2 of 4

The CRAS was conducted in compliance with Section 106 of the *National Historic Preservation Act (NHPA) of 1966* (Public Law 89-665, as amended), as implemented by 36 Code of Federal Regulations (CFR) 800 -- *Protection of Historic Properties* (incorporating amendments effective August 5, 2004); Stipulation VII of the *Programmatic Agreement among the Federal Highway Administration (FHWA), the Advisory Council on Historic Preservation (ACHP), the Florida Division of Historical Resources (FDHR), the State Historic Preservation Officer (SHPO), and the FDOT Regarding Implementation of the Federal-Aid Highway Program in Florida* (Section 106 Programmatic Agreement, effective March 2016, amended June 7, 2017); the revised Chapter 267, *Florida Statutes (F.S.)*; and the standards embodied in the FDHR's *Cultural Resource Management Standards and Operational Manual* (February 2003), and Chapter 1A-46 (*Archaeological and Historical Report Standards and Guidelines*), *Florida Administrative Code*. In addition, this report was prepared in conformity with standards set forth in Part 2, Chapter 8 (*Archaeological and Historical Resources*) of the *FDOT Project Development and Environment Manual* (effective June 14, 2017). The objective of the CRAS was to identify cultural resources within the project area of potential effect (APE) and assess the resources in terms of their eligibility for listing in the *National Register of Historic Places* (National Register) according to the criteria set forth in 36 CFR Section 60.4.

No previously recorded or newly recorded archaeological sites were identified during the archaeological resources survey. The historic resources survey resulted in the identification of a total of 46 historic resources within the historic resources APE. This includes two previously recorded resources and 44 newly recorded resources. The previously recorded resources include the Immokalee Ice Plant (8CR642) and the Immokalee Regional Airport (8CR1087). The 44 newly recorded resources include 35 buildings (8CR1180–8CR1196, 8CR1236–8CR1238, 8CR1245–8CR1246, 8CR1323–8CR1329, 8CR1331–8CR1334, 8CR1369–8CR1370), two bridges (8CR1496–8CR1497), four canals (8CR1256, 8CR1368, 8CR1498–8CR1499), one road (8CR1309) and two resource groups (8CR1252 and CR1500).

Forty-five of the resources are considered ineligible for listing in the National Register either individually or as part of a historic district. One resource, the Immokalee Ice Plant (8CR642) is considered National Register-eligible. The Ice Plant was constructed in 1945 and, although there have been several additions, it maintains much of its integrity. This resource is representative of Immokalee's conversion from a community of individual isolated farmsteads to a more modern agricultural community and is considered eligible for the National Register under Criterion A for its role in Immokalee's Community Planning and Development, Agriculture, and Industry.

A webinar was held on June 20, 2018 with Alyssa McManus of the SHPO/FDHR Transportation Compliance Review Program, FDOT District 1, and the consultant team to provide an overview of the results of the CRAS and discuss the potential effects of the project on the potentially eligible Immokalee Ice Plant. The level of documentation needed to determine the effects to the Ice Plant were also discussed. Ms. McManus noted that it appeared there would be no adverse effect to the Ice Plant and agreed that the effects analysis could be included in this CRAS transmittal letter.

Timothy A. Parsons, Ph.D.
SR 29 PD&E from Oil Well Road (CR 858) to SR 82
Collier County, Florida
Financial Project ID No.: 417540-1-22-01
July 11, 2018
Page 3 of 4

The Criteria of Adverse Effects, as defined in the Section 106 implementing regulations, 36 CFR part 800.5, states:

An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Consideration shall be given to all qualifying characteristics of a historic property, including those that may have been identified subsequent to the original evaluation of the property's eligibility for the National Register. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance, or be cumulative.

Neither of the proposed alternatives included any acquisition of property from the Ice Plant parcel. The proposed at-grade roadway improvements on SR 29 adjacent to the National Register-eligible Immokalee Ice Plant will fall entirely within the existing ROW and will match the existing roadway typical section (Attachment 1). The existing typical section includes two 12-foot lanes, concrete sidewalks and bike lanes in each direction separated by a raised median. The existing driveway access to the Ice Plant will remain. Improvements along SR 29, west of New Market Road, are limited to milling and resurfacing of the existing pavement in order to transition the proposed improvements to the existing roadway. None of the proposed improvements directly or indirectly impact the Ice Plant or diminish its integrity. Therefore, based on the criteria of adverse effect, the proposed project will not adversely affect those characteristics of the Immokalee Ice Plant that qualify this resource for listing in the National Register.

This letter and the enclosed CRAS report are respectfully provided for your review and concurrence with both the determinations of eligibility and the effects determination. This information is being provided in accordance with provisions contained in *Section 106 of the National Historic Preservation Act*. If you have any questions, please do not hesitate to call me at (863) 519-2375 or Gwen.Pipkin@dot.state.fl.us

Sincerely,



Gwen G. Pipkin
Environmental Manager

Timothy A. Parsons, Ph.D.
SR 29 PD&E from Oil Well Road (CR 858) to SR 82
Collier County, Florida
Financial Project ID No.: 417540-1-22-01
July 11, 2018
Page 4 of 4

Enclosures

Cc: Marlon Bizerra, FDOT
Jonathon Bennett, FDOT
Matthew Marino, FDOT
Roy Jackson, FDOT
Bill Howell, Lochner
Amy Streehman, Janus Research
Kathleen Hoffman, Janus Research

The Florida State Historic Preservation Officer finds the attached Cultural Resources Assessment Report complete and sufficient and concurs/ does not concur with the determinations of historic significance provided in this cover letter and does does not find applicable the determinations of effects and adverse effects provided in this cover letter for SHPO/FDHR Project File Number 2018-3480.

FDHR Comments:

CR 688 should be CR 668. Please correct and submit in report - files on disc. However, this office concurs w/ the determinations - findings of this report.

Timothy A. Parsons, Director, and
State Historic Preservation Officer

[DATE]

Florida Division of Historical Resources

APPENDIX G

Pond and Floodplain Compensation Site Maps






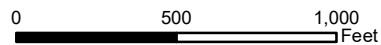
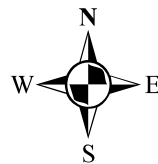
MAP SOURCE : ESRI
 DATA SOURCE : AECOM 2018

Pond and Floodplain Compensation Sites
SR 29 from Oil Well Road to SR 82
PD&E Study
Central Alternative #2

Date: 6/5/2018

Legend

-  Central Alternative #2
-  Floodplain Compensation Sites
-  Pond Sites








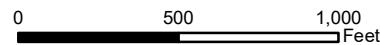
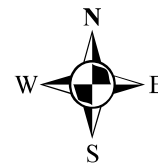
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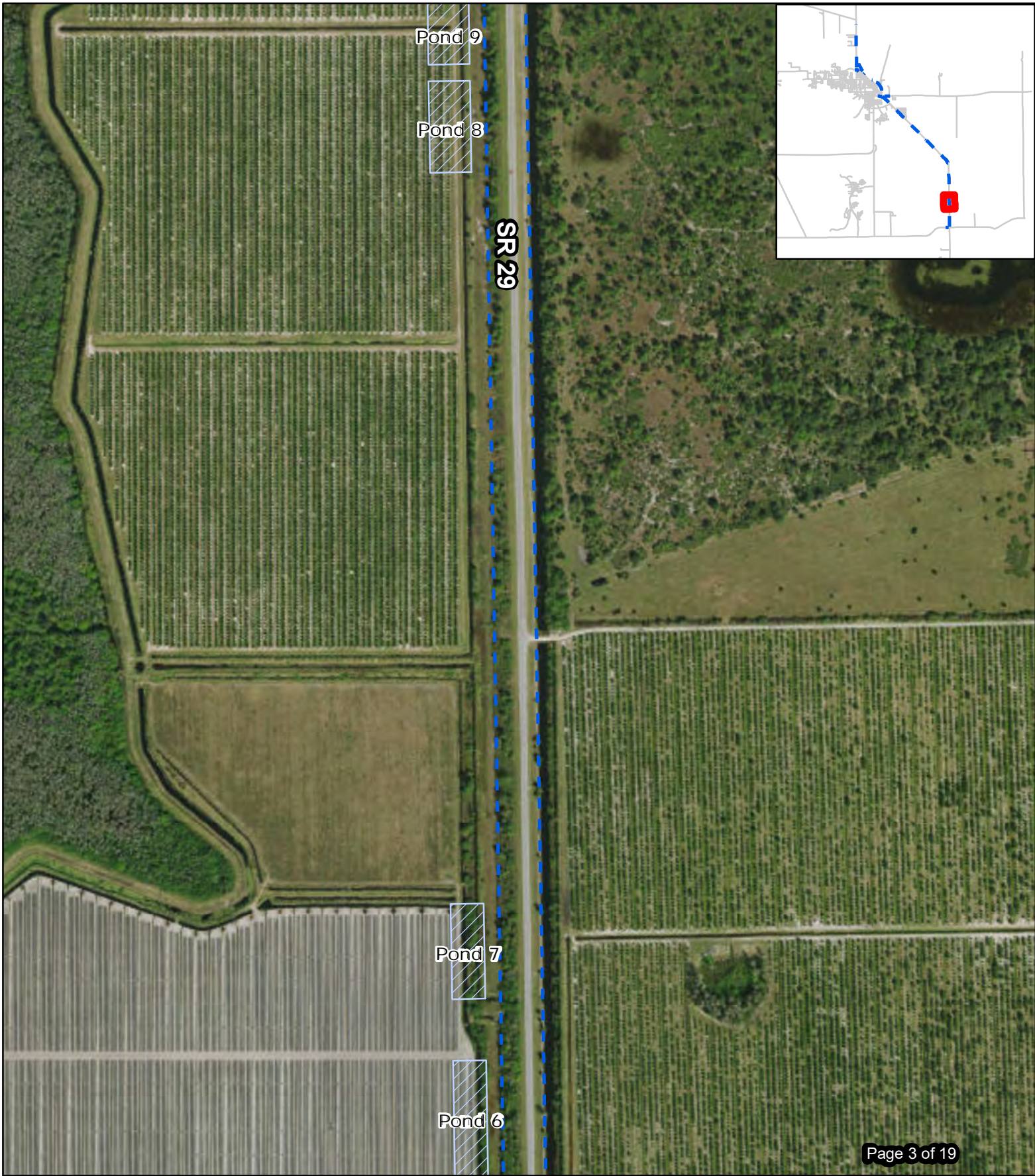
Pond and Floodplain Compensation Sites
SR 29 from Oil Well Road to SR 82
PD&E Study
Central Alternative #2

Date: 6/5/2018

Legend




-  Central Alternative #2
-  Floodplain Compensation Sites
-  Pond Sites






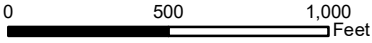
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 DATA SOURCE : AECOM 2018

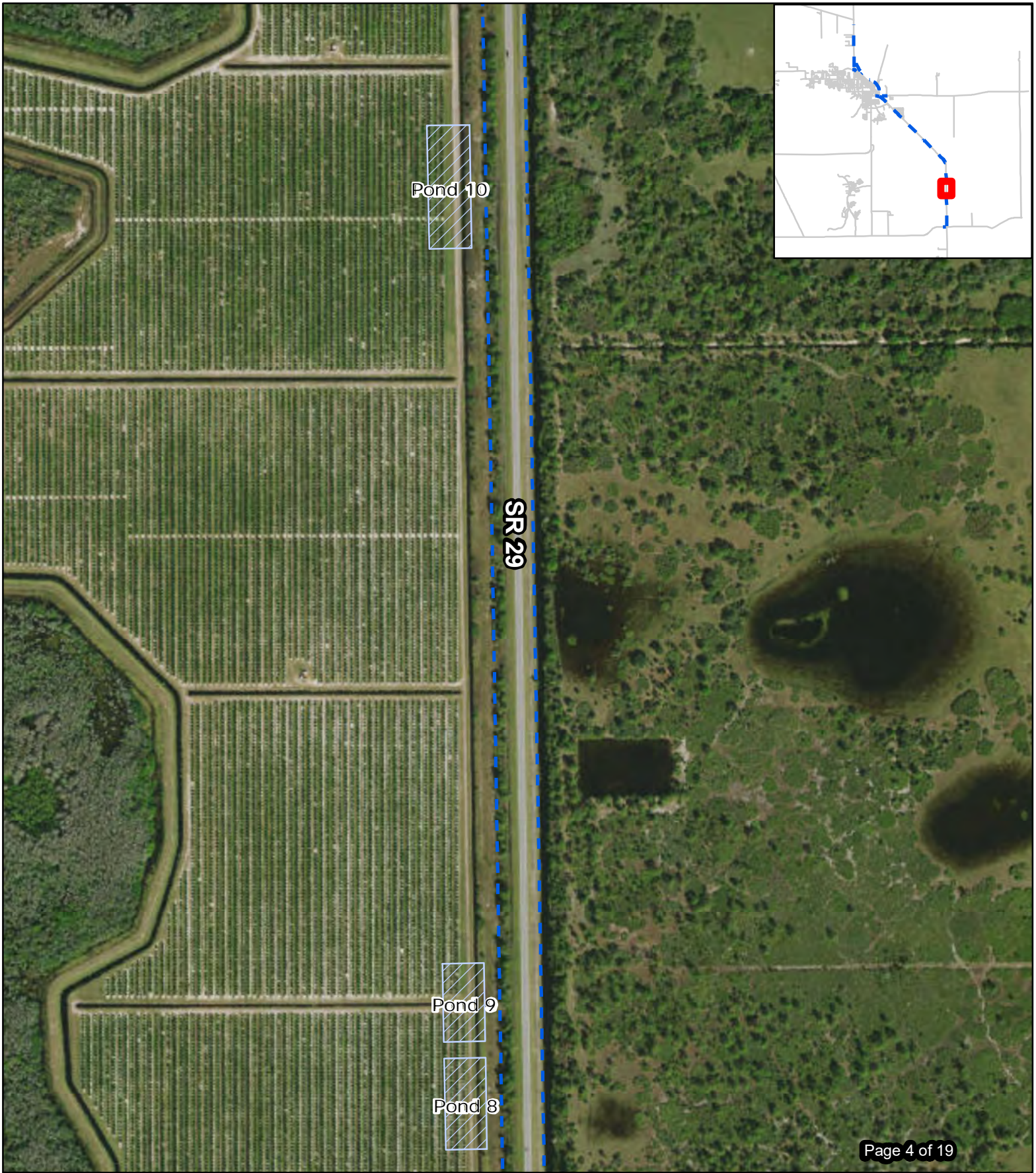
Legend

-  Central Alternative #2
-  Floodplain Compensation Sites
-  Pond Sites

Pond and Floodplain Compensation Sites
SR 29 from Oil Well Road to SR 82
PD&E Study
Central Alternative #2




Date: 6/5/2018




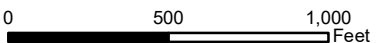
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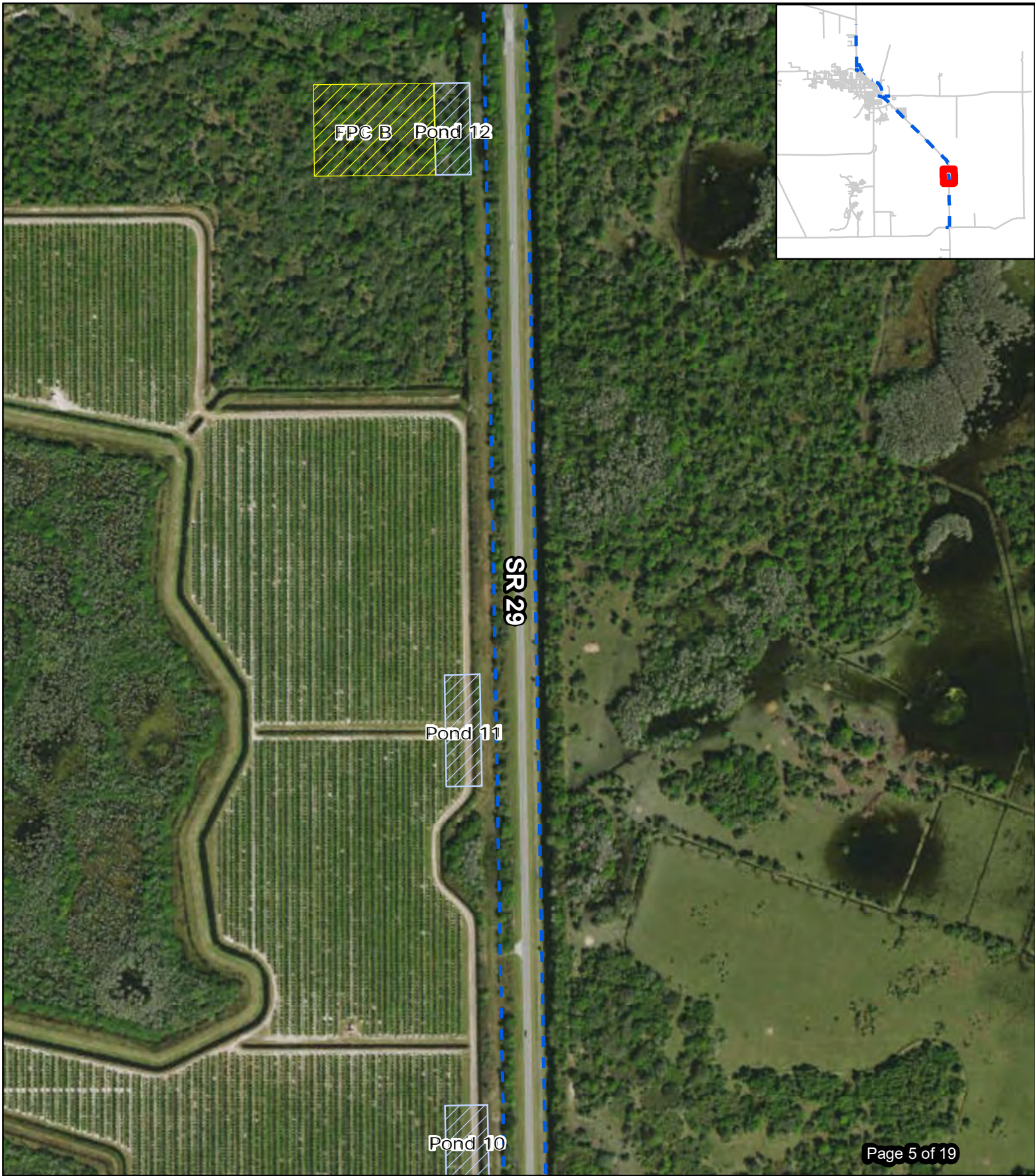
Legend

-  Central Alternative #2
-  Floodplain Compensation Sites
-  Pond Sites




Pond and Floodplain Compensation Sites
SR 29 from Oil Well Road to SR 82
PD&E Study
Central Alternative #2

Date: 6/5/2018

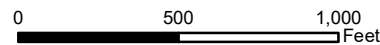
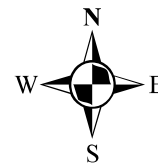


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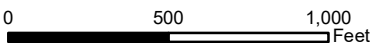
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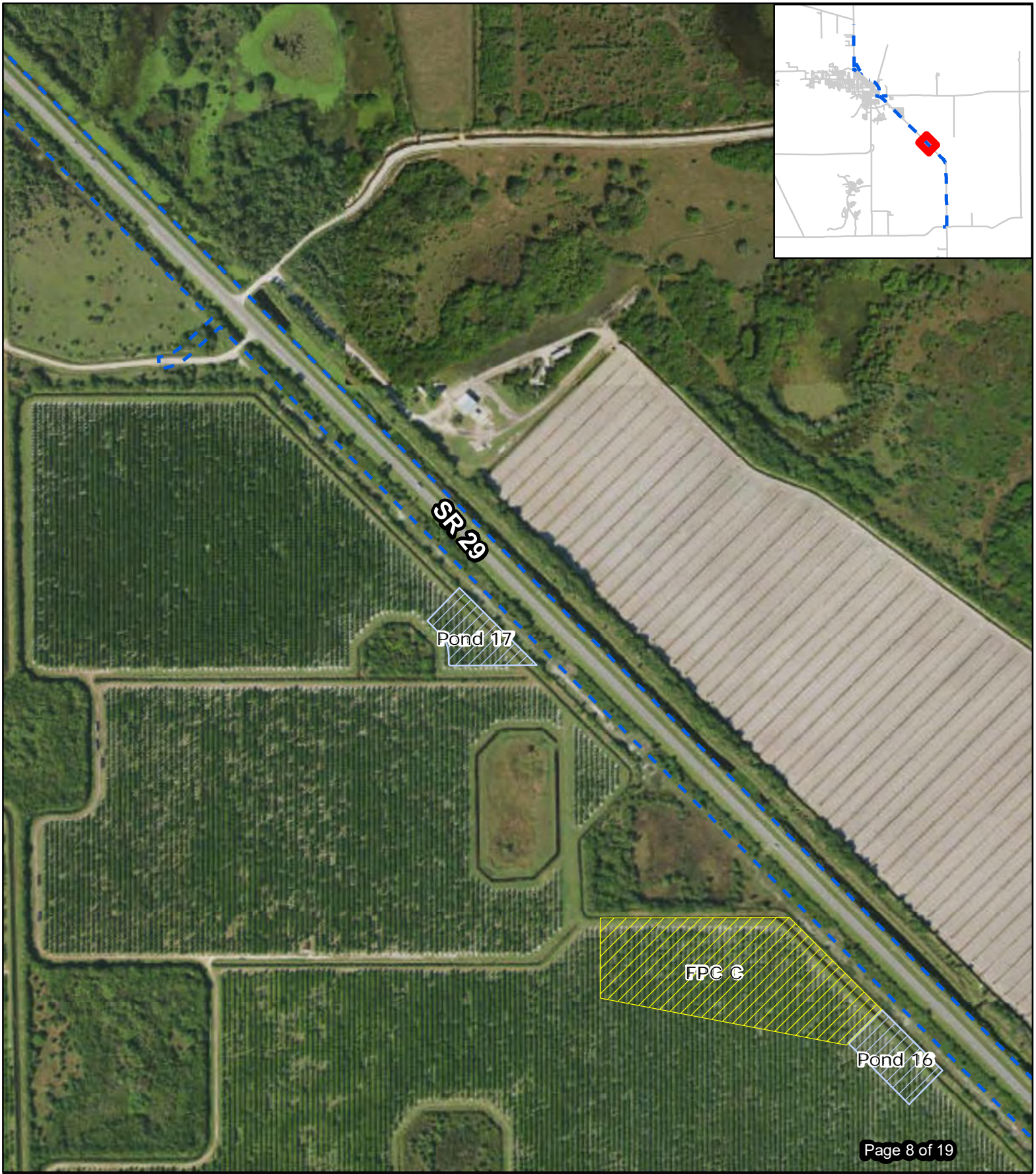
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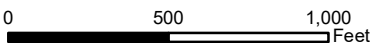
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Pond and Floodplain Compensation Sites
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PD&E Study
Central Alternative #2

Date: 6/5/2018


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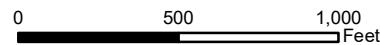
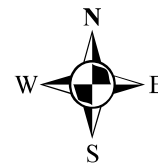
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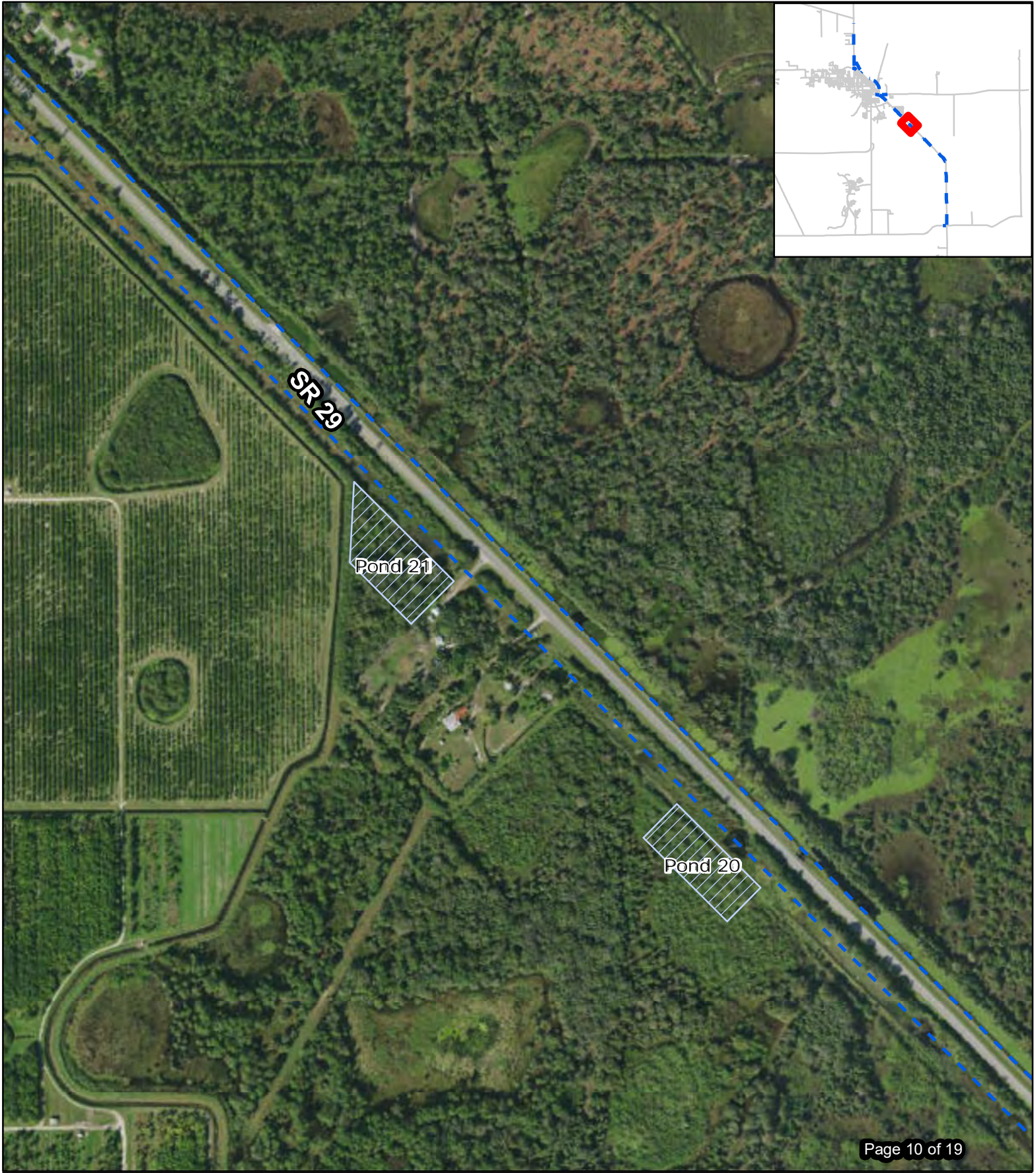
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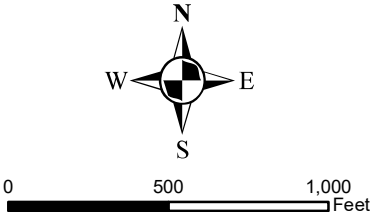
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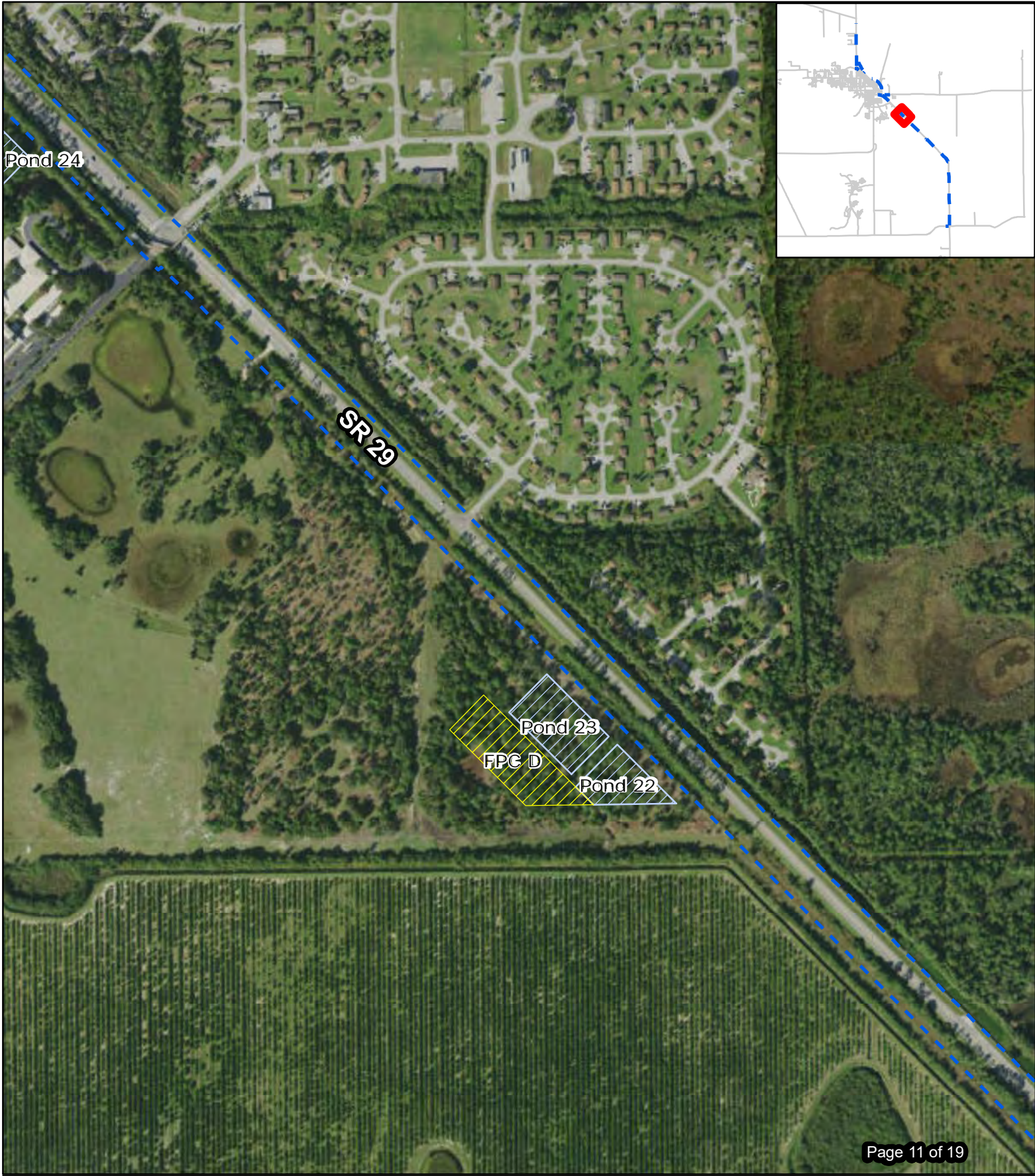
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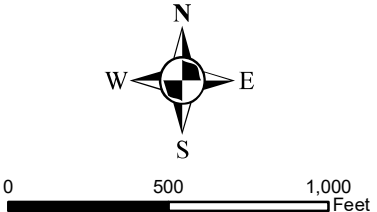
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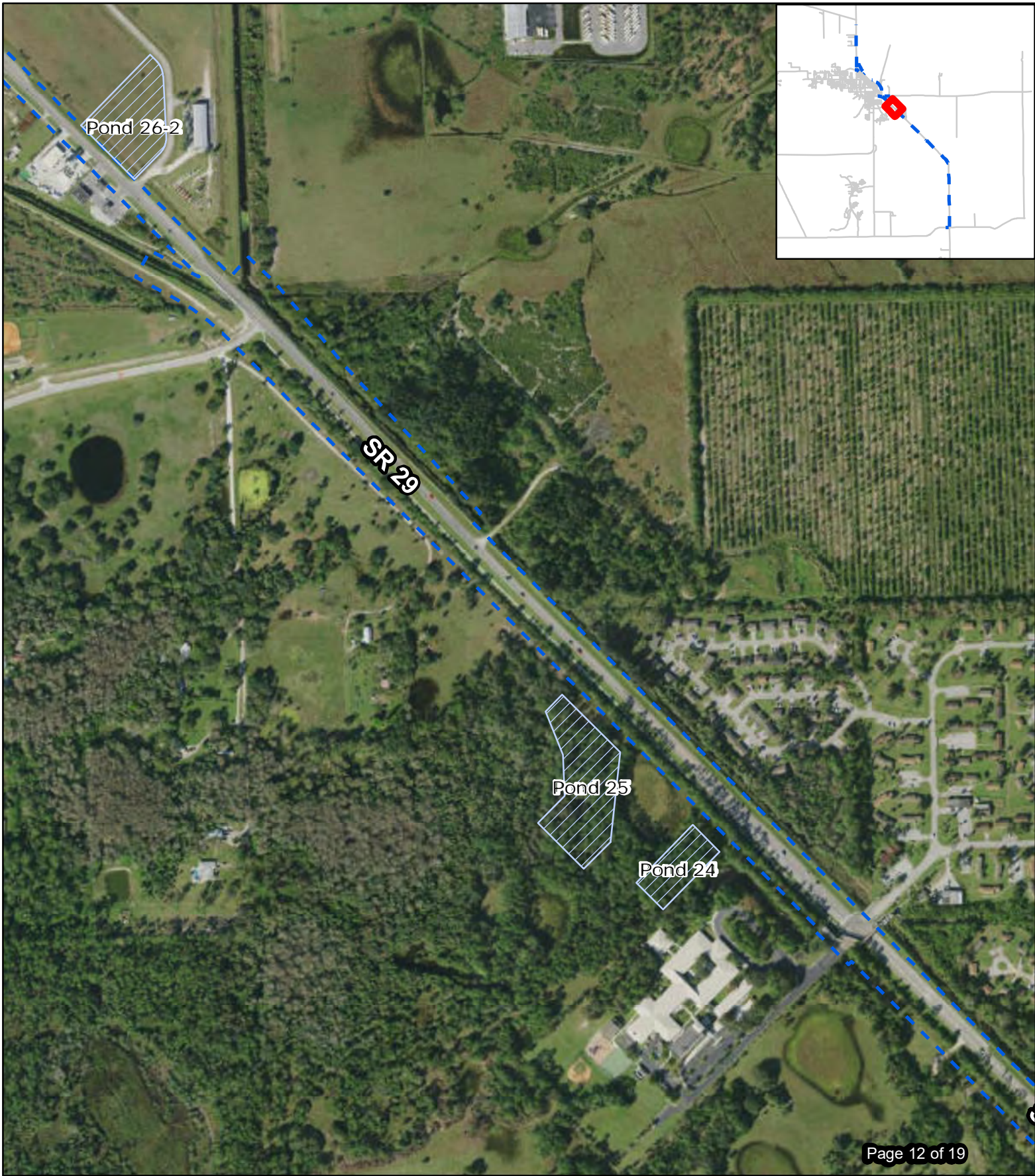
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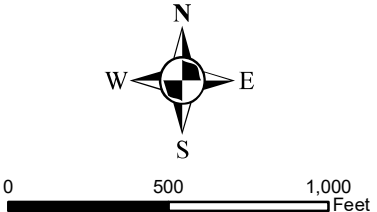
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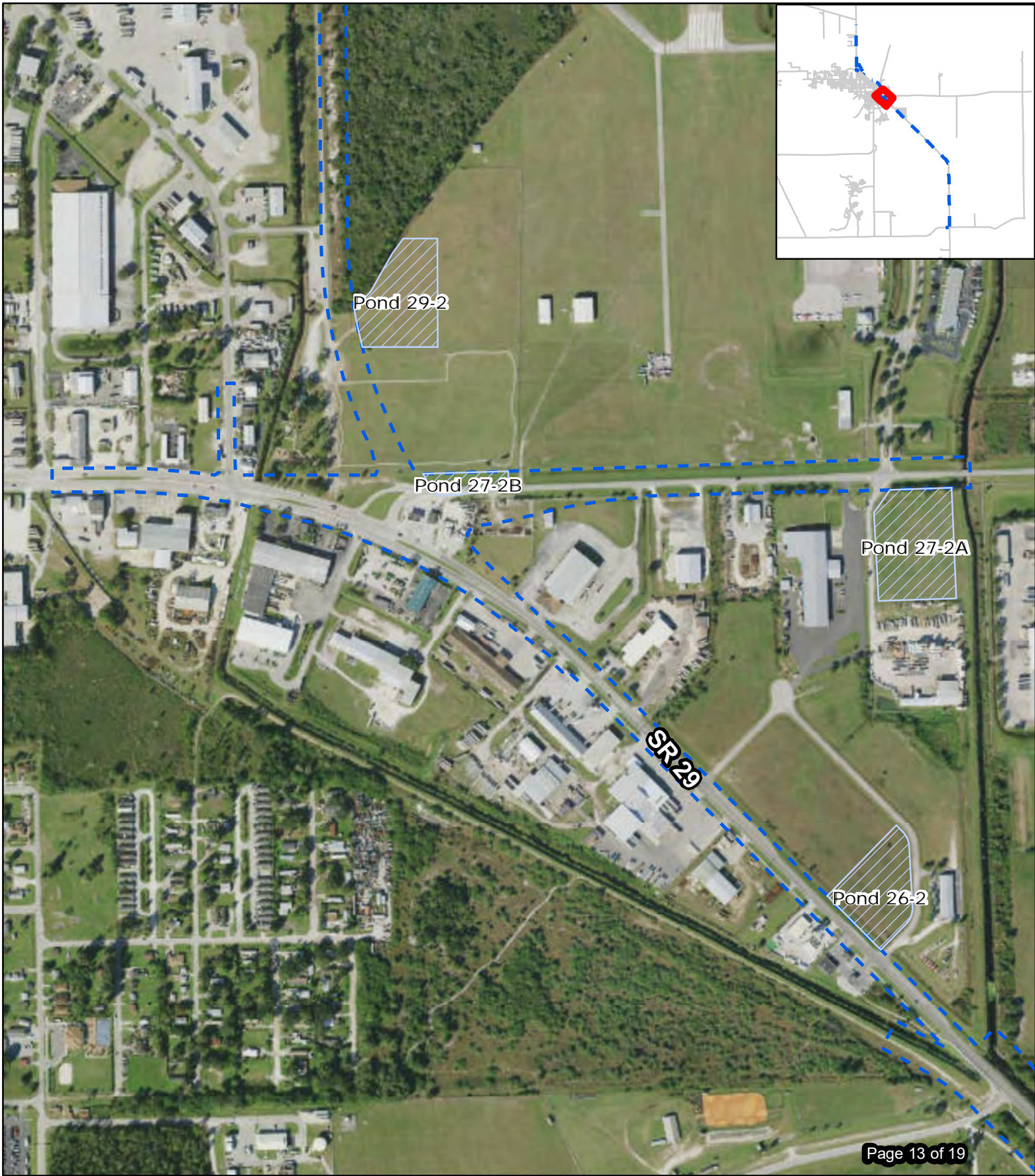
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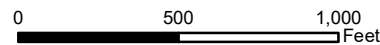
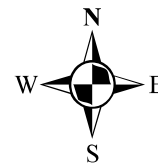


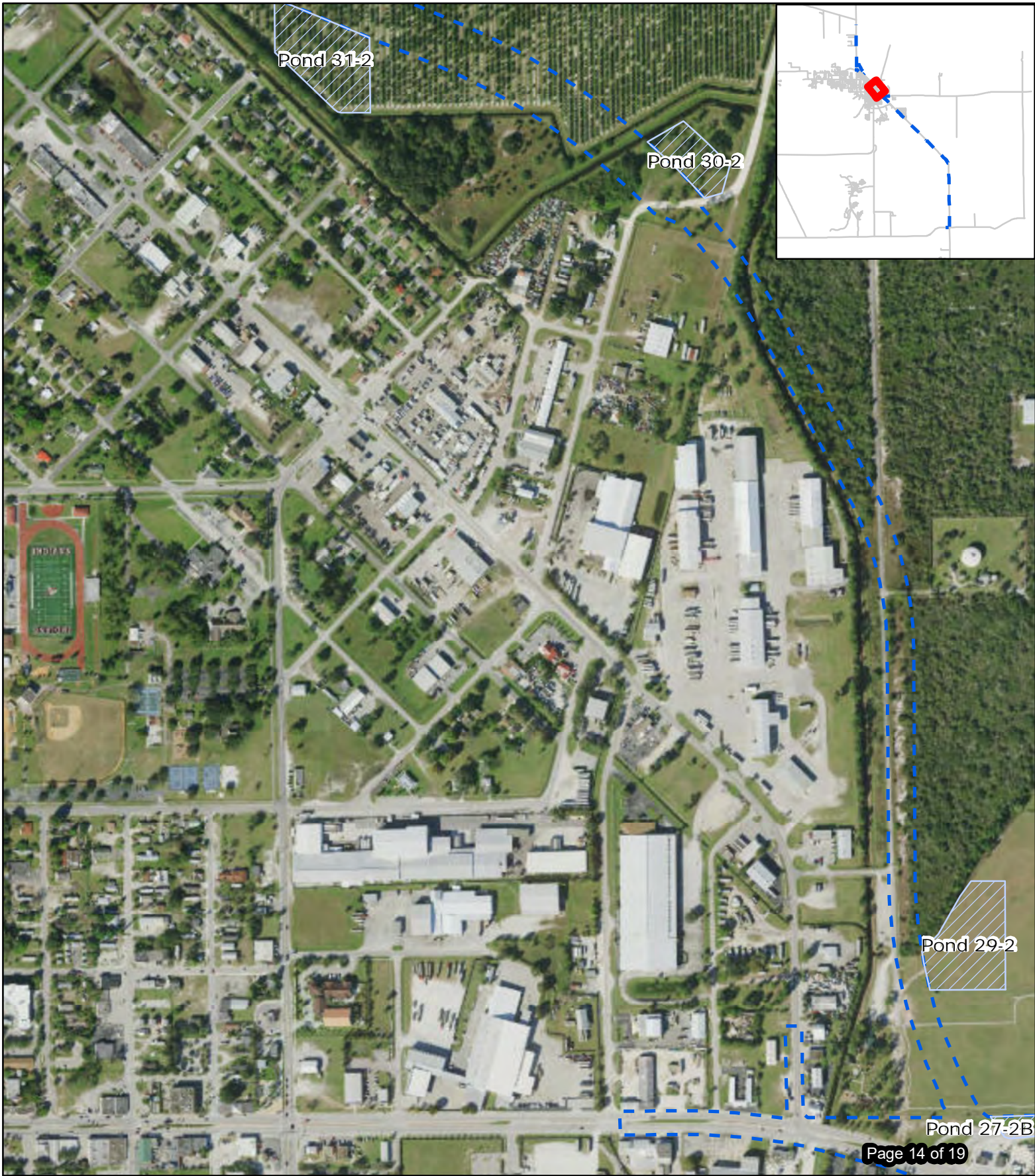
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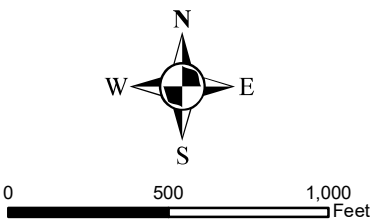


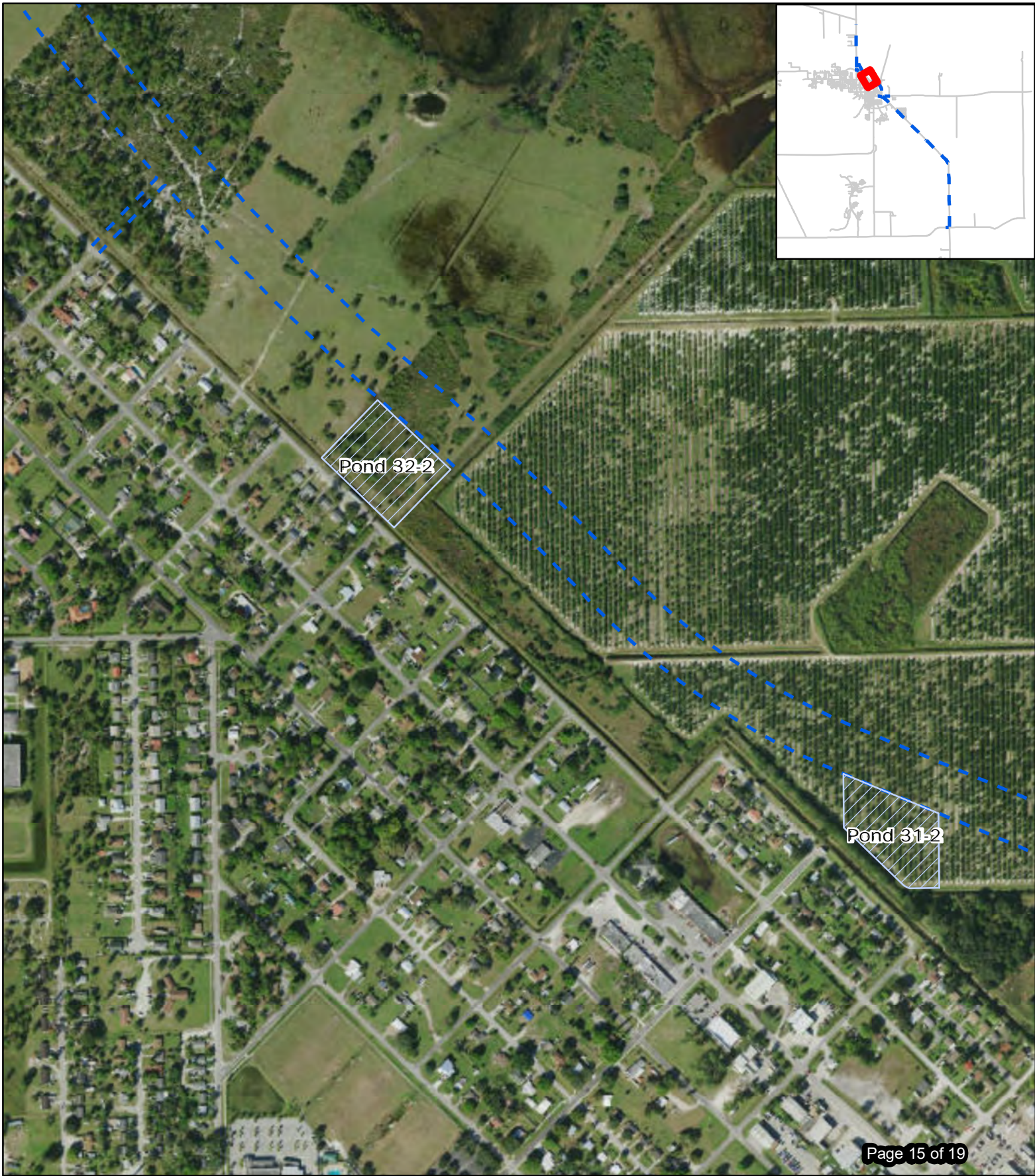
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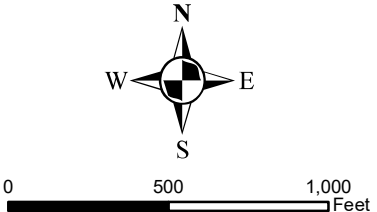


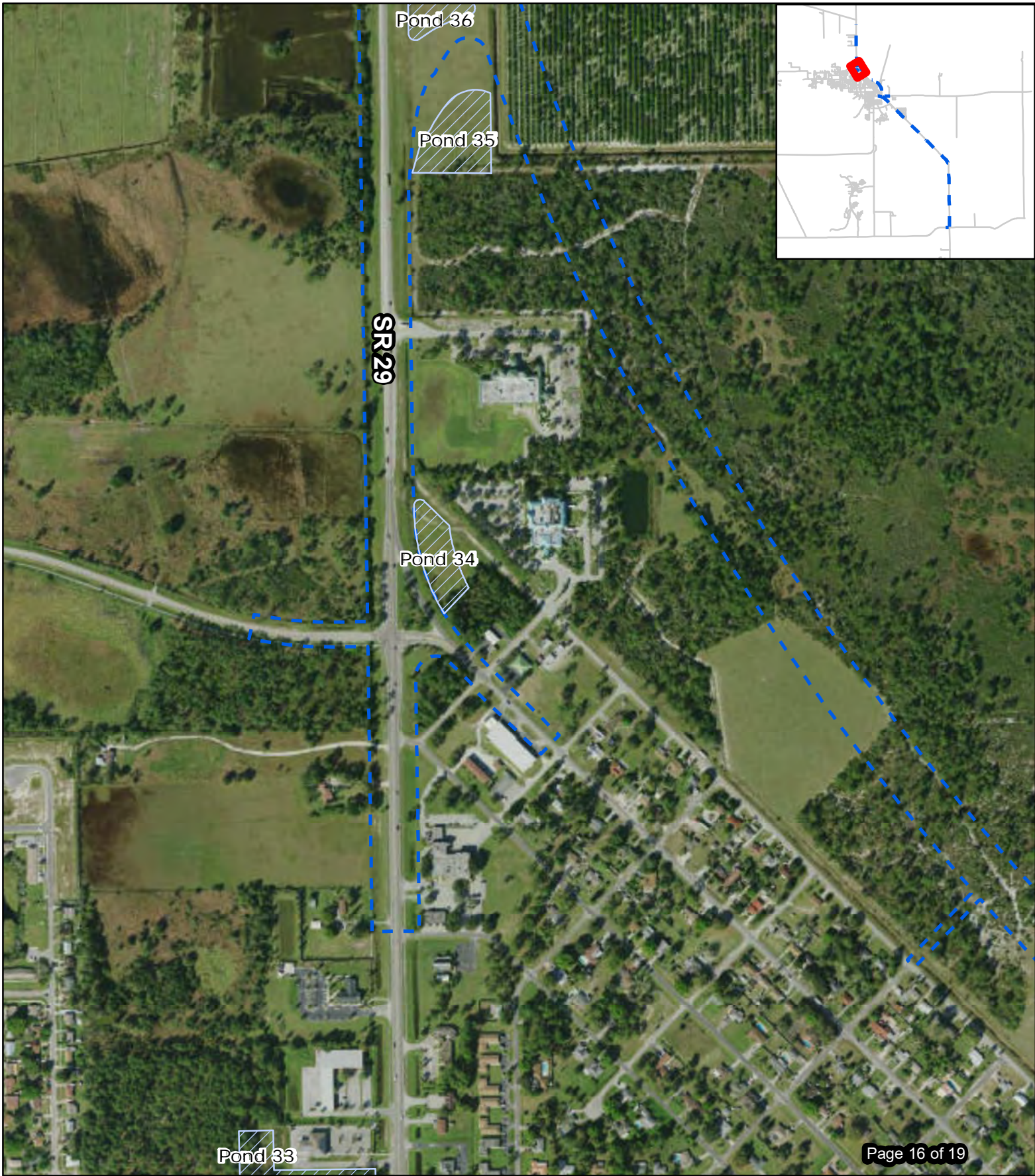
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 Central Alternative #2**

Date: 6/5/2018








Pond 33

Page 16 of 19

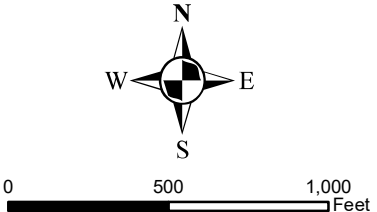
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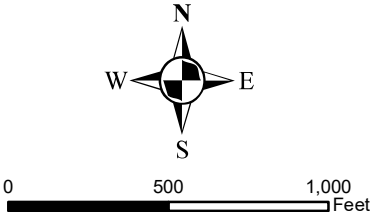
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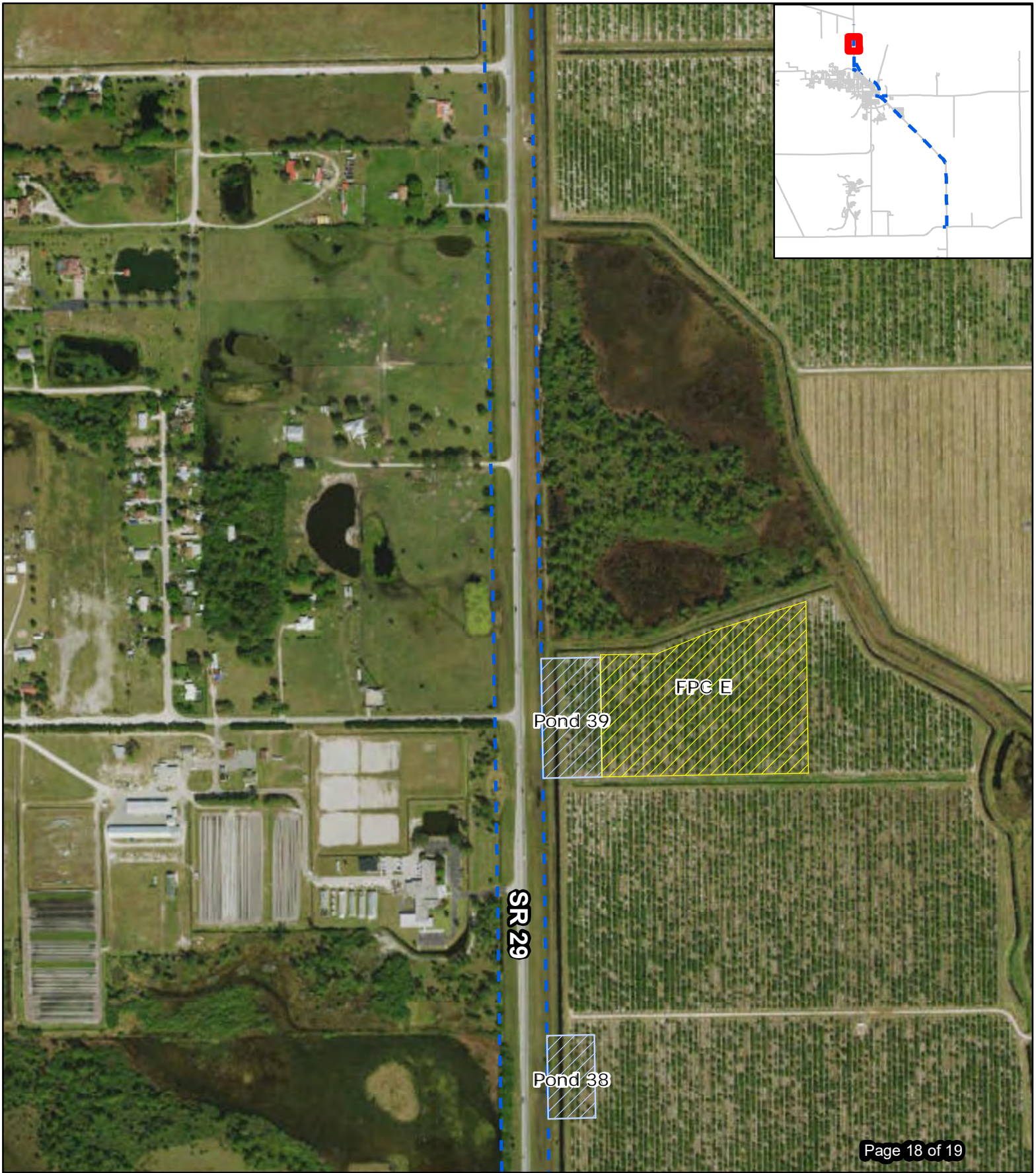
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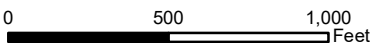
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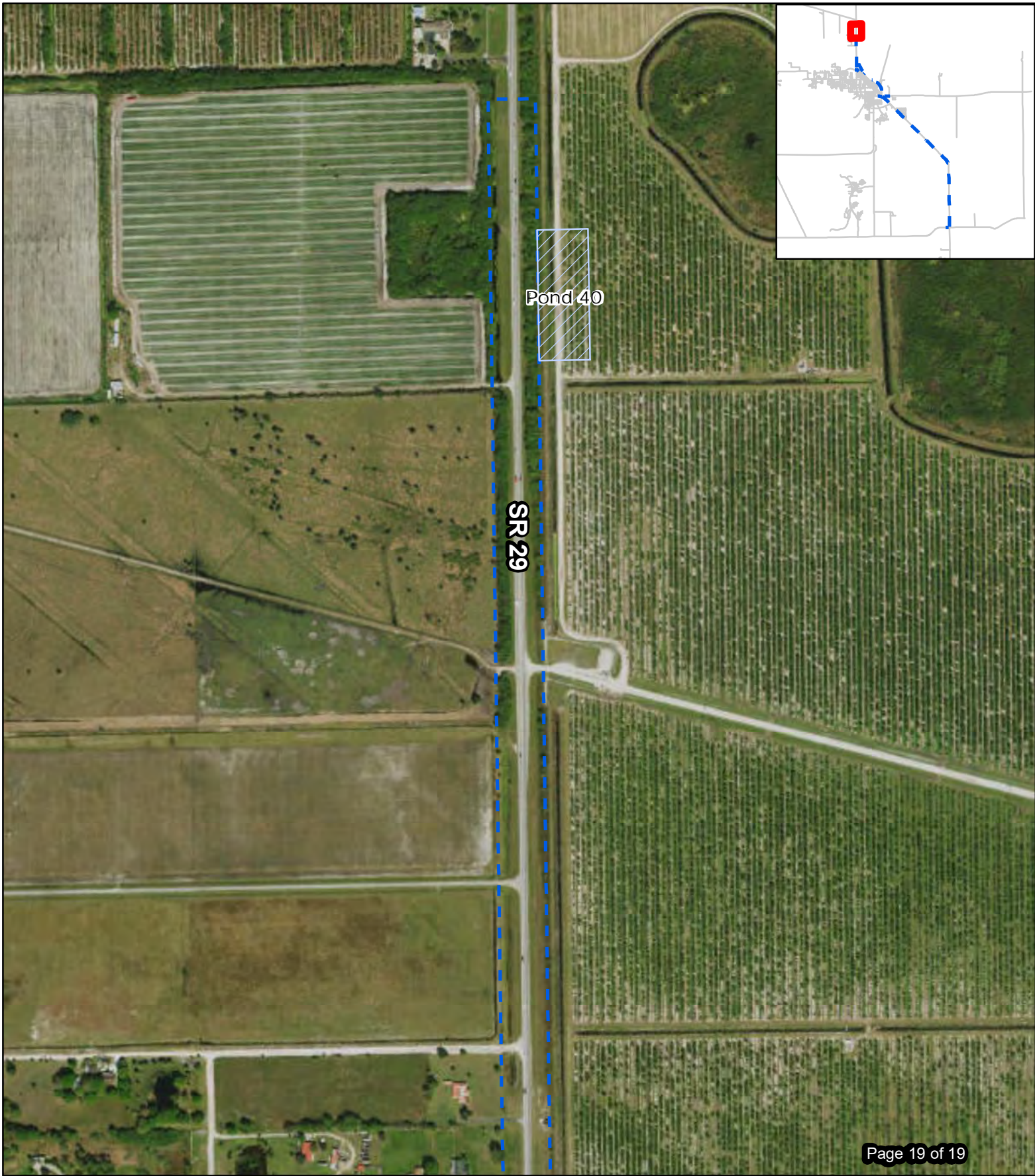
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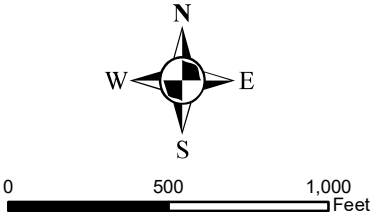
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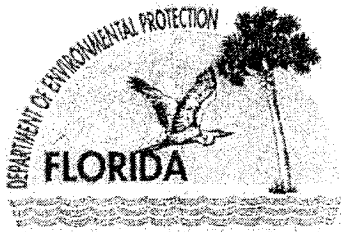
Date: 6/5/2018



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APPENDIX H

***Coastal Zone Management Program
Consistency Letter***



Florida Department of Environmental Protection

Marjory Stoneman Douglas Building
3900 Commonwealth Boulevard
Tallahassee, Florida 32399-3000

RECEIVED

OCT 17 2007

ENVIRONMENTAL
MANAGEMENT OFFICE

Florida Dept
of Environment

Dept of Planning
& Community

Local Government
Services

October 5, 2007

Mr. Mark A. Schulz, Environmental Administrator
Florida Department of Transportation, District One
Post Office Box 1249
Bartow, FL 33831-1249

RE: Department of Transportation – Advance Notification – SR 29 PD&E Study, from
Oil Well Road to SR 82, FPID No. 417540-1-22-01 – Collier County, Florida.
(Previous Review ETDM No. 3752)
SAI # FL200708063678C

Dear Mr. Schulz:

The Florida State Clearinghouse, pursuant to Presidential Executive Order 12372, Gubernatorial Executive Order 95-359, the Coastal Zone Management Act, 16 U.S.C. §§ 1451-1464, as amended, and the National Environmental Policy Act, 42 U.S.C. §§ 4321, 4331-4335, 4341-4347, as amended, has coordinated a review of the referenced advance notification.

The Florida Department of Environmental Protection (DEP) South District office advises that the South Florida Water Management District (SFWMD) will handle the required Environmental Resource Permit (ERP). Please refer to the DEP's earlier comments on this project for ETDM No. 3752.

The SFWMD notes that this project will require an ERP. As part of the ERP review process, the project will need to meet a pre- versus post-discharge rate since it is an expansion of the existing two-lane road. The project design will also need to meet the required water quality treatment volume. The alternative roadway to by-pass downtown Immokalee should be eliminated, especially due to its proximity to ecologically significant lands such as CREW. Widening the existing roadway will have significantly less wetland impacts to wetlands/uplands that provide habitat for listed species such as panther, bear and wading birds (especially wood storks). The proposed project is located within a Critical Foraging Area according to U.S. Fish and Wildlife Service. Wildlife crossings should be incorporated into the design of the road. In addition, the surface water runoff will need to be fully treated prior to discharging into the adjacent wetlands. Careful analysis needs to be done for locations of stormwater lakes and structures to help minimize impacts. Any proposed mitigation needs to address direct, secondary and

Mr. Mark A. Schulz
October 5, 2007
Page 2 of 2

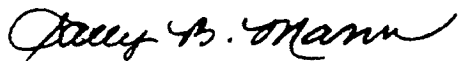
cumulative impacts. Mitigation at a mitigation bank should not be the only mitigation option considered. Please also note that a Water Use Permit will be required for any proposed ground or surface water withdrawals for landscape irrigation and may also be required for certain construction dewatering activities, if proposed.

The Florida Department of Community Affairs (DCA) has determined that each alignment referenced in the ETDM proposal is consistent with the Collier County Comprehensive Plan's Future Transportation Map. In addition, summary response comments for this project submitted by FDOT staff in 2005 were re-reviewed by DCA staff during the recent ETDM review cycle. Staff concurs with FDOT's findings that the State Road 29 alignment, which would bypass the Immokalee Community Redevelopment Area, could have possible adverse effects on redevelopment planning efforts in the area.

Based on the information contained in the advance notification and enclosed state agency comments, the state has no objections to the allocation of federal funds for the subject project and, therefore, the funding award is consistent with the Florida Coastal Management Program (FCMP). The applicant must, however, address the concerns identified by our reviewing agencies prior to project implementation. The state's continued concurrence with the project will be based, in part, on the adequate resolution of issues identified during this and subsequent reviews. The state's final concurrence of the project's consistency with the FCMP will be determined during the environmental permitting stage.

Thank you for the opportunity to review the proposed project. Should you have any questions regarding this letter, please contact Mr. Chris Stahl at (850) 245-2169.

Yours sincerely,



Sally B. Mann, Director
Office of Intergovernmental Programs

SBM/cjs
Enclosures

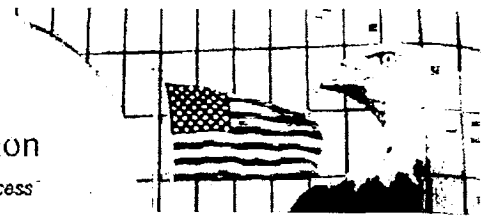
cc: Jim Golden, SFWMD
Johnna Mattson, DCA



Florida

Department of Environmental Protection

'More Protection, Less Process'



Categories

[DEP Home](#) | [OIP Home](#) | [Contact DEP](#) | [Search](#) | [DEP Site Map](#)

Project Information	
Project:	FL200708063678C
Comments Due:	09/10/2007
Letter Due:	10/05/2007
Description:	DEPARTMENT OF TRANSPORTATION - ADVANCE NOTIFICATION - SR 29 PD&E STUDY, FROM OIL WELL ROAD TO SR 82, FPID NO. 417540-1-22-01 - COLLIER COUNTY, FLORIDA. (PREVIOUS REVIEW ETDM NO. 3752)
Keywords:	DOT - SR 29 PD&E STUDY, FROM OIL WELL ROAD TO SR 82 - COLLIER CO.
CFDA #:	20.205
Agency Comments:	
STATE - FLORIDA DEPARTMENT OF STATE	
No Comment/Consistent	
COMMUNITY AFFAIRS - FLORIDA DEPARTMENT OF COMMUNITY AFFAIRS	
Identified Resources and Level of Importance: Each alignment referenced in this ETDM project is consistent with the Collier County Future Transportation Map. In addition, summary response comments for this project submitted by FDOT staff in 2005 were re-reviewed by DCA staff during the current ETDM review cycle. Staff concurs with FDOT findings that the State Road 29 alignment, which would bypass the Immokalee Community Redevelopment Area, could have possible adverse effects on redevelopment planning efforts in the area.	
ENVIRONMENTAL PROTECTION - FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION	
The South Florida Water Management District will handle the required Environmental Resource Permit. Please refer to the Department's earlier comments on this project under the ETDM No. 3752.	
FISH and WILDLIFE COMMISSION - FLORIDA FISH AND WILDLIFE CONSERVATION COMMISSION	
No Comments Received	
SOUTH FLORIDA WMD - SOUTH FLORIDA WATER MANAGEMENT DISTRICT	
This project will require an Environmental Resource Permit (ERP). As part of the ERP review process, the project will need to meet a pre versus post discharge rate since it is an expansion of the existing 2-lane road. The project design will also need to meet the required water quality treatment volume. The alternative roadway to by-pass downtown Immokalee should be eliminated, especially due to proximity to ecologically significant lands such as CREW. Widening the existing roadway will have significantly less wetland impacts to wetlands/uplands that provide habitat for listed species such as panther, bear, wading birds (especially wood storks). The proposed project is located within a Critical Foraging Area according to USFWS. Wildlife crossings should be incorporated into the design of the road. In addition, the surface water runoff will need to be fully treated prior to discharging into the adjacent wetlands. Careful analysis needs to be done for locations of stormwater lakes and structures to help minimize impacts. Any proposed mitigation needs to address direct, secondary and cumulative impacts. Mitigation at a mitigation bank should not be the only mitigation option considered. A Water Use Permit will be required for any proposed ground or surface water withdrawals for landscape irrigation. A Water Use Permit may also be required for certain construction dewatering activities, if proposed.	
SW FLORIDA RPC - SOUTHWEST FLORIDA REGIONAL PLANNING COUNCIL	
The proposal has been found to be Regionally Significant and Consistent with the adopted goals, objectives, and policies of the Strategic Regional Policy Plan.	
COLLIER - COLLIER COUNTY	

For more information or to submit comments, please contact the Clearinghouse Office at:

3900 COMMONWEALTH BOULEVARD, M.S. 47
 TALLAHASSEE, FLORIDA 32399-3000
 TELEPHONE: (850) 245-2161



Southwest Florida Regional Planning Council

1926 Victoria Avenue, Fort Myers,
(239)338-2550 FAX (239)338-2560 SUNCOM (239)748-2550

September 25, 2007

Mr. Mark A. Schulz
FDOT
PO Box 1249
Bartow, FL 33831-1249

RE: IC&R Project #2007-057
State Clearinghouse #FL200708063678C
FDOT - Advance Notification - SR 29 PD&E Study from Oil Well
Road to SR 82, FPID No. 417540-1-22-01 - Collier County, Florida
(Previous Review ETDM No. 3752)

Dear Mr. Schulz:

The staff of the Southwest Florida Regional Planning Council reviews various proposals, Notifications of Intent, Preapplications, permit applications, and Environmental Impact Statements for compliance with regional goals, strategies, and actions, as determined by the Strategic Regional Policy Plan. The staff reviews such items in accordance with the Florida Intergovernmental Coordination and Review Process (Chapter 29I-5, F.A.C.), and adopted regional clearinghouse procedures.

These designations determine Council staff procedure in regards to the reviewed project. The four designations are:

Less Than Regionally Significant and Consistent no further review of the project can be expected from Council.

Less Than Regionally Significant and Inconsistent Council does not find the project of regional importance, but will note certain concerns as part of its continued monitoring for cumulative impact within the noted goal area.

Regionally Significant and Consistent project is of regional importance, and appears to be consistent with Regional goals, objectives, and policies.

Regionally Significant and Inconsistent project is of regional importance and does not appear to be consistent with Regional goals, objectives, and policies. Council will oppose the project as submitted, but is willing to participate in any efforts to modify the project to mitigate the concerns.

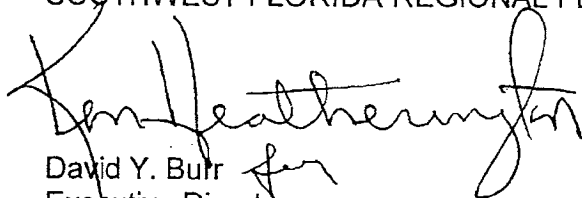
To: Mr. Mark A. Schulz
Date: September 25, 2007
Re: SWFRPC #2007-057
Page: 2

The above referenced document has been reviewed by this office, based on the information contained in the document, and on local knowledge, has been found **Regionally Significant and Consistent** with adopted goals, objectives, and policies of the Strategic Regional Policy Plan.

Should you or any other party request this finding to be reconsidered, please contact Nichole L. Gwinnett, IC&R Coordinator, with this request, or any questions concerning staff review of this item. This recommendation will be discussed at the next scheduled Council meeting. Should Council action differ from the staff recommendation, you will be notified.

Sincerely,

SOUTHWEST FLORIDA REGIONAL PLANNING COUNCIL



David Y. Burr
Executive Director

DYB/NLG

cc: Sally B. Mann, Florida State Clearinghouse Director

2007-6490 CS

COUNTY: COLLIER

DATE: Collier 8/6/2007

COMMENTS DUE DATE: 9/10/2007

CLEARANCE DUE DATE: 10/5/2007

SAI#: FL200708063678C

MESSAGE:

STATE AGENCIES	WATER MNGMNT. DISTRICTS	OPB POLICY UNIT	RPCS & LOC GOVS
COMMUNITY AFFAIRS	SOUTH FLORIDA WMD		
ENVIRONMENTAL PROTECTION			
FISH and WILDLIFE COMMISSION			
X STATE			

RECEIVED
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OIP / OLGA

The attached document requires a Coastal Zone Management Act/Florida Coastal Management Program consistency evaluation and is categorized as one of the following:

- X Federal Assistance to State or Local Government (15 CFR 930, Subpart F). Agencies are required to evaluate the consistency of the activity.
- Direct Federal Activity (15 CFR 930, Subpart C). Federal Agencies are required to furnish a consistency determination for the State's concurrence or objection.
- Outer Continental Shelf Exploration, Development or Production Activities (15 CFR 930, Subpart E). Operators are required to provide a consistency certification for state concurrence/objection.
- Federal Licensing or Permitting Activity (15 CFR 930, Subpart D). Such projects will only be evaluated for consistency when there is not an analogous state license or permit.

Project Description:

DEPARTMENT OF TRANSPORTATION - ADVANCE NOTIFICATION - SR 29 PD&E STUDY, FROM OIL WELL ROAD TO SR 82. FPID NO. 417540-1-22-01 - COLLIER COUNTY, FLORIDA. (PREVIOUS REVIEW ETDM NO. 3752)

To: Florida State Clearinghouse

AGENCY CONTACT AND COORDINATOR (SCH)
3900 COMMONWEALTH BOULEVARD MS-47
TALLAHASSEE, FLORIDA 32399-3000
TELEPHONE: (850) 245-2161
FAX: (850) 245-2190

EO. 12372/NEPA Federal Consistency

- No Comment
- Comment Attached
- Not Applicable
- No Comment/Consistent
- Consistent/Comments Attached
- Inconsistent/Comments Attached
- Not Applicable

From:

Division/Bureau: Division of Historical Resources/Bureau of Historic Preservation

Reviewer: Samantha Earnest

Date: 8/9/2007

Barbara E. Mettick for Fred Clarke 8/15/07

2007 AUG -8 - 10:59
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HISTORIC PRESERVATION

APPENDIX I

FWS Coordination

From: Pipkin, Gwen G
To: [Bizerra, Marlon](#); [Howell, Bill](#); [Peate, Martin](#); [Brooks, Lauren](#); kwarren@rkk.com
Subject: FW: SR 29 Immokalee
Date: Tuesday, March 20, 2018 10:10:51 AM
Importance: High

We have concurrence from John Wrublik (see below) on our plan to do some species surveys as part of design. We will do the NRE as usual and get concurrence on the species we can do now, and include commitments to do during design for the rest. Please forward as needed.

Gwen G. Pipkin

Environmental Manager

Office - 863.519.2375

Cell - 863-280-5850

gwen.pipkin@dot.state.fl.us

From: Wrublik, John [mailto:john_wrublik@fws.gov]
Sent: Tuesday, March 20, 2018 8:26 AM
To: Pipkin, Gwen G <Gwen.Pipkin@dot.state.fl.us>
Subject: Re: SR 29 Immokalee

Gwen,

The proposal that the listed species surveys indicated for this project be conducted during the design phase of the project is acceptable to the Service. I don't have any further comments at this time.

John

John M. Wrublik
U.S. Fish and Wildlife Service
1339 20th Street
Vero Beach, Florida 32960
Office: (772) 469-4282
Fax: (772) 562-4288
email: John_Wrublik@fws.gov

NOTE: This email correspondence and any attachments to and from this sender is subject to the Freedom of Information Act (FOIA) and may be disclosed to third parties.

On Tue, Mar 20, 2018 at 7:30 AM, Pipkin, Gwen G <Gwen.Pipkin@dot.state.fl.us> wrote:

Hi John,

We spoke a while back about completing some of our species surveys during design for this

project. I followed up I with an email (see attached). I would like to know if you have had a chance to review that, and if we could get a response back?

I am also including the following additional information for your use.

- Panther: This is the major wildlife issue south of Immokalee, especially considering the number of panther vehicle strikes. A wildlife crossing at Owl Hammock curve is needed. PHUs for lost habitat will also need to be calculated as part of the PD&E.
- Crested caracara: No nests currently known in PD&E study area; surveys will be required during design for those segments that are not right in town.
- Scrub jay: An updated survey will be required during design for the new alignment segment northwest of the airport (a colony is known to exist in this area). There is no suitable habitat south of Immokalee.
- Wood stork: Suitable foraging habitat is present in all segments and at least three colonies are within 18.6 miles. A foraging habitat assessment should be completed during design.

Thanks, John, I look forward to your response!

Gwen G. Pipkin

Environmental Manager

Office - 863.519.2375

Cell - 863-280-5850

gwen.pipkin@dot.state.fl.us

----- Forwarded message -----

From: "Pipkin, Gwen G" <Gwen.Pipkin@dot.state.fl.us>

To: "John Wrublik (john_wrublik@fws.gov)" <john_wrublik@fws.gov>

Cc:

Bcc:

Date: Thu, 8 Mar 2018 17:36:41 +0000

Subject: 417540-1 - SR 29 from Oil Well Rd to SR 82, Immokalee

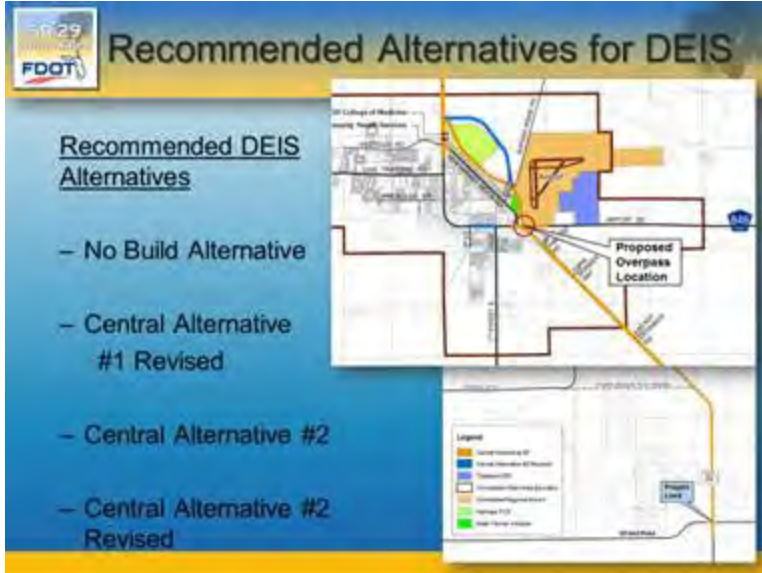
John,

We spoke last week about the method FDOT would like to use to accomplish the species surveys for this project, and I was going to send you an email with more information so you could reply back. My apologies for taking so long!

Due to time constraints on the project, and the sensitivity of the species issues in the area, we feel it would be more appropriate to complete the NRE with commitments to do the formal surveys and coordination during the design phase, when the plans are more detailed. The species we feel would be best to complete later are snail kite, scrub jay, caracara, bonneted bat, and panther. The forthcoming NRE will address the rest of the species, and contain the commitments for completing the rest during design.

Also, just to update you, we are planning to move forward with only two build alternatives and the

no-build alternative. We are in the process of officially eliminating Central Alternative #2 Revised, shown in blue below.



Thanks,

Gwen G. Pipkin

Environmental Manager

Office - 863.519.2375

Cell - 863-280-5850

gwen.pipkin@dot.state.fl.us

APPENDIX J

***FWS and FWC Concurrence Letters
for NRE and NRE Addendum***

From: John Wrublik
To: [Bennett, Jonathon](mailto:Jonathon.Bennett@dot.state.fl.us)
Subject: Re: [EXTERNAL] 417540-1-22-01 NRE Transmittal
Date: Friday, August 03, 2018 9:05:31 AM

EXTERNAL SENDER: Use caution with links and attachments.

John M. Wrublik
U.S. Fish and Wildlife Service
1339 20th Street
Vero Beach, Florida 32960
Office: (772) 469-4282
Fax: (772) 562-4288
email: John.Wrublik@fws.gov

NOTE: This email correspondence and any attachments to and from this sender is subject to the Freedom of Information Act (FOIA) and may be disclosed to third parties.

Jonathon,

Yes I have downloaded the documents for the SR 29 project. I thought that I had sent you a response to your email, letter, and NRE dated July 20, 2018, but I can not locate in my records so maybe I neglected to send it. Anyway, her is the response I thought I had sent to you. You indicated in your letter that the FDOT intends to re-initiate consultation with the Service regarding the project's adverse effects to the Florida panther and the Florida scrub-jay during the project's design and permitting phase. In order to avoid unnecessary duplication of effort and better manage my workload, I will respond to determinations for all listed species (i.e., panther, scrub-jay, and all species that you made a MANLAA determination in your July 20th, 2018 letter) at the time of re-initiation of consultation for this project (i.e., during the final design and permitting phase). I have no other comments on the project at this time.

Sincerely,

John Wrublik

On Thu, Aug 2, 2018 at 1:16 PM Bennett, Jonathon <Jonathon.Bennett@dot.state.fl.us> wrote:

Good afternoon,

The email below was sent Friday July 20th, 2018, it is for a review of the SR 29 from Oil Well Rd to SR 82 Collier County Natural Resource Evaluation Report (NRE). The link will expire on Friday August 3rd, please let me know if you need me to resend the link for your availability to download and review the NRE. If you have already retrieved this file, please disregard this email.

Thank you,

Jonathon A. Bennett

Environmental Project Manager

Florida Department of Transportation District One

801 North Broadway Avenue

Bartow, Florida 33830

Office – (863) 519-2495

Main – (863) 519-2300

62.05 MB

417540-1 NRE Transmittal_xxx.pdf

127.30 KB

Thank you for sharing files securely.

Secured by [Accellion](#)



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Conservation
Commission**

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800-955-8771 (T)
800 955-8770 (V)

MyFWC.com

August 21, 2018

Mr. Jonathon A. Bennett
Environmental Project Manager
Florida Department of Transportation (FDOT) District 1
801 N. Broadway Avenue
Bartow, FL 33830
Jonathon.Bennett@dot.state.fl.us

Re: SR 29 from Oil Well Road to SR 82, Collier County, Natural Resources
Evaluation Report, File Number 417540-1-22-01

Dear Mr. Bennett:

The Florida Fish and Wildlife Conservation Commission (FWC) staff has reviewed the Natural Resources Evaluation Report (NRE) and the NRE Addendum for the above-referenced project. The NRE was prepared as part of the Project Development and Environment Study for the proposed project. Since 2005, we have been involved in the review of this project via the Efficient Transportation Decision Making process as ETDM 3752, and through meetings and correspondence with FDOT District 1 and environmental resource agency staffs. We provide the following comments and recommendations for your consideration in accordance with Chapter 379, Florida Statutes and Rule 68A-27, Florida Administrative Code (F.A.C.).

Project Description

The project involves the widening of SR 29 from two lanes to four lanes between Oil Well Road and SR 82, a distance of approximately 15.6 miles, and including a new four-lane roadway bypassing the downtown area of Immokalee. The two build alternatives under consideration differ only in their alignment of the Immokalee bypass near the Immokalee Regional Airport. The Central Alternative #1 Revised runs to the west of the airport through developed land within Immokalee, while Central Alternative #2 runs through the Upland Management Area on the west side of airport property where the FWC holds a conservation easement associated with Gopher Tortoise (*Gopherus polyphemus*) Incidental Take Permit No. COL 36, and which is managed to benefit the resident Florida scrub-jays (*Aphelocoma coerulescens*). Central Alternative #2 would result in 4.45 acres of direct impact to this conservation easement. The project area is dominated by agricultural land use (pasture, rangeland, and citrus) with urban land use within the City of Immokalee. Natural land cover includes some pine flatwoods and several forested and herbaceous wetlands. The Big Cypress Area of Critical State Concern borders the east side of SR 29 in the southern portion of the project area.

Potentially Affected Resources

The NRE evaluated potential project impacts to 18 wildlife species classified under the Endangered Species Act as Federally Endangered (FE) or Threatened (FT), or by the State of Florida as Threatened (ST). Listed species were evaluated based on range and

potential appropriate habitat or because the project is within a U.S. Fish and Wildlife Service (USFWS) Consultation Area. Included were: eastern indigo snake (*Drymarchon corais couperi*, FT), American alligator (*Alligator mississippiensis*, FT based on similarity of appearance to American crocodile, *Crocodylus acutus*), Audubon's crested caracara (*Polyborus plancus audubonii*, FT), Everglade snail kite (*Rostrhamus sociabilis plumbeus*, FE), Florida grasshopper sparrow (*Ammodramus savannarum floridanus*, FE), Florida scrub-jay (FT), red-cockaded woodpecker (*Picoides borealis*, FE), wood stork (*Mycteria americana*, FT), Florida panther (*Puma concolor coryi*, FE), Florida bonneted bat (*Eumops floridanus*, FE), gopher tortoise (ST), Florida burrowing owl (*Athene cunicularia floridana*, ST), southeastern American kestrel (*Falco sparverius paulus*, ST), Florida sandhill crane (*Antigone canadensis pratensis*, ST), little blue heron (*Egretta caerulea*, ST), tricolored heron (*Egretta tricolor*, ST), roseate spoonbill (*Platalea ajaja*, ST), and Big Cypress fox squirrel (*Sciurus niger avicennia*, ST). Also evaluated were the bald eagle (*Haliaeetus leucocephalus*), which was delisted by state and federal agencies, but this species remains protected under state rule in Section 68A-16.002, F.A.C., and by the federal Bald and Golden Eagle Protection Act (16 U.S.C. 668-668d); the osprey (*Pandion haliaetus*), which is protected under the Migratory Bird Treaty Act (16 U.S.C. 703-712); and the Florida black bear (*Ursus americanus floridanus*), which is protected in Section 68A-4.009 F.A.C.

Comments and Recommendations

Due to the lack of both appropriate habitat and observation during on-site surveys, project biologists made a finding of "no effect" for the red-cockaded woodpecker and Florida grasshopper sparrow. For the other federally listed species and the gopher tortoise, the biologist's findings were "may affect, but is not likely to adversely affect". The other state-listed species were given a "no adverse effect anticipated" determination. With adherence to the project commitments, we agree with these determinations.

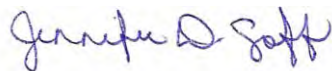
We support the project commitments for protected species, which include the following:

1. The FDOT will perform updated wildlife surveys for the species discussed in the NRE and other wildlife species during the project design phase to ascertain the involvement, if any, of listed/protected species.
2. The FDOT will coordinate further with the FWC during the project design phase for impacts associated with state-listed wildlife species.
3. A Section 7 Consultation with the USFWS will be completed during project design and permitting for the panther, scrub-jay, crested caracara, and wood stork. Appropriate mitigation will be completed for habitat impacts to these species.
4. A wildlife crossing will be constructed near the Owl Hammock curve, which has a high number of panther road kills.
5. The Standard Protection Measures for the Eastern Indigo Snake will be followed during construction.
6. For gopher tortoise burrows that cannot be avoided, the tortoises will be relocated per current FWC guidelines. For gopher tortoise survey methodology and permitting guidance, we recommend that FDOT refer to the FWC's Gopher Tortoise Permitting Guidelines (Revised January 2017) at (<http://www.myfwc.com/license/wildlife/gopher-tortoise-permits/>).

7. Should the Central Alternative #2 be selected for construction, FDOT will provide compensatory land acquisition to mitigate the loss of land within FWC's Immokalee Regional Airport Conservation Easement. As stated in the NRE Addendum, FWC has identified six priority parcels contiguous to the Platt Branch Wildlife and Environmental Area in Highlands County as preferred potential site options for mitigation.
8. The FDOT will resurvey the project limits for the presence of bald eagle nests prior to construction commencement. If a bald eagle nest is identified within the 660-foot construction buffer zone of the project area, the FDOT will coordinate with the FWS (as applicable) to secure all necessary approvals regarding this species prior to project construction.
9. The FDOT will resurvey the project limits for the presence of active osprey nests prior to construction commencement. If an active osprey nest is identified within the project area, the FDOT will coordinate with the FWC (as applicable) to secure all necessary approvals regarding this species prior to project construction.
10. The FDOT will follow the FDOT Supplemental Standard Specification 7-1.4.1 Additional Requirements for the Florida Black Bear to minimize human-bear interactions associated with construction sites during project construction.
11. Wetland impacts resulting from construction of this project will be mitigated pursuant to Section 373.4137, F.S., to satisfy all mitigation requirements of Part IV of Chapter 373, F.S., and 33 U.S.C. §1344. Compensatory mitigation for this project will be completed through the use of mitigation banks and any other mitigation options that satisfy state and federal requirements.
12. During the construction phase of this project, the FDOT will implement the Standard Specifications for Road and Bridge Construction and other best management practices to avoid, where possible, and otherwise minimize adverse impacts to wetlands and water quality within the project limits to the maximum extent practicable.

Thank you for the opportunity to review the NRE for the SR 29 from Oil Well Road to SR 82 project in Collier County. If you need further assistance, please do not hesitate to contact our office by email at FWCConservationPlanningServices@MyFWC.com. If you have specific technical questions regarding the content of this letter, contact Brian Barnett at (772) 579-9746 or email brian.barnett@MyFWC.com.

Sincerely,

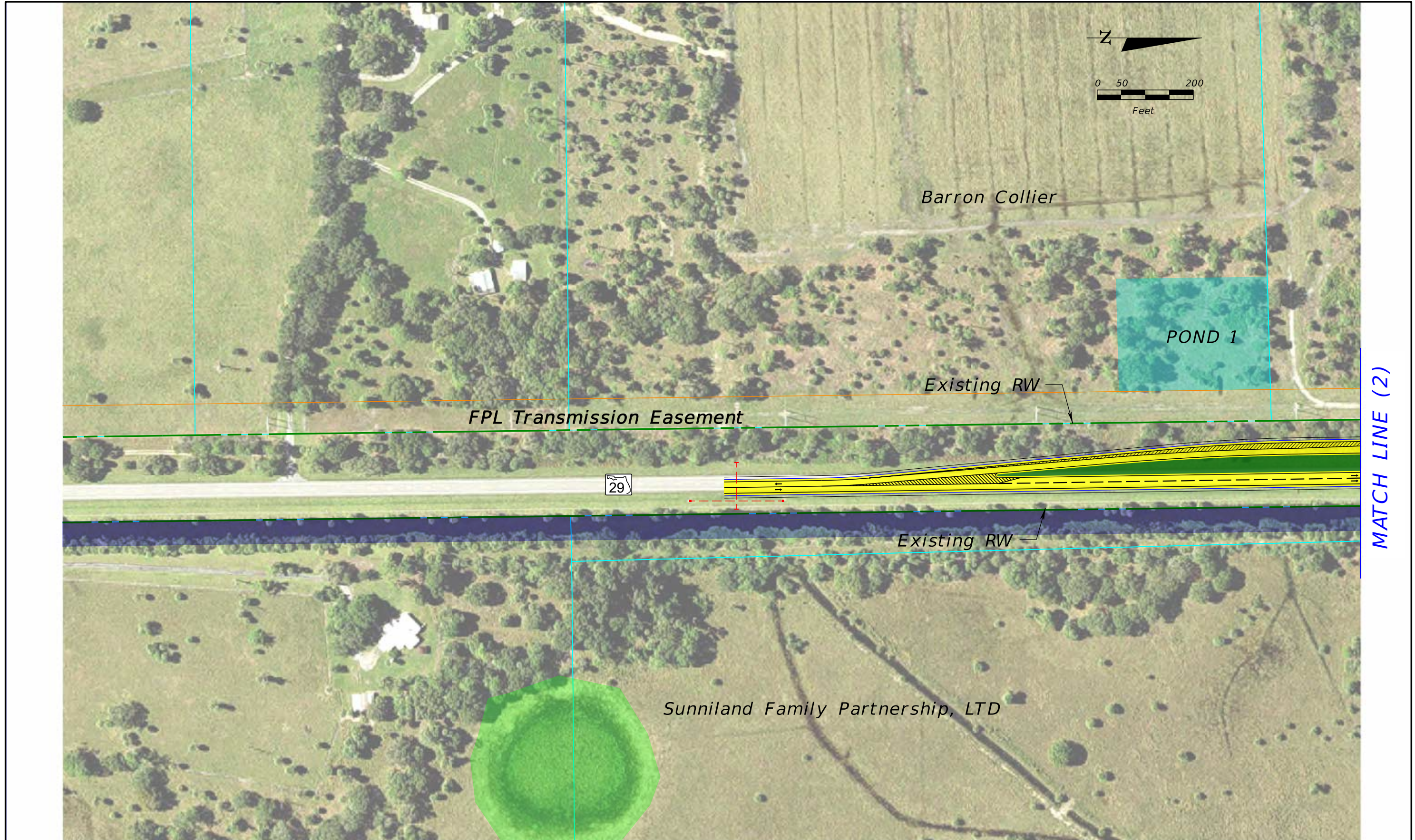


Jennifer D. Goff, Director
Office of Conservation Planning Services

Post-Public Hearing

APPENDIX K

***Preferred Alternative Concept Plans –
CR 846 to SR 82 Refinements (Sheets 21 – 34)***



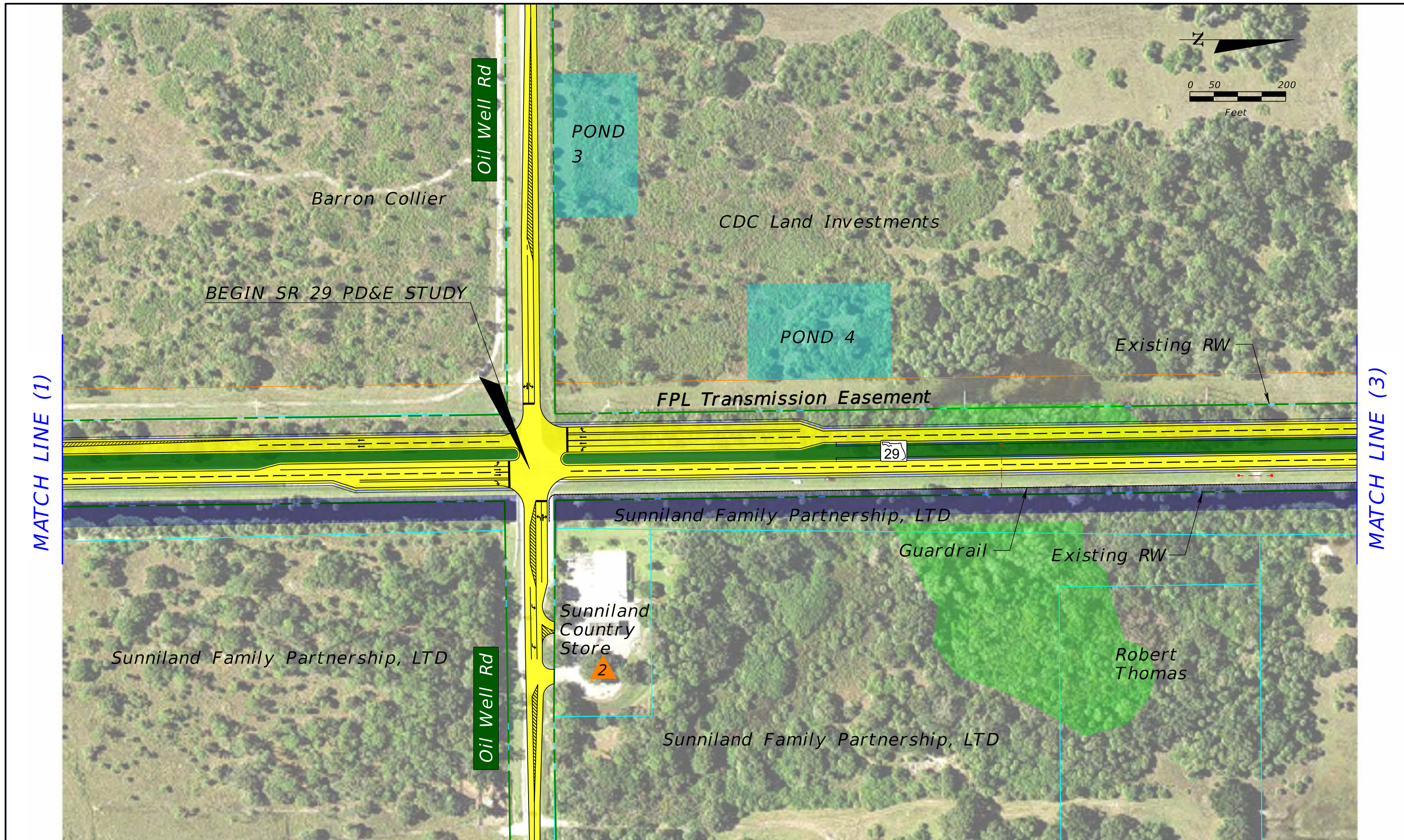
SR 29 PD&E Study
 From Oil Well Road to SR 82
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Legend			
Existing Right-of-Way	Proposed Pond	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Floodplain Compensation	Proposed Sidewalks/ Shared Use Path	Potential Contamination (Low)
Water/Canal	Traffic Signal	Proposed Traffic Separator	Potential Contamination (Medium or High)
Wetland		Existing Drainage Structure	

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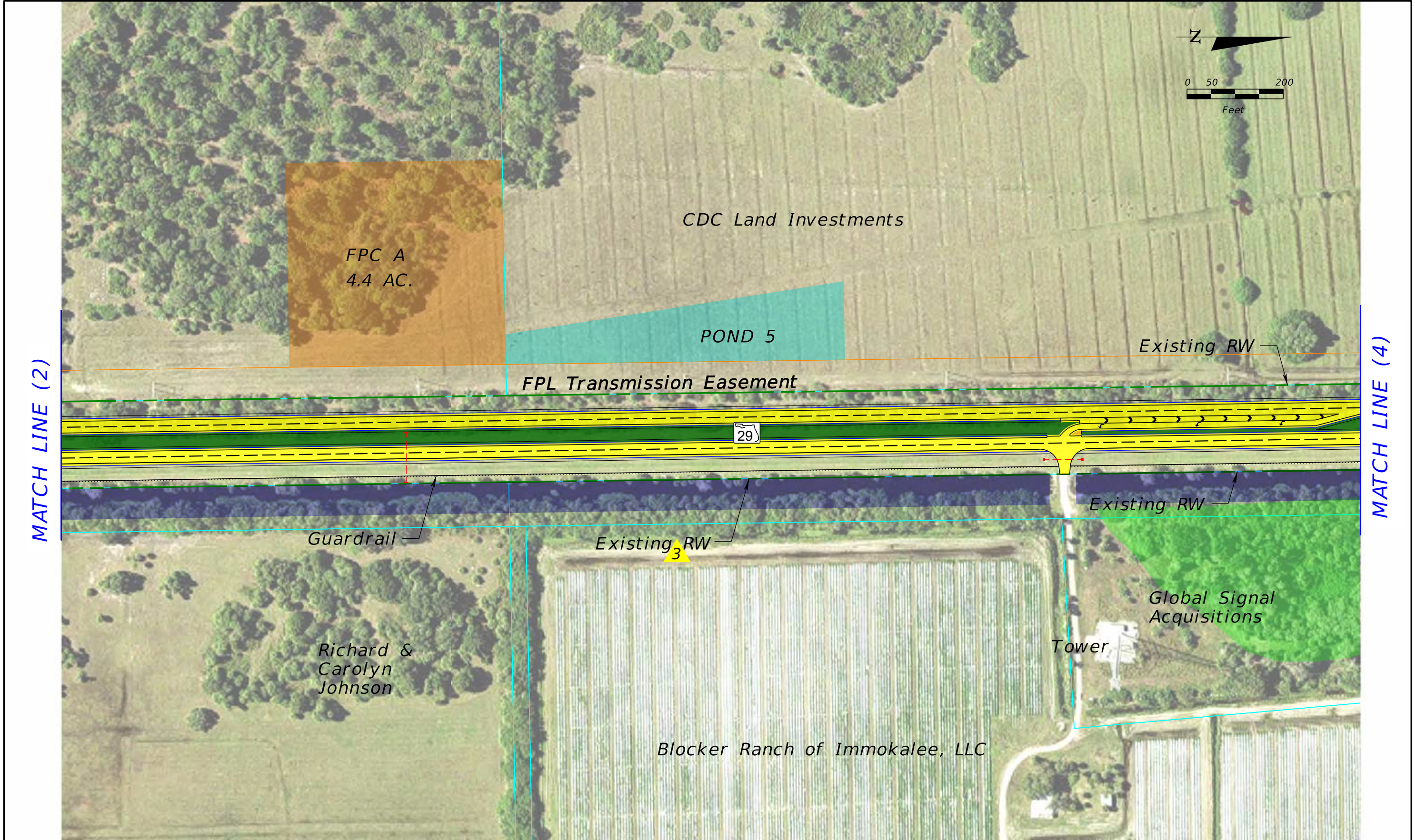
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Parcels	Potential Pond
Proposed Right-of-Way	Potential Floodplain Compensation
Water/Canal	Traffic Signal
Wetland	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks/ Shared Use Path
	Proposed Traffic Separator
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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MATCH LINE (4)

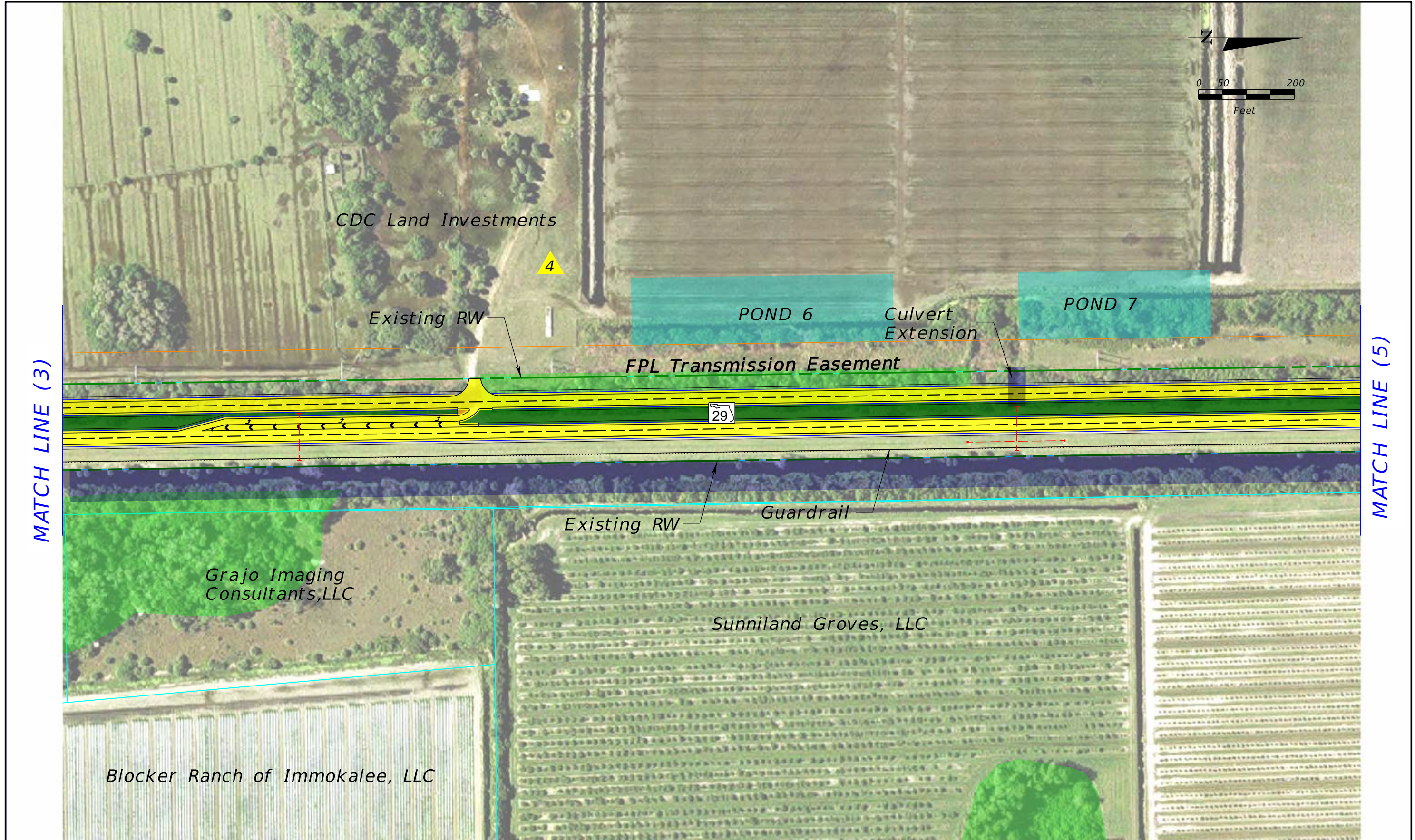
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks/ Shared Use Path
			Proposed Traffic Separator
			Existing Drainage Structure
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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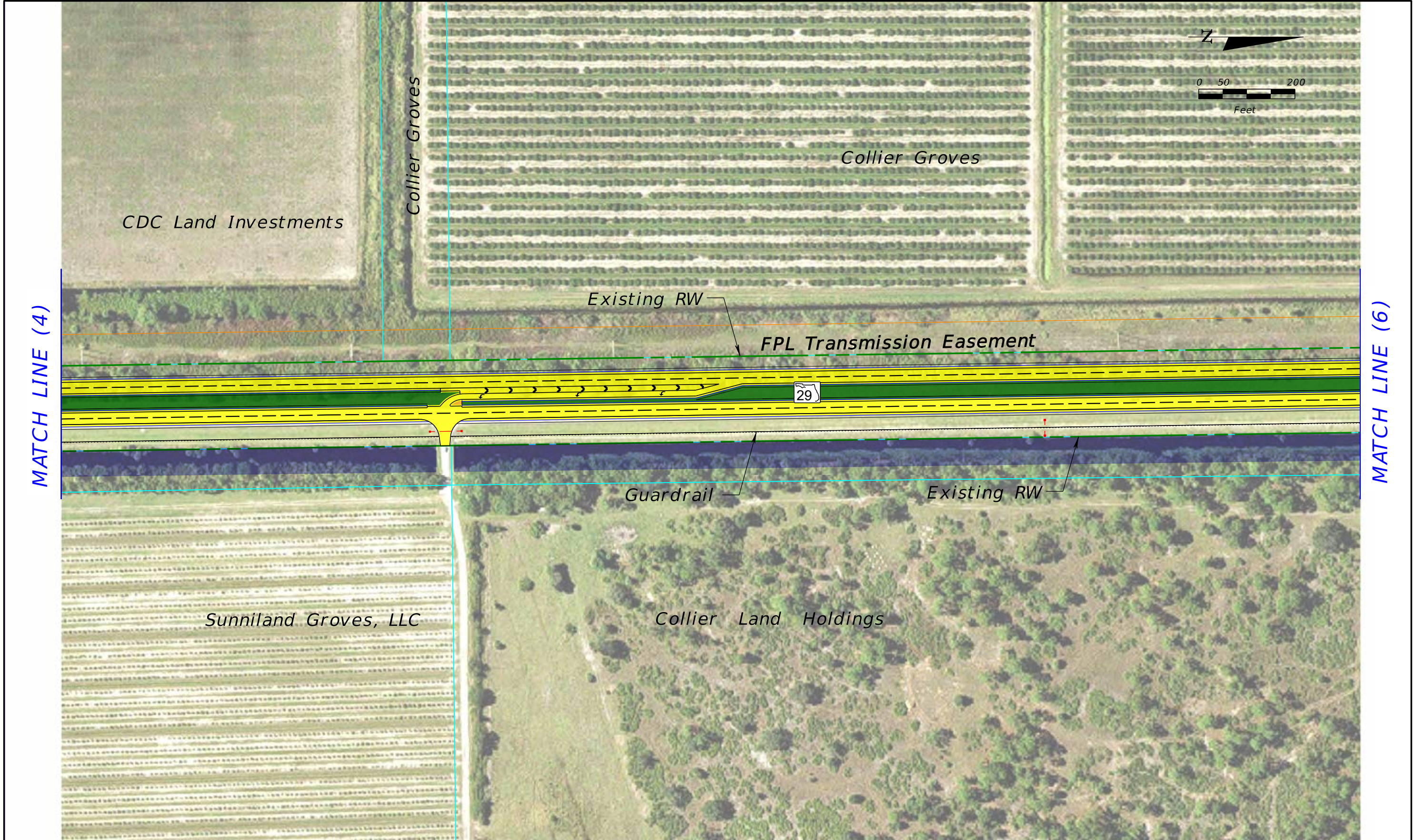
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks/ Shared Use Path
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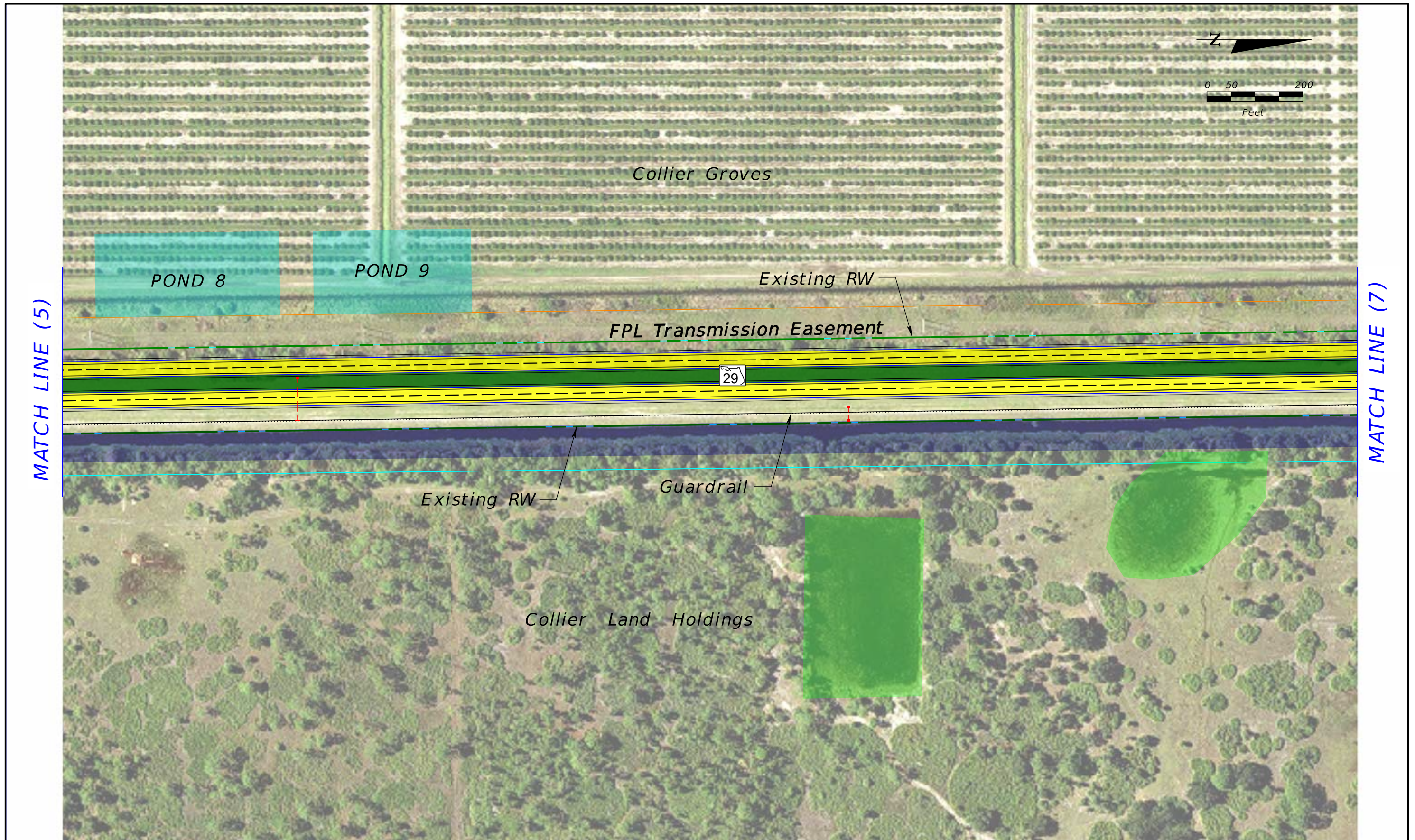
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 From Oil Well Road to SR 82
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks/ Shared Use Path
			Proposed Traffic Separator
			Existing Drainage Structure
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			Potential Business Relocation
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SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

- Existing Right-of-Way
- Parcels
- Proposed Right-of-Way
- Water/Canal
- Wetland

- Proposed Pond
- Potential Pond
- Potential Floodplain Compensation
- Traffic Signal

Legend

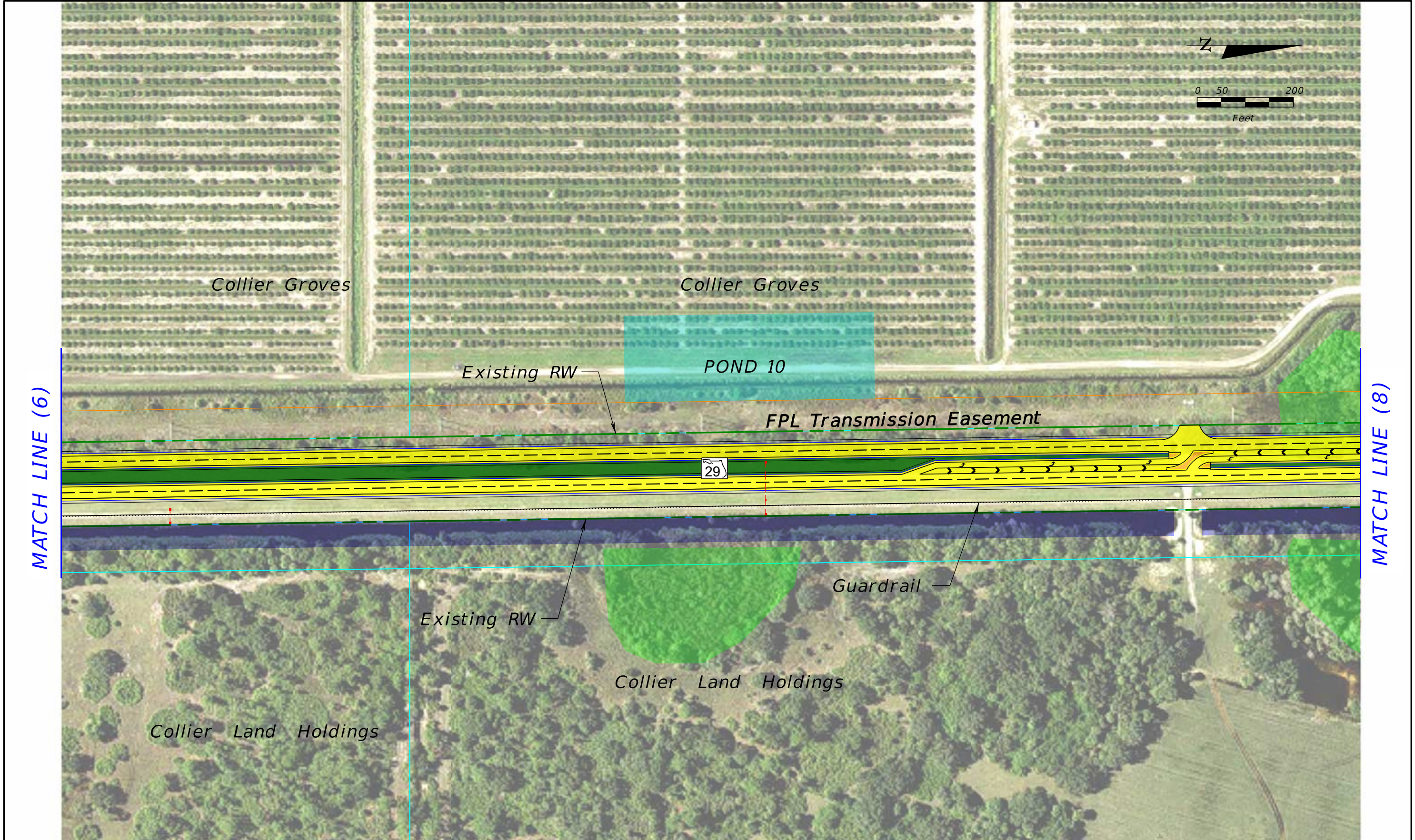
- Proposed Pavement
- Proposed Median/Border
- Proposed Sidewalks/ Shared Use Path
- Proposed Traffic Separator
- Existing Drainage Structure

- Proposed Guardrail
- Potential Business Relocation
- Potential Contamination (Low)
- Potential Contamination (Medium or High)

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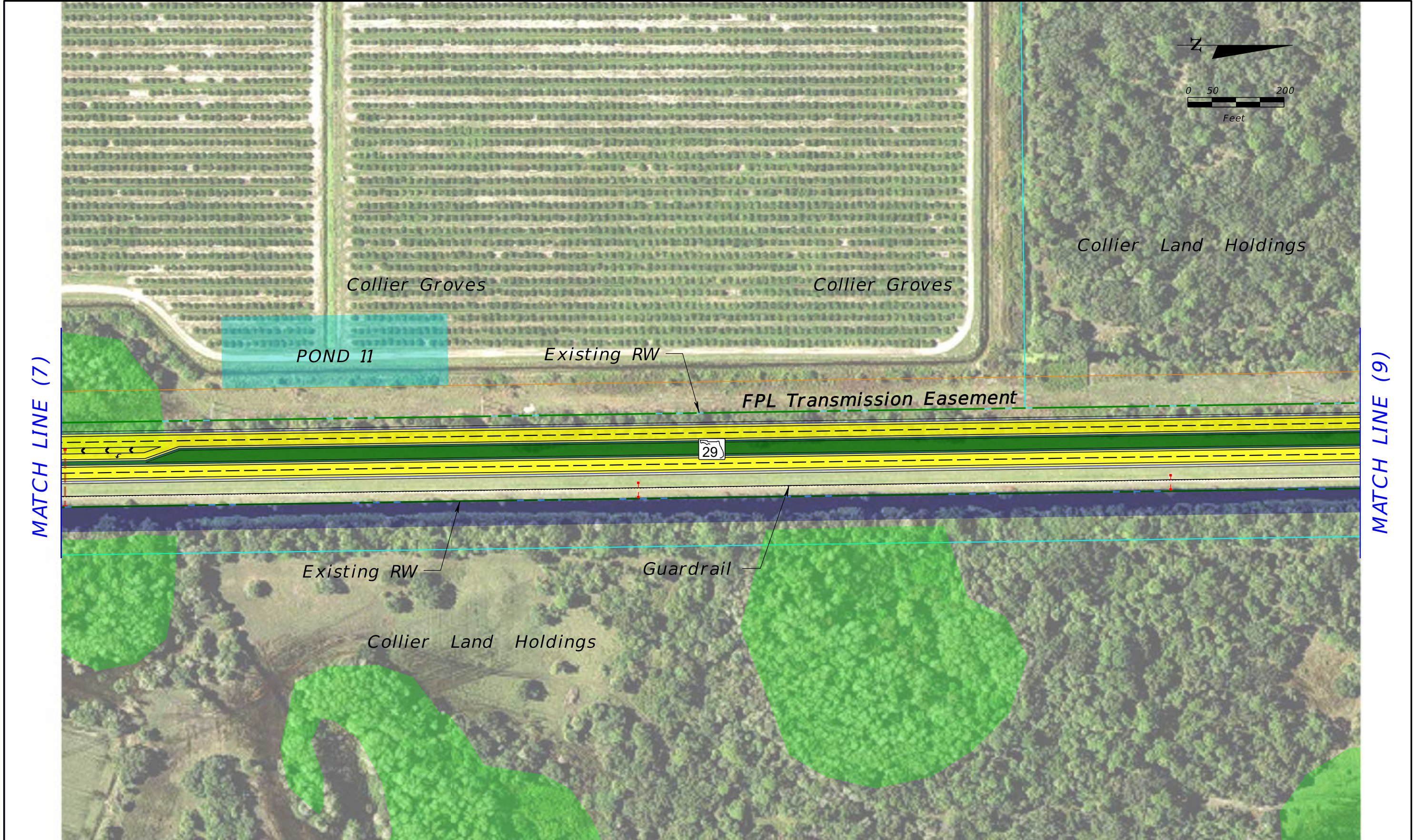
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 From Oil Well Road to SR 82
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
	Traffic Signal		Proposed Sidewalks/ Shared Use Path
			Proposed Traffic Separator
			Existing Drainage Structure
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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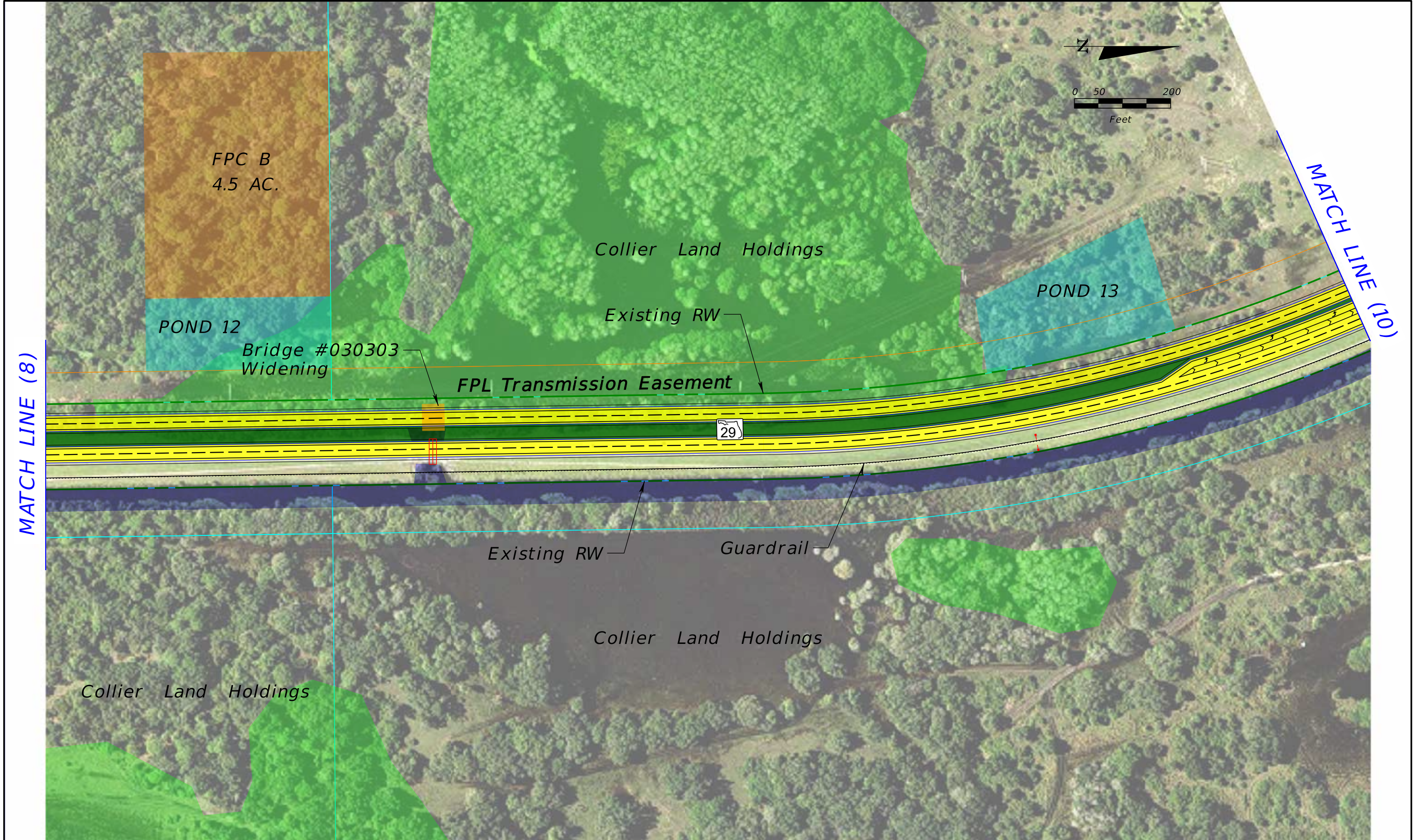
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
			Proposed Sidewalks/ Shared Use Path
			Proposed Traffic Separator
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)



SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
Existing Right-of-Way	Proposed Pond	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Floodplain Compensation	Proposed Sidewalks/ Shared Use Path	Potential Contamination (Low)
Water/Canal	Traffic Signal	Proposed Traffic Separator	Potential Contamination (Medium or High)
Wetland		Existing Drainage Structure	

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9



SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

- Existing Right-of-Way
- Parcels
- Proposed Right-of-Way
- Water/Canal
- Wetland

- Proposed Pond
- Potential Pond
- Potential Floodplain Compensation
- Traffic Signal

Legend

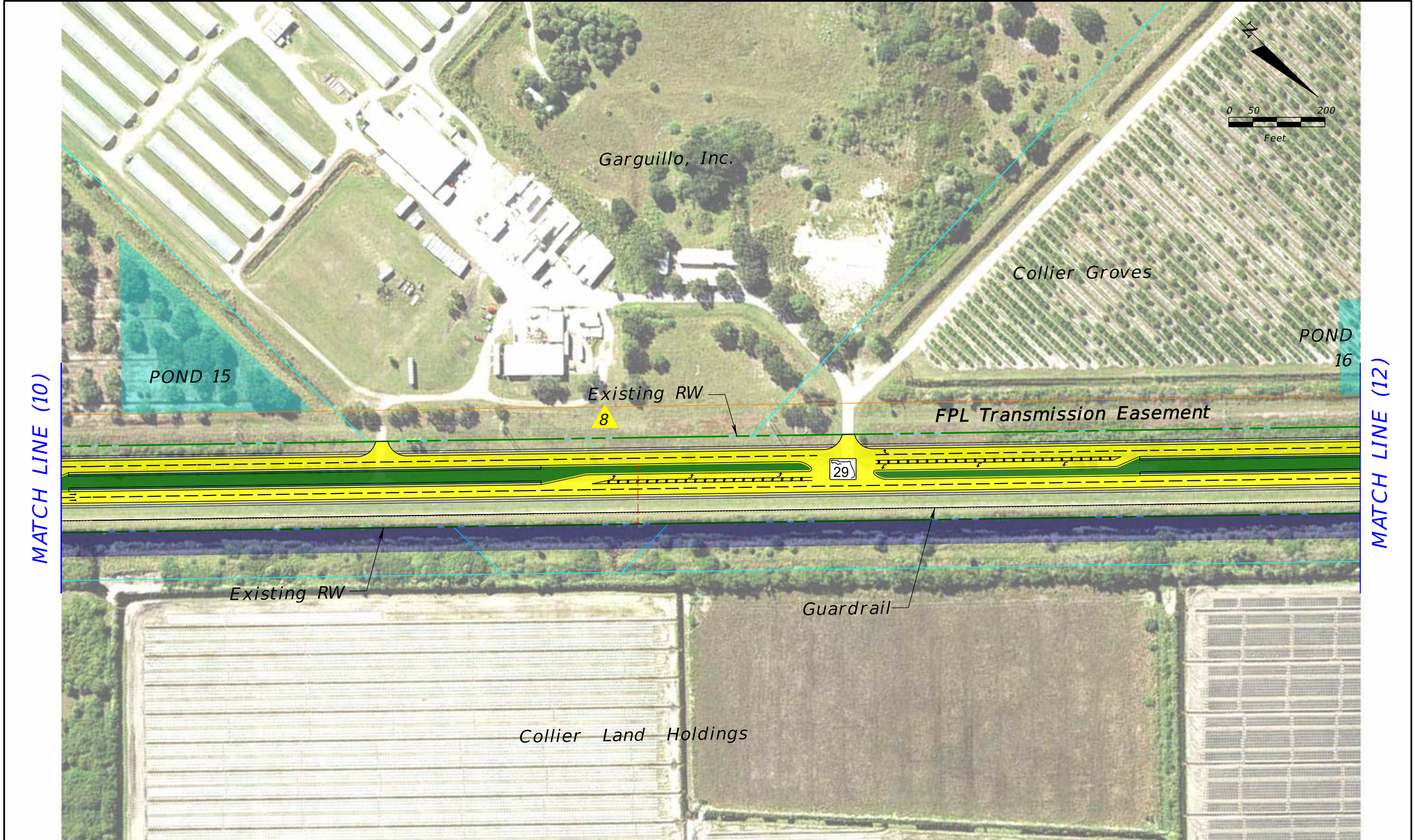
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- Proposed Median/Border
- Proposed Sidewalks/ Shared Use Path
- Proposed Traffic Separator
- Existing Drainage Structure

- Proposed Guardrail
- Potential Business Relocation
- Potential Contamination (Low)
- Potential Contamination (Medium or High)

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Preferred Alternative

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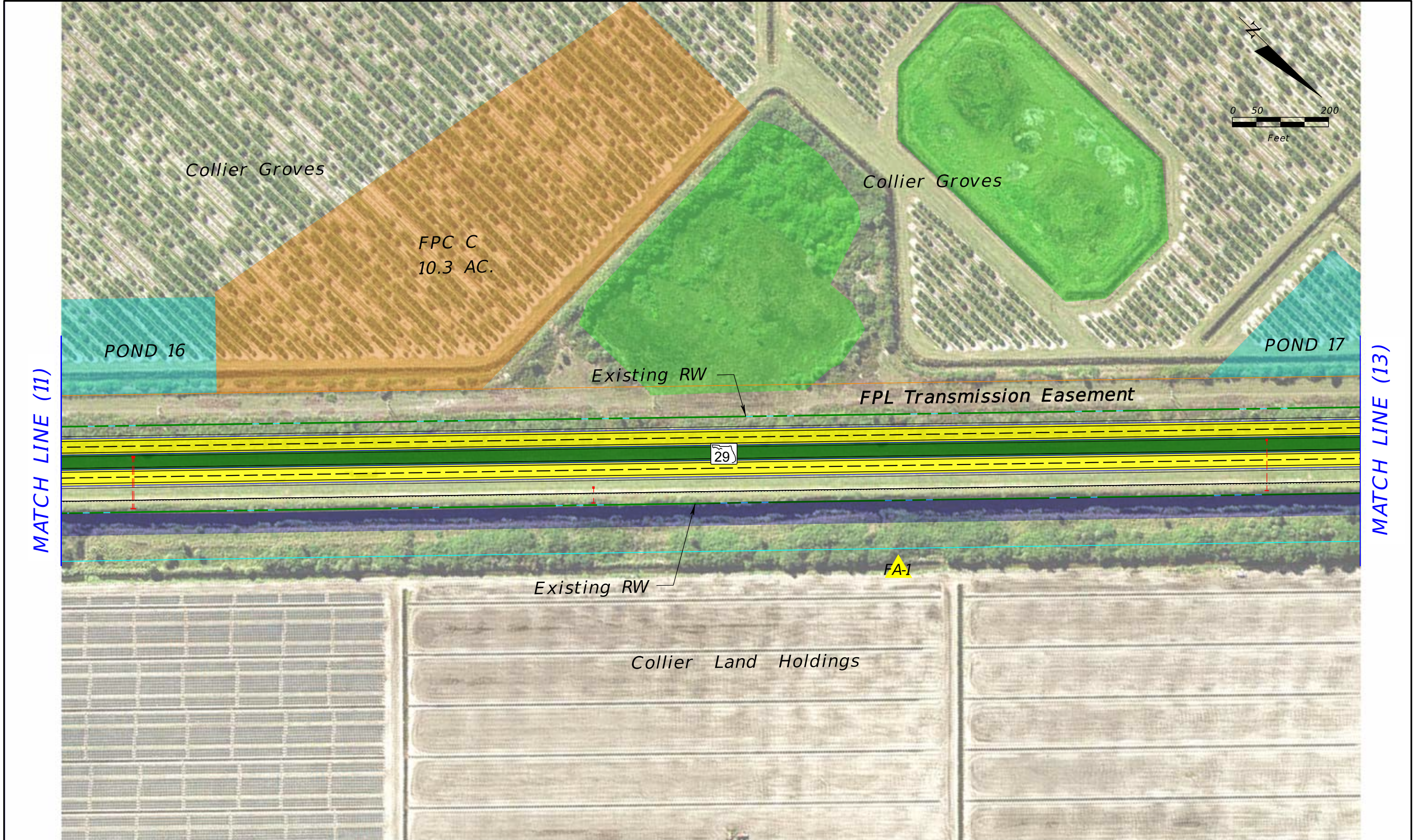
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 From Oil Well Road to SR 82
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Legend	
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Parcels	Potential Pond
Proposed Right-of-Way	Potential Floodplain Compensation
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Preferred Alternative

Sheet No.
 11



SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
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Preferred Alternative

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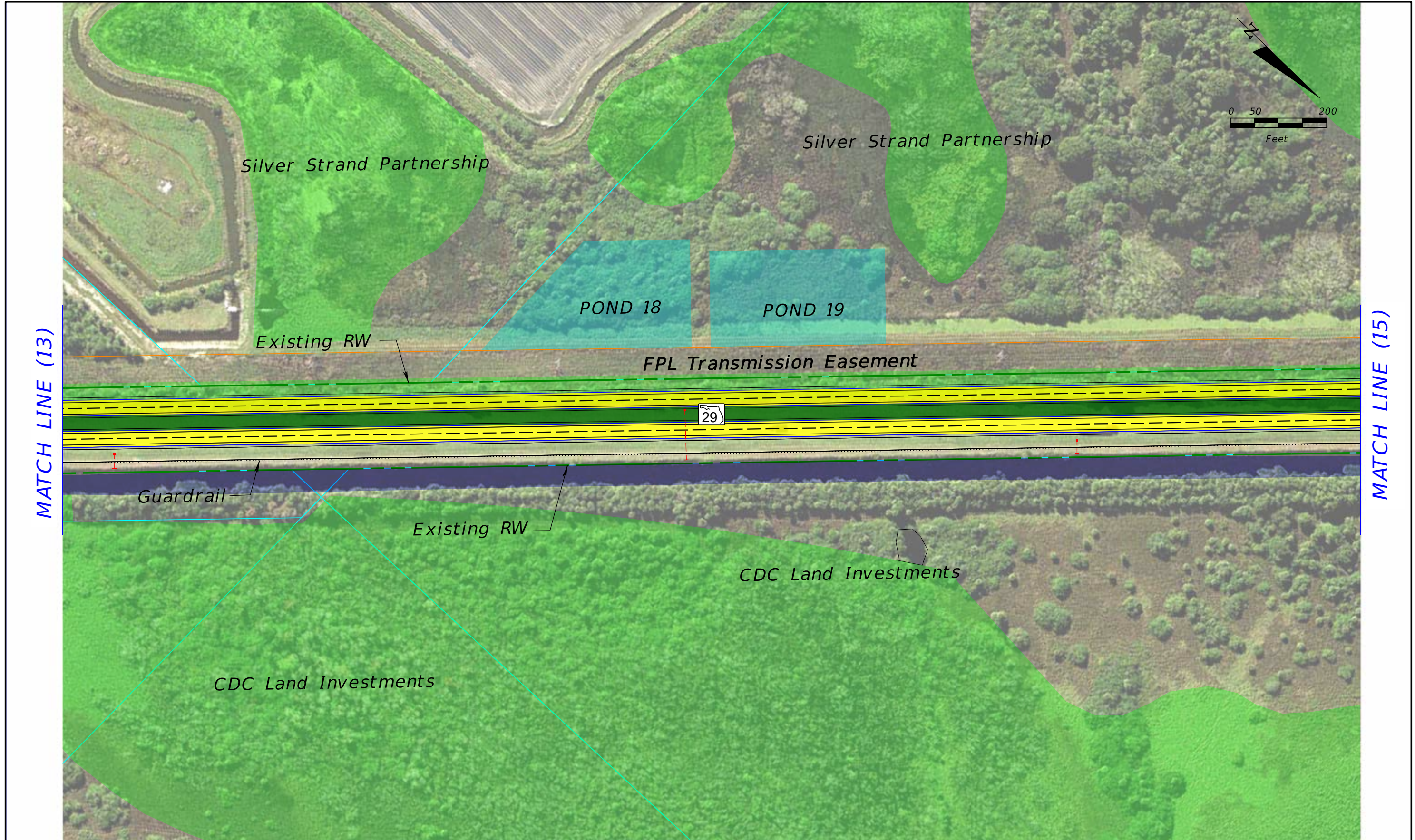
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Legend			
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	Parcels		Potential Pond
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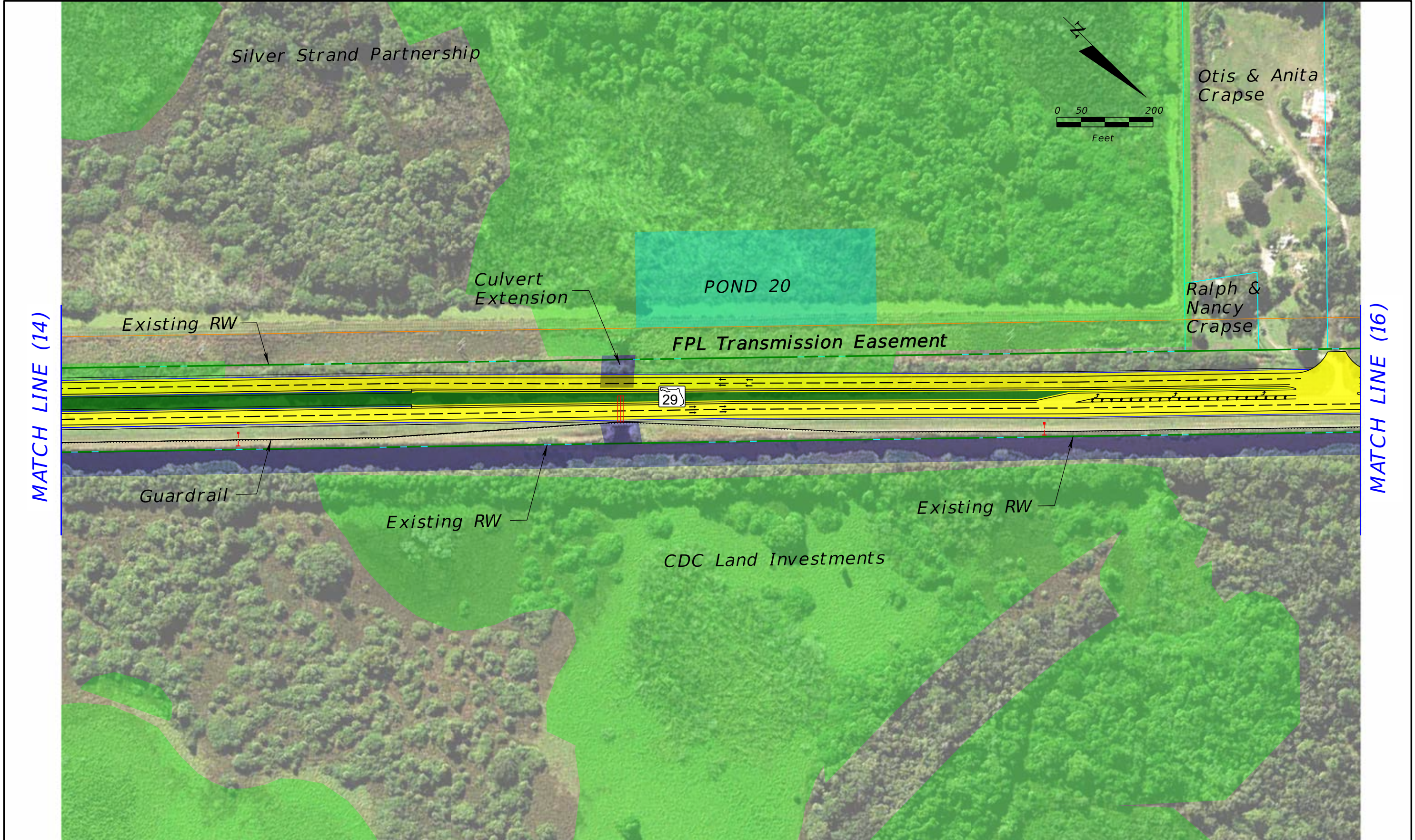
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Legend			
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
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Sheet No.
 14



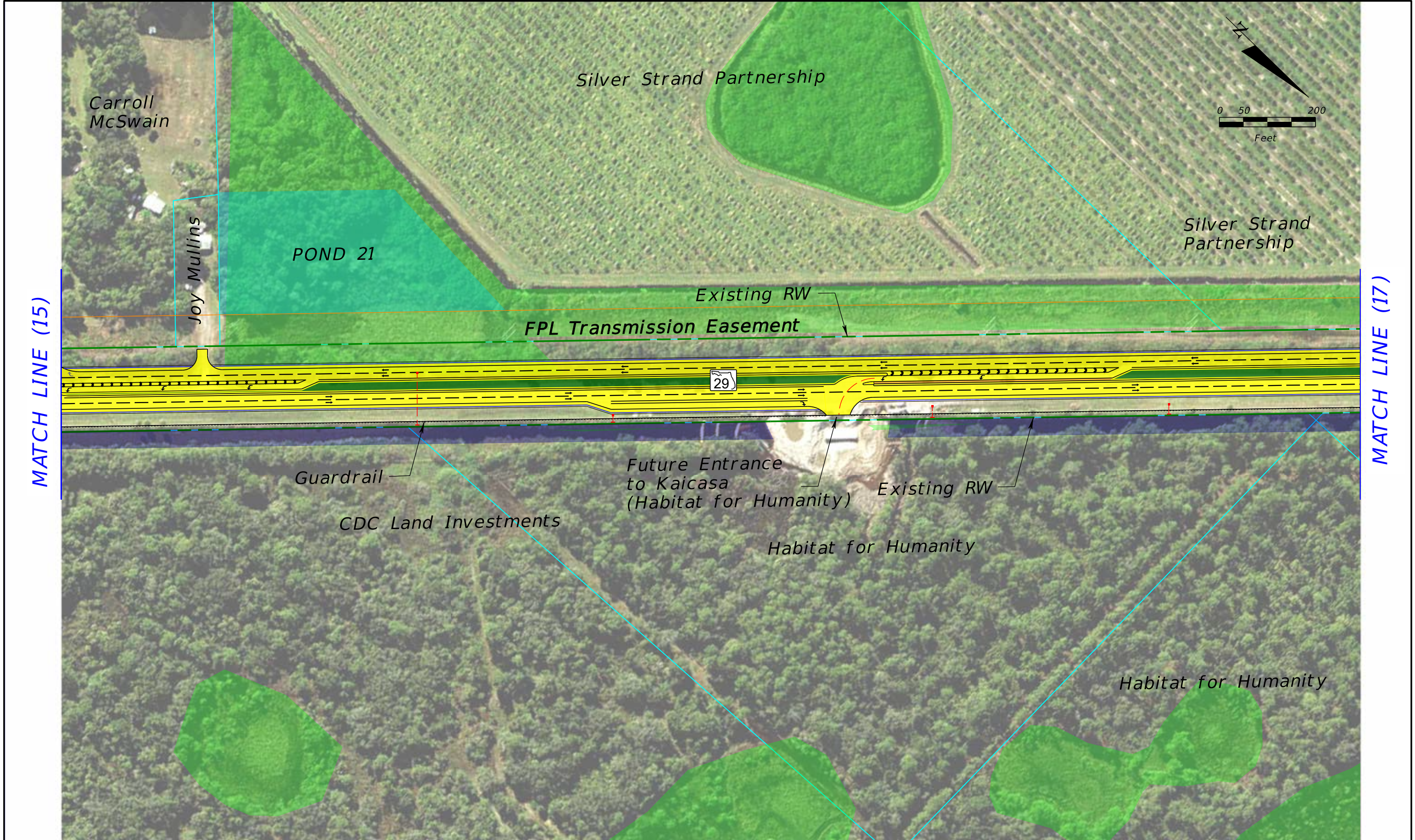
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
Existing Right-of-Way	Proposed Pond	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Floodplain Compensation	Proposed Sidewalks/ Shared Use Path	Potential Contamination (Low)
Water/Canal	Traffic Signal	Proposed Traffic Separator	Potential Contamination (Medium or High)
Wetland		Existing Drainage Structure	

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Preferred Alternative

Sheet No.
 15



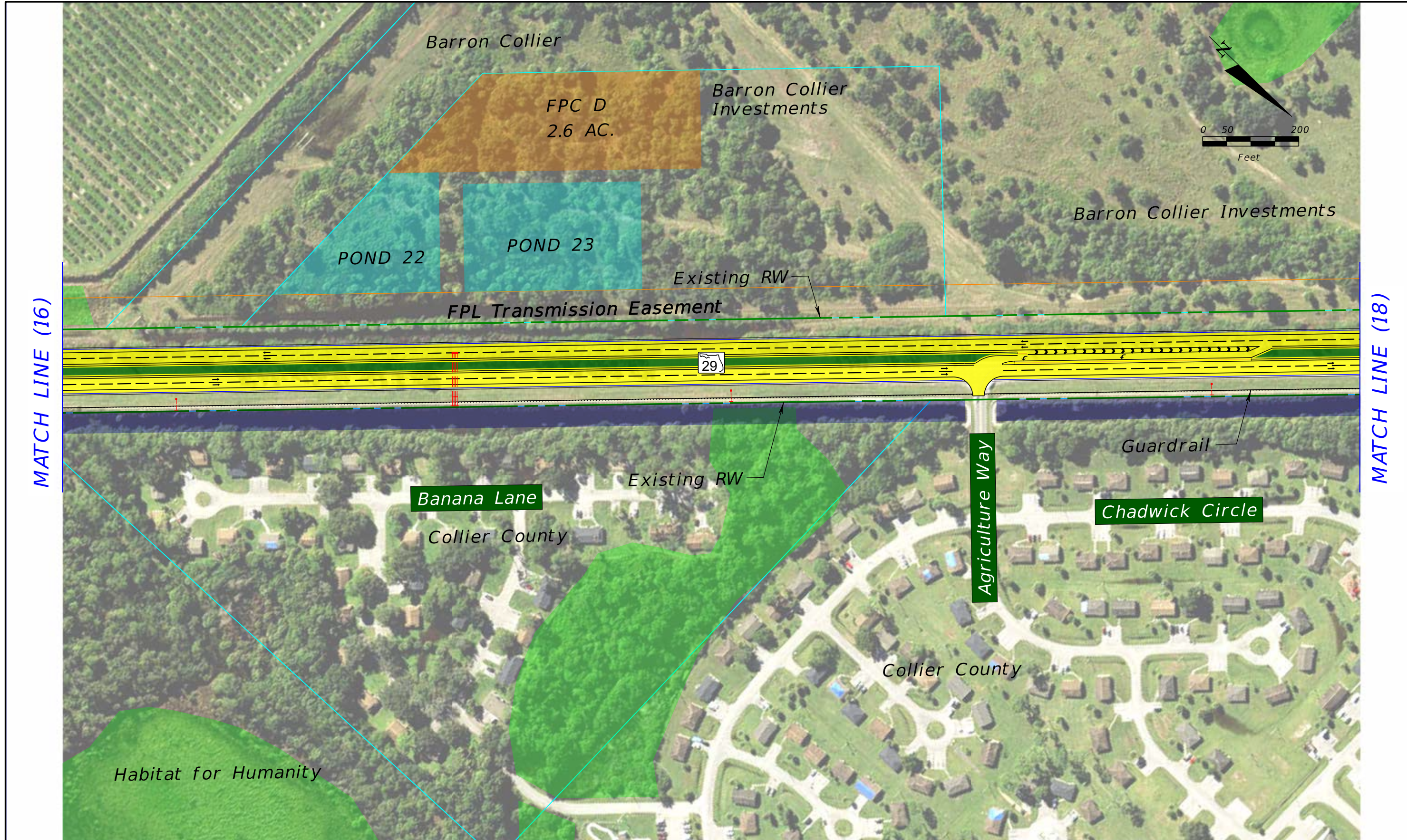
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 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks/ Shared Use Path
			Proposed Traffic Separator
			Existing Drainage Structure
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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Sheet No.
 16



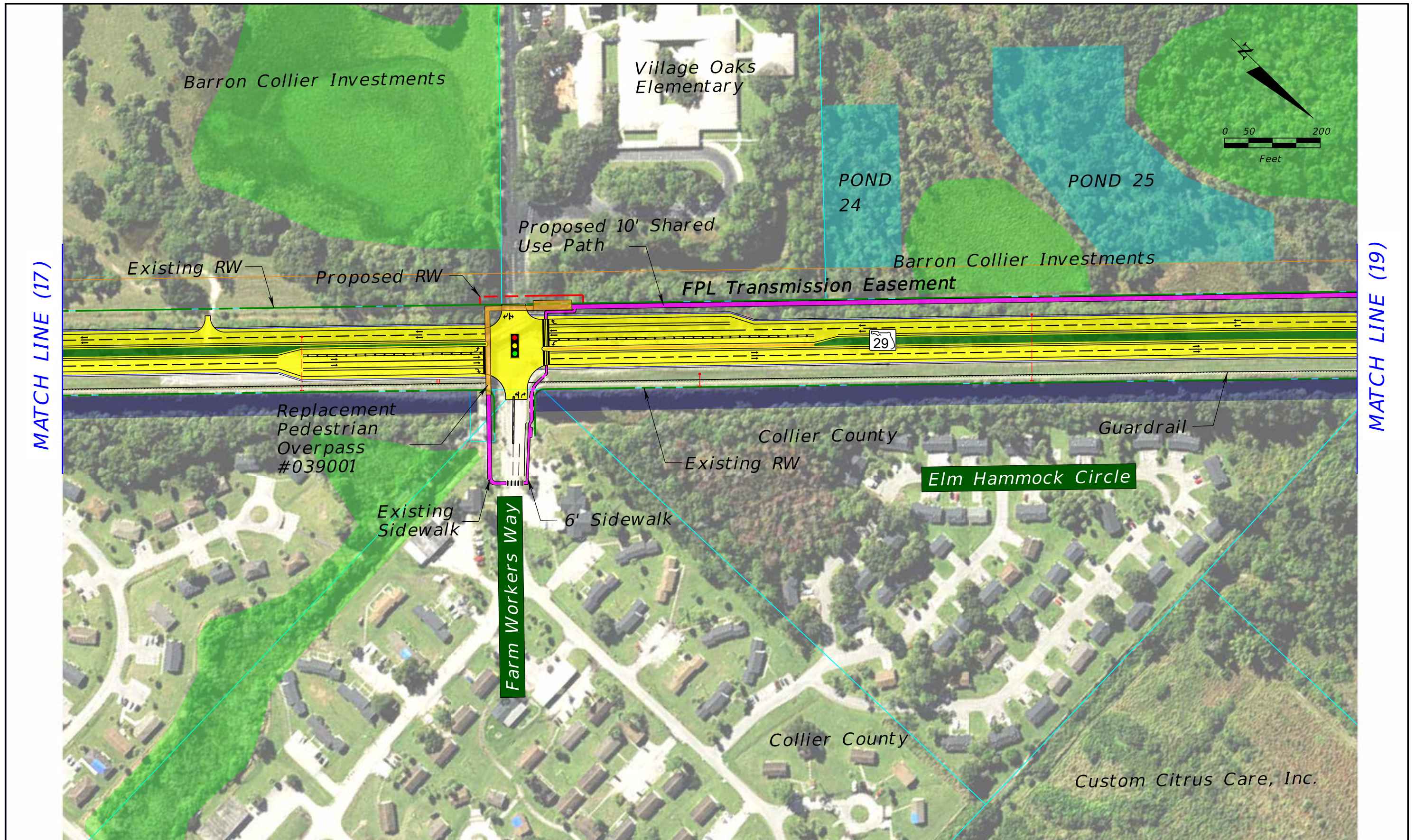
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
	Proposed Pond		Proposed Sidewalks/ Shared Use Path
	Proposed Pond		Proposed Traffic Separator
	Potential Pond		Existing Drainage Structure
	Potential Floodplain Compensation		Proposed Guardrail
	Proposed Pavement		Potential Business Relocation
	Proposed Sidewalks/ Shared Use Path		Potential Contamination (Low)
	Proposed Traffic Separator		Potential Contamination (Medium or High)
	Existing Drainage Structure		

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Sheet No.
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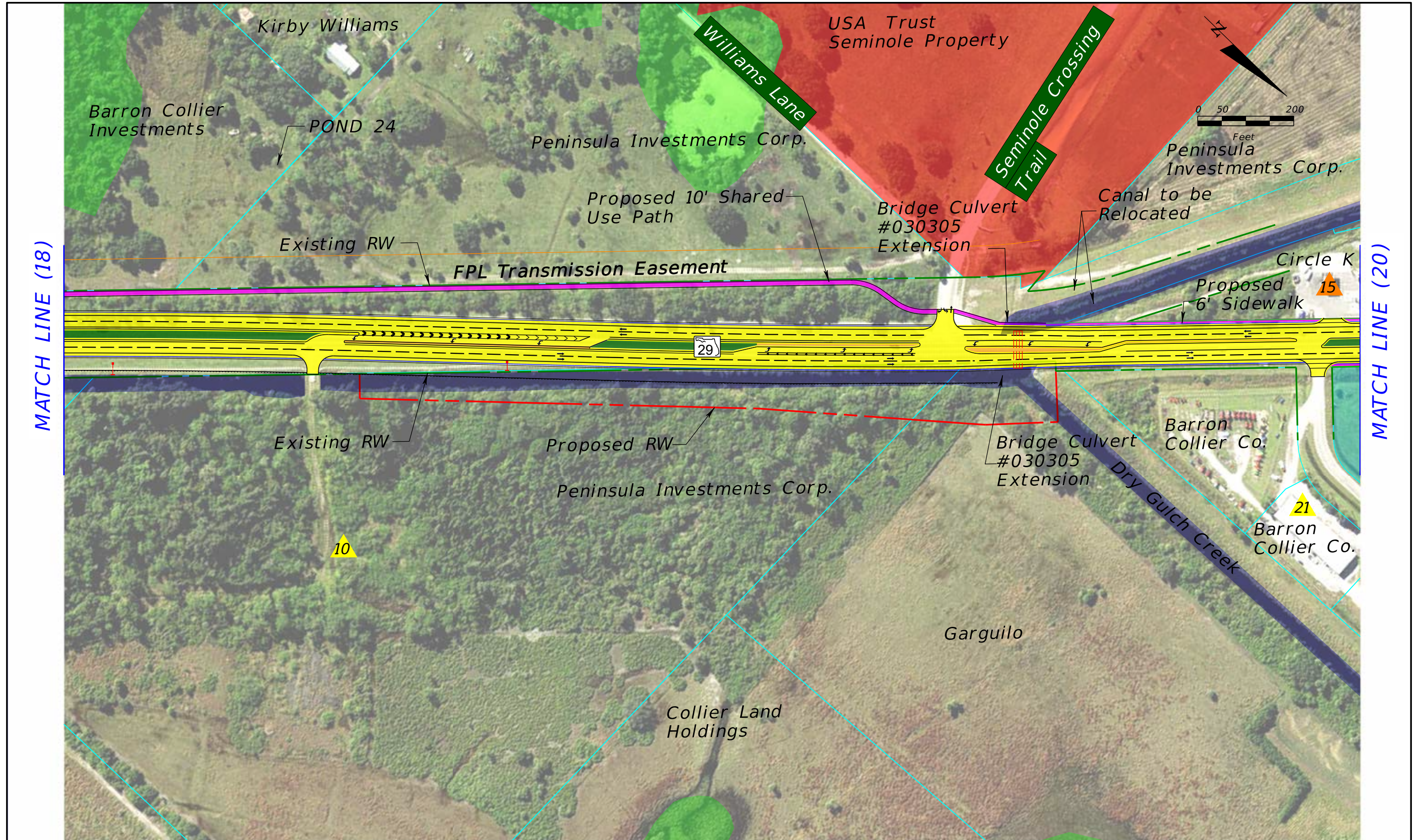
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
			Proposed Sidewalks/ Shared Use Path
			Proposed Traffic Separator
	Existing Drainage Structure		Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)
	Proposed Guardrail		Traffic Signal

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MATCH LINE (18)

MATCH LINE (20)



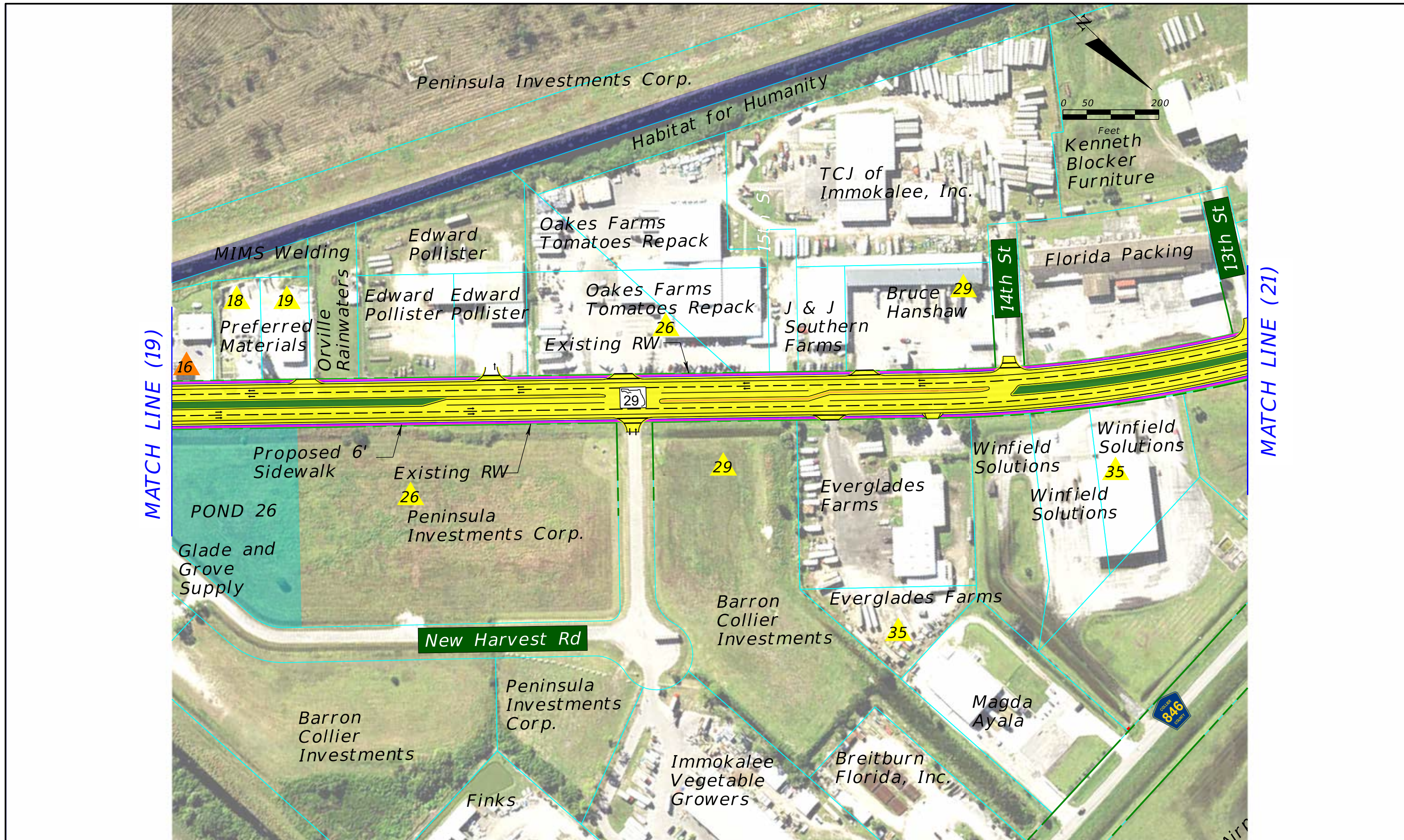
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
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	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
	Proposed Median/Border		Proposed Guardrail
	Proposed Sidewalks/Shared Use Path		Potential Business Relocation
	Proposed Traffic Separator		Potential Contamination (Low)
	Existing Drainage Structure		Potential Contamination (Medium or High)

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Preferred Alternative

Sheet No.
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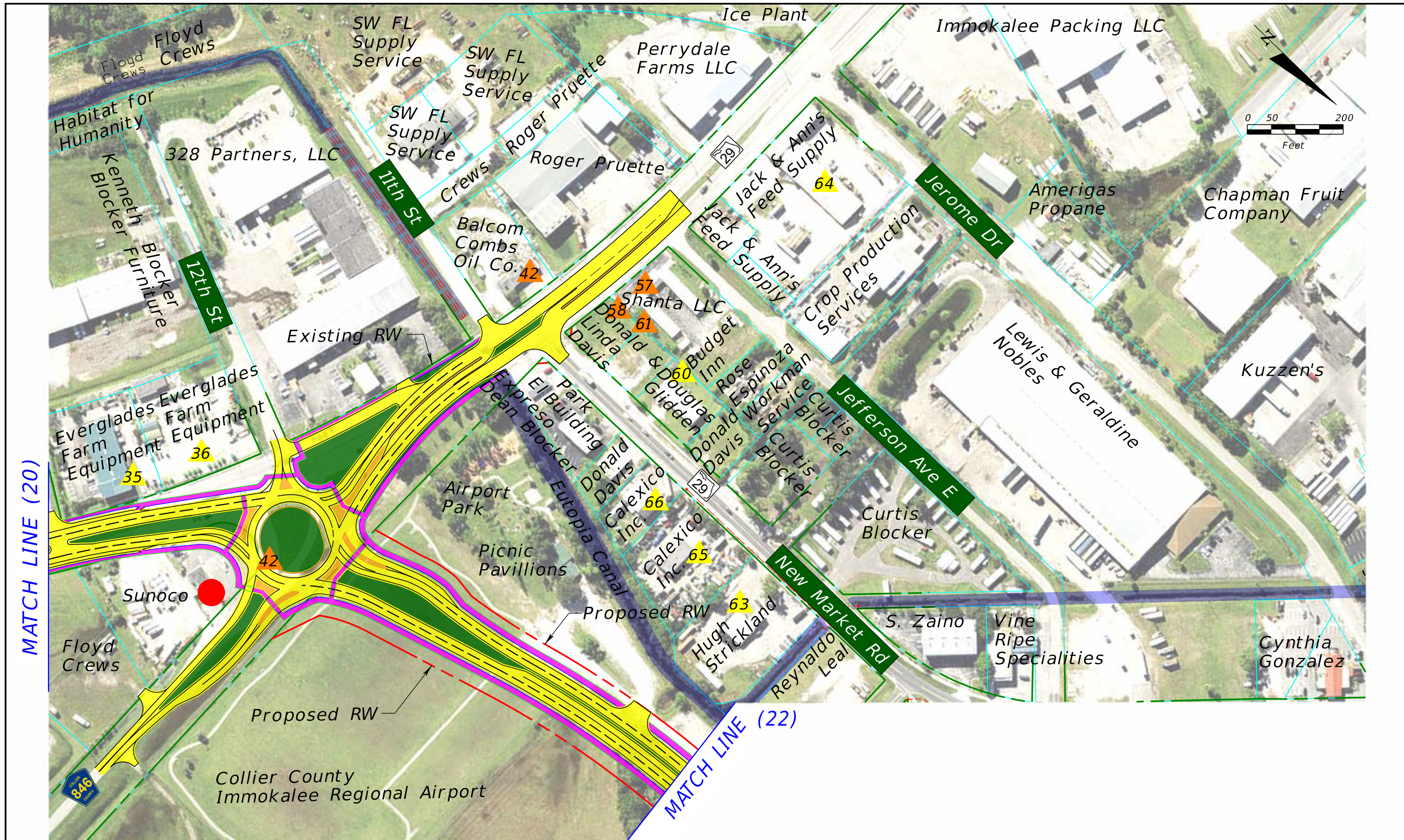
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend	
	Existing Right-of-Way
	Parcels
	Proposed Right-of-Way
	Water/Canal
	Wetland
	Proposed Pond
	Potential Pond
	Potential Floodplain Compensation
	Traffic Signal
	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks/Shared Use Path
	Proposed Traffic Separator
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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Preferred Alternative

Sheet No.
 20



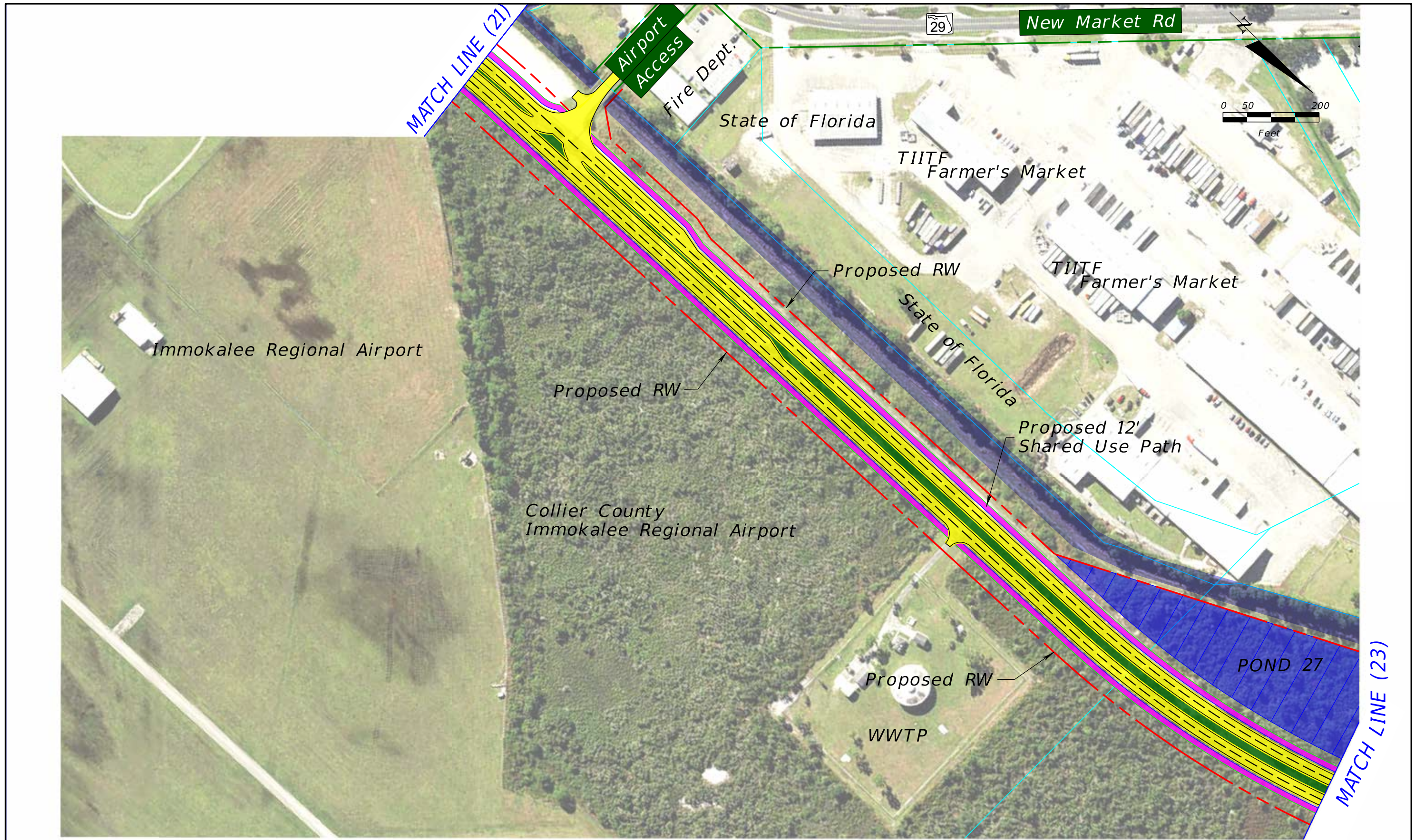
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
	Existing Right-of-Way		Proposed Median/Border
	Proposed Right-of-Way		Proposed Sidewalks/ Shared Use Path
	Water/Canal		Proposed Traffic Separator
	Wetland		Existing Drainage Structure
	Proposed Right-of-Way		Proposed Guardrail
	Potential Business Relocation		Potential Contamination (Low)
	Potential Contamination (Medium or High)		

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Preferred Alternative

Sheet No.
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SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

- Existing Right-of-Way
- Parcels
- - - - Proposed Right-of-Way
- Water/Canal
- Wetland

- ▨ Proposed Pond
- ▨ Potential Pond
- ▨ Potential Floodplain Compensation
- ▨ Traffic Signal

Legend

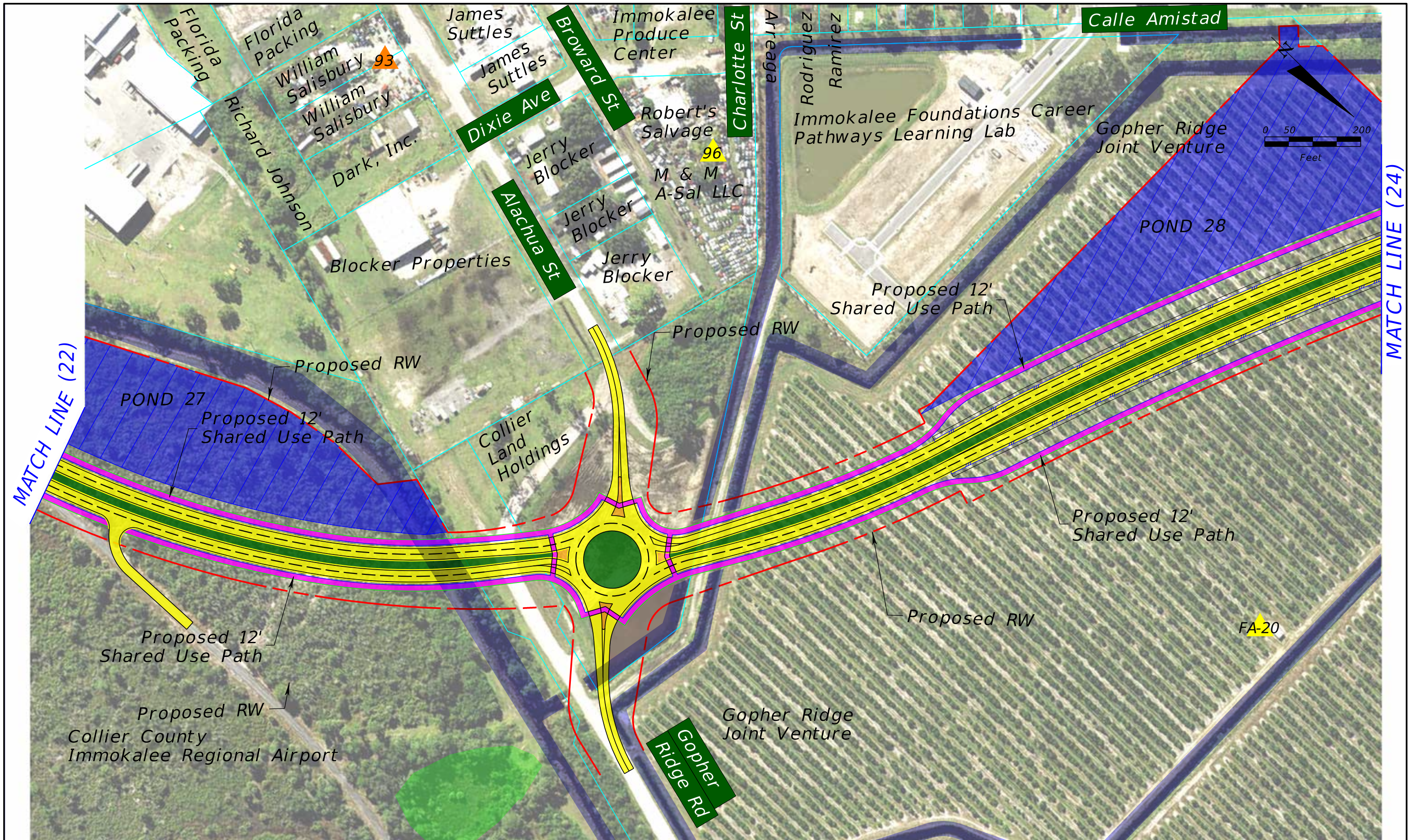
- Proposed Pavement
- Proposed Median/Border
- Proposed Sidewalks/ Shared Use Path
- Proposed Traffic Separator
- - - - Existing Drainage Structure

- Proposed Guardrail
- Potential Business Relocation
- ▲ Potential Contamination (Low)
- ▲ Potential Contamination (Medium or High)

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Preferred Alternative

Sheet No.
22



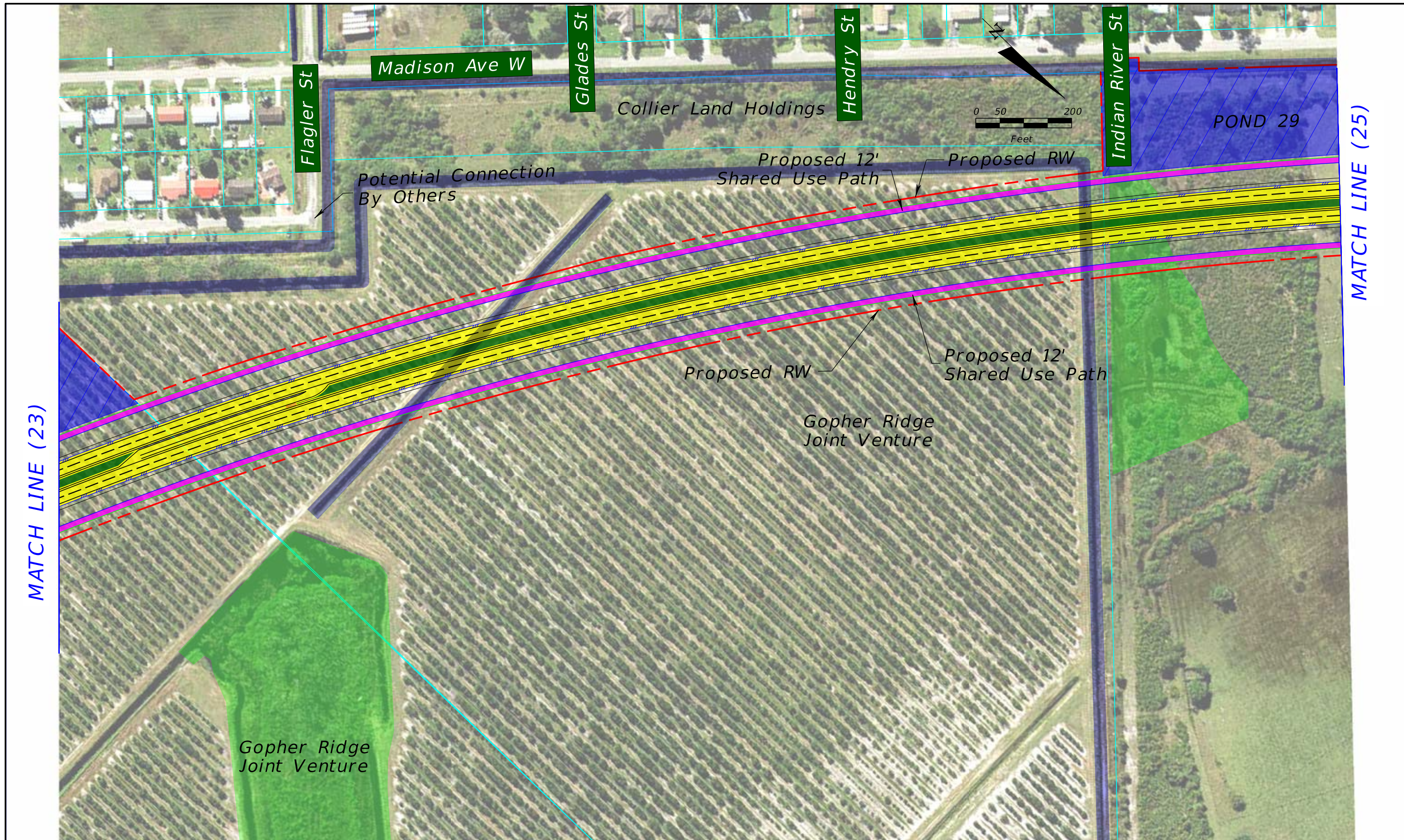
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
Existing Right-of-Way	Proposed Pond	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Floodplain Compensation	Proposed Sidewalks/ Shared Use Path	Potential Contamination (Low)
Water/Canal	Wetland	Proposed Traffic Separator	Potential Contamination (Medium or High)
Wetland	Traffic Signal	Existing Drainage Structure	

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Sheet No.
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SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

- Existing Right-of-Way
- Parcels
- - - Proposed Right-of-Way
- Water/Canal
- Wetland

- ▨ Proposed Pond
- ▨ Potential Pond
- ▨ Potential Floodplain Compensation
- ⚡ Traffic Signal

Legend

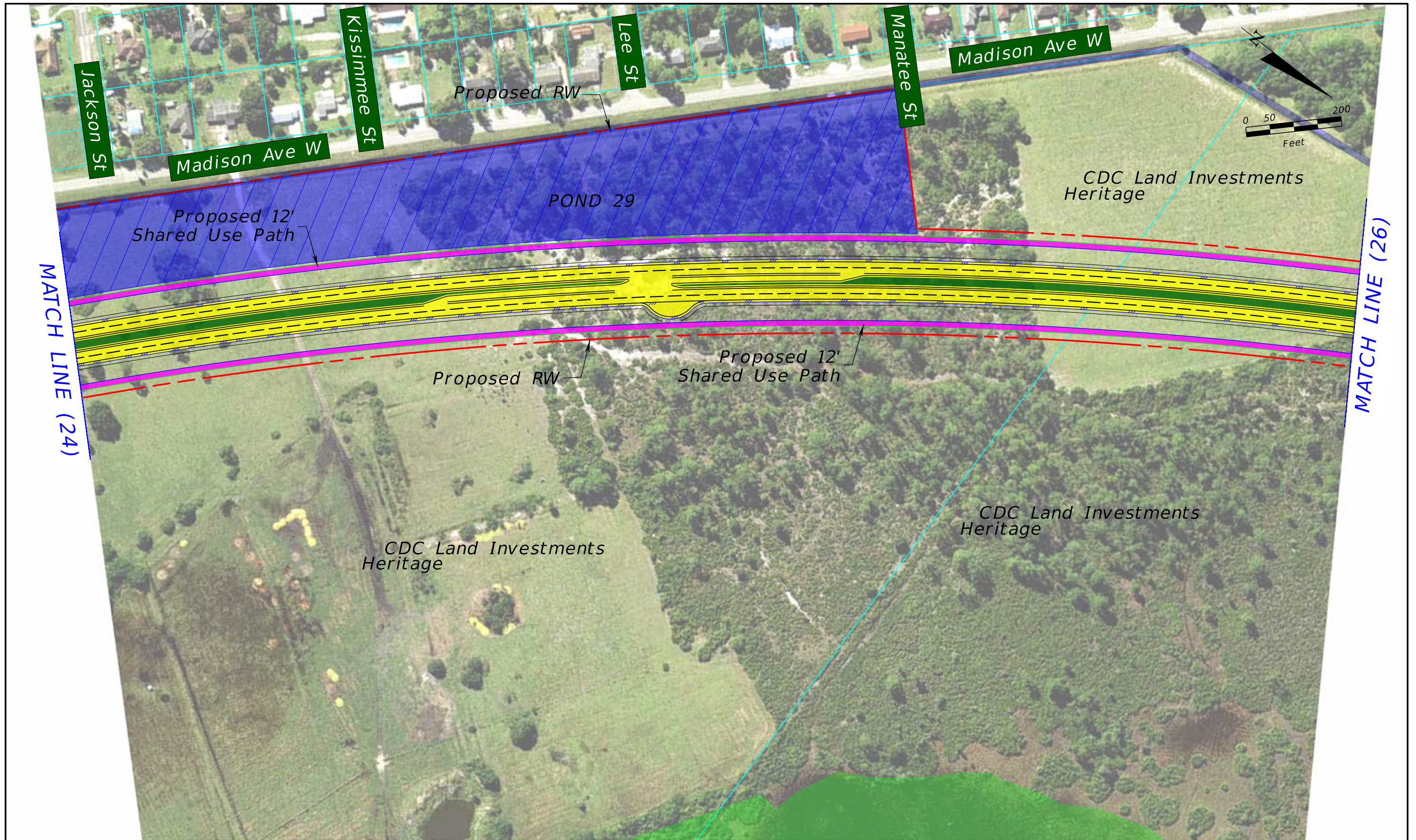
- ▨ Proposed Pavement
- ▨ Proposed Median/Border
- ▨ Proposed Sidewalks/ Shared Use Path
- ▨ Proposed Traffic Separator
- - - Existing Drainage Structure

- Proposed Guardrail
- Potential Business Relocation
- ▲ Potential Contamination (Low)
- ▲ Potential Contamination (Medium or High)

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 Tampa, Florida 33607
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Preferred Alternative

Sheet No.
 24



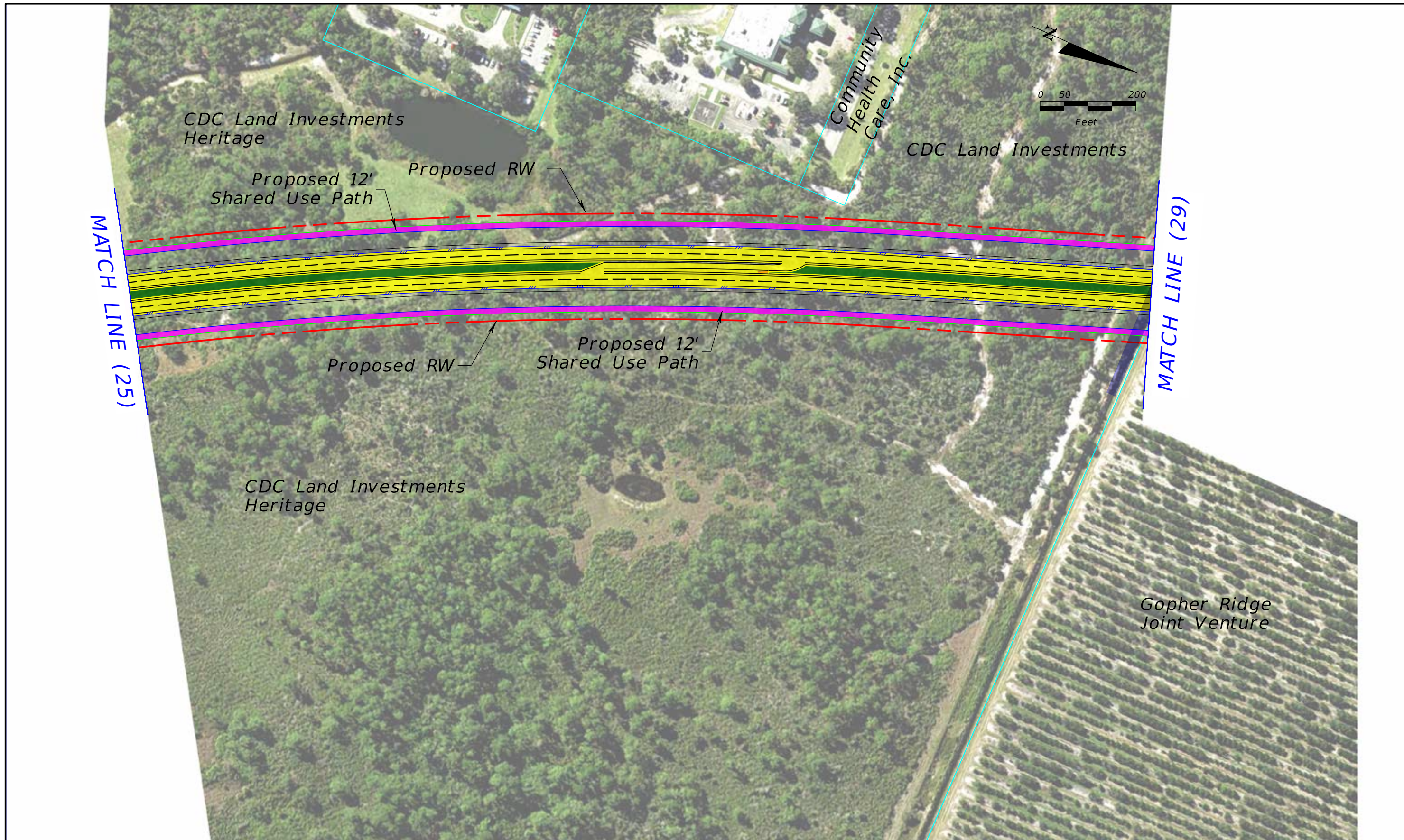
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
Existing Right-of-Way	Proposed Pond	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Floodplain Compensation	Proposed Sidewalks/ Shared Use Path	Potential Contamination (Low)
Water/Canal	Traffic Signal	Proposed Traffic Separator	Potential Contamination (Medium or High)
Wetland		Existing Drainage Structure	

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Preferred Alternative

Sheet No.
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SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
	Proposed Pavement		Proposed Sidewalks/Shared Use Path
	Proposed Traffic Separator		Proposed Guardrail
	Existing Drainage Structure		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Medium or High)
	Traffic Signal		

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Preferred Alternative

Sheet No.
 26



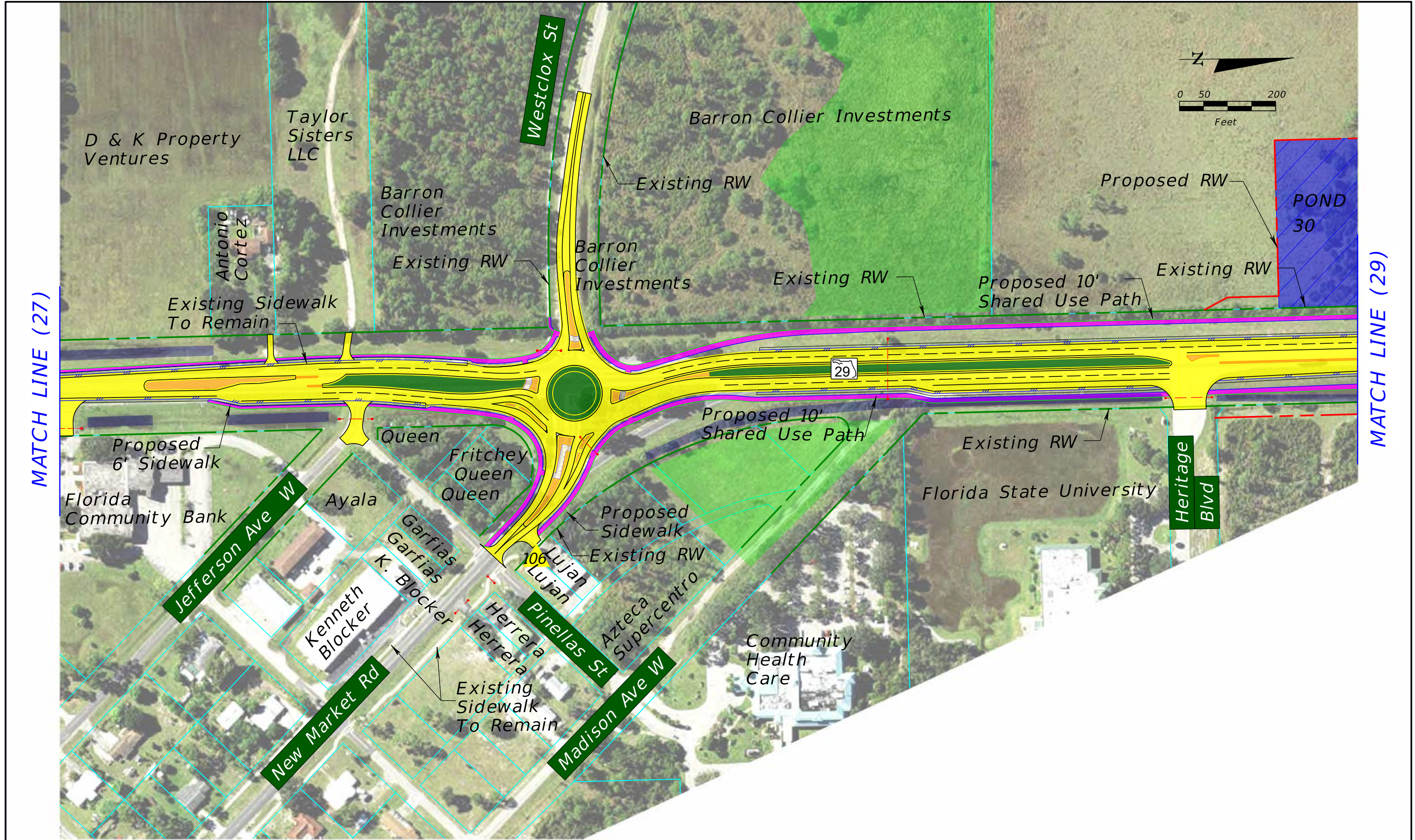
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
	Traffic Signal		Proposed Sidewalks/ Shared Use Path
			Proposed Traffic Separator
			Existing Drainage Structure
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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Preferred Alternative

Sheet No.
 27

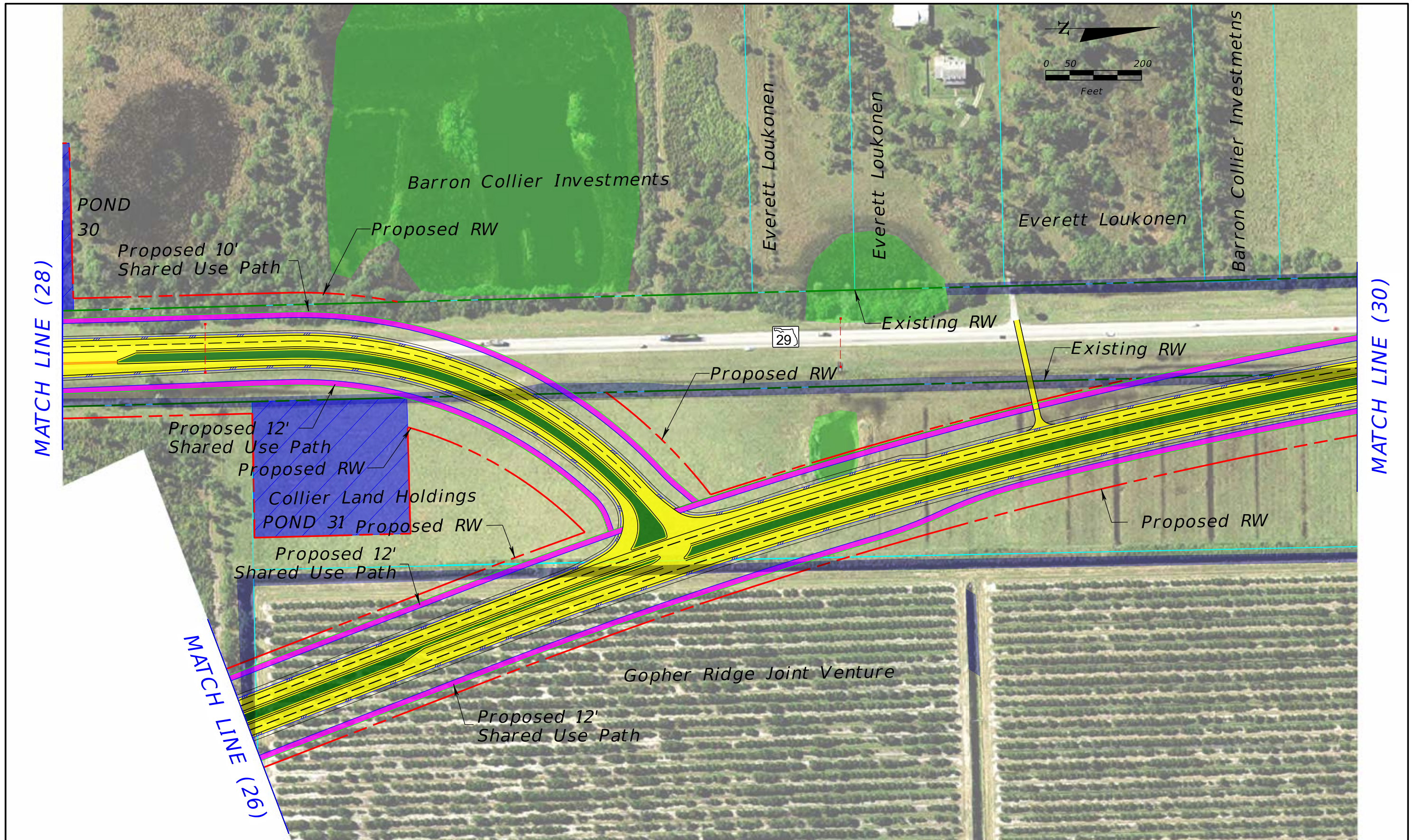


Legend	
Existing Right-of-Way	Proposed Pond
Parcels	Potential Pond
Proposed Right-of-Way	Potential Floodplain Compensation
Water/Canal	Traffic Signal
Wetland	Proposed Pavement
	Proposed Median/Border
	Proposed Sidewalks/ Shared Use Path
	Proposed Traffic Separator
	Existing Drainage Structure
	Proposed Guardrail
	Potential Business Relocation
	Potential Contamination (Low)
	Potential Contamination (Medium or High)

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Preferred Alternative

Sheet No.
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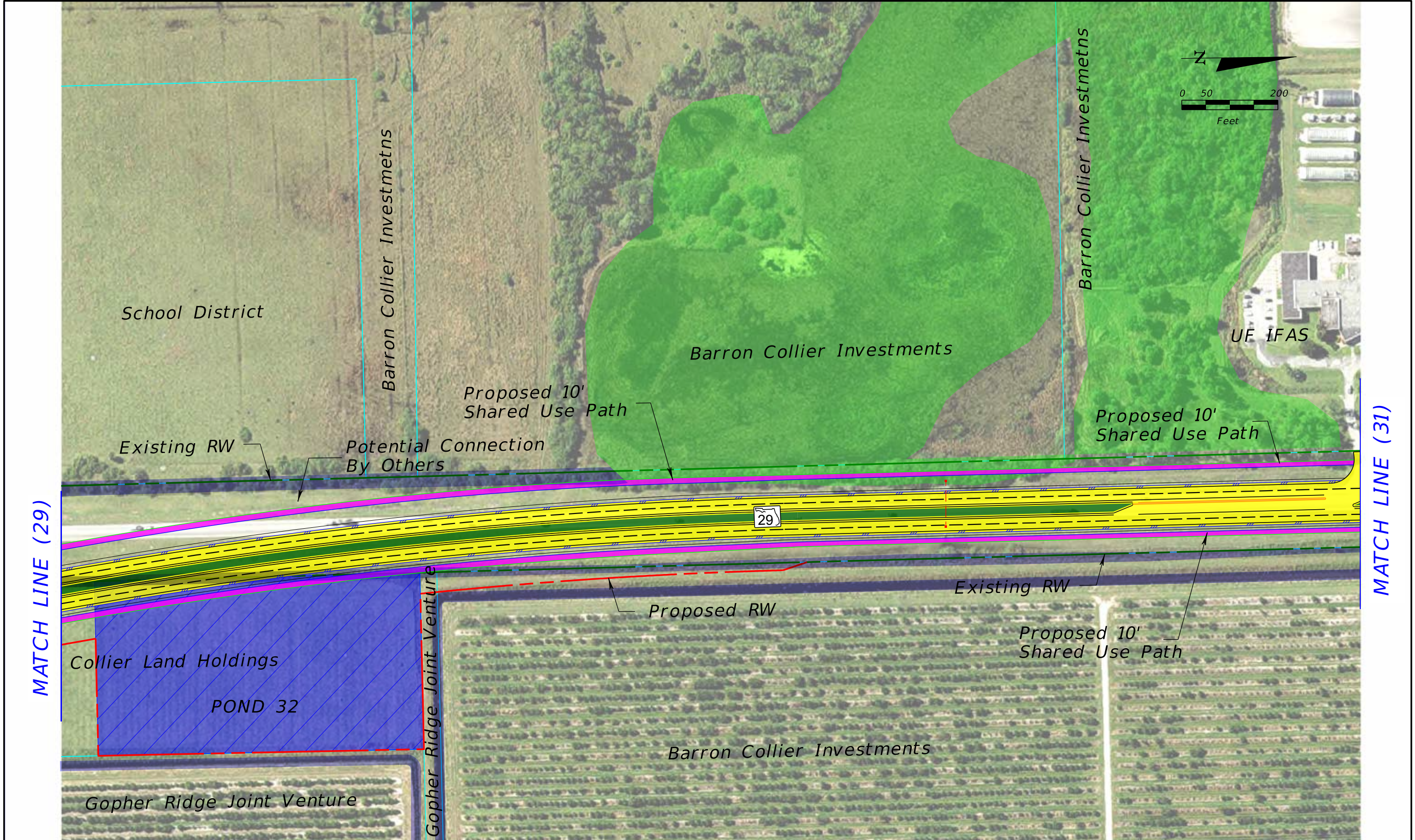
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks/Shared Use Path
			Proposed Traffic Separator
			Existing Drainage Structure
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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 Tampa, Florida 33607
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Preferred Alternative

Sheet No.
 29



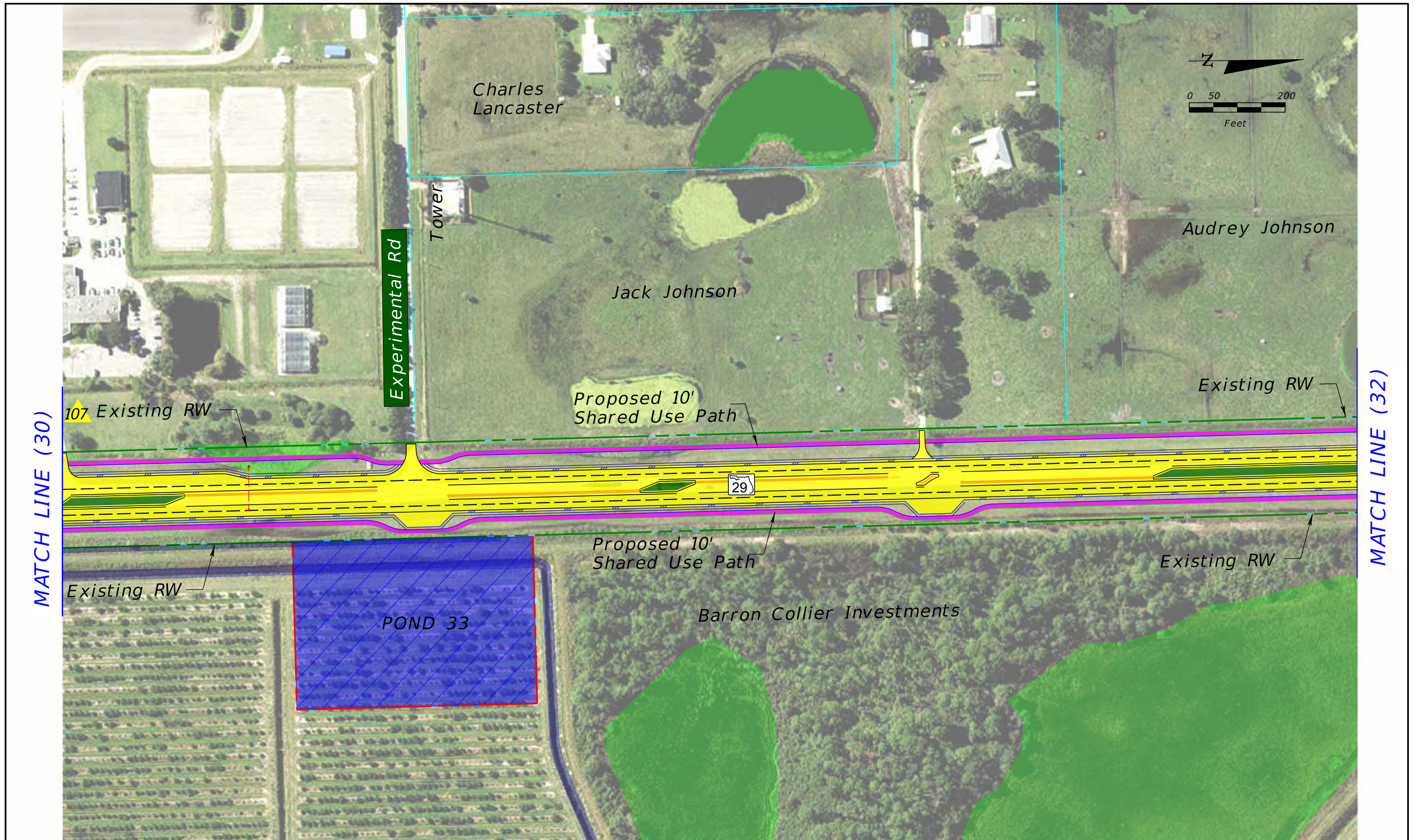
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend	
Existing Right-of-Way	Proposed Pond
Parcels	Potential Pond
Proposed Right-of-Way	Potential Floodplain Compensation
Water/Canal	Traffic Signal
Wetland	Proposed Pavement
Proposed Median/Border	Potential Business Relocation
Proposed Sidewalks/ Shared Use Path	Potential Contamination (Low)
Proposed Traffic Separator	Potential Contamination (Medium or High)
Existing Drainage Structure	Proposed Guardrail

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Preferred Alternative

Sheet No.
 30



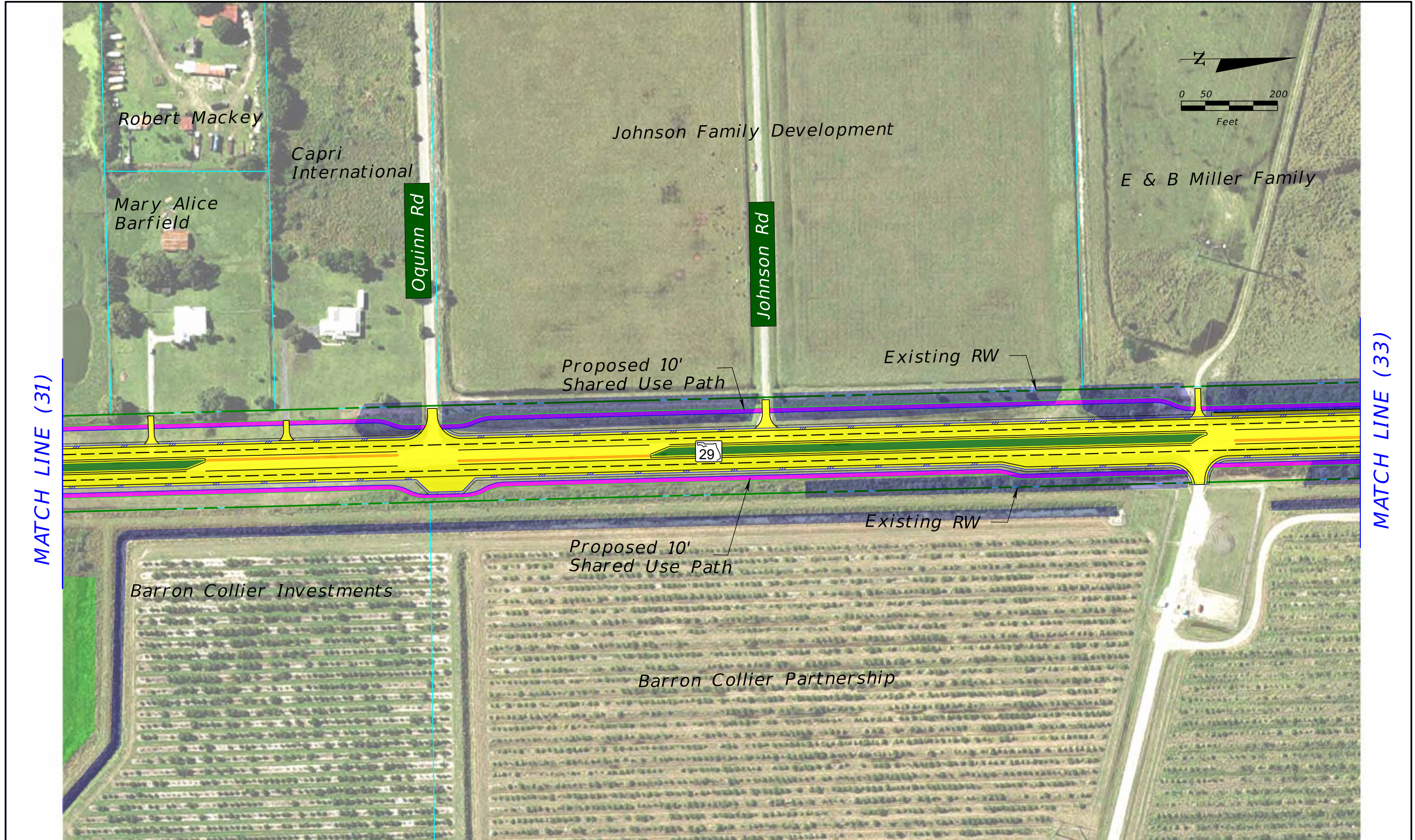
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
	Proposed Pavement		Proposed Sidewalks/Shared Use Path
	Proposed Traffic Separator		Proposed Guardrail
	Existing Drainage Structure		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Low)
	Potential Contamination (Medium or High)		Potential Contamination (Medium or High)

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Preferred Alternative

Sheet No.
31



MATCH LINE (31)

MATCH LINE (33)

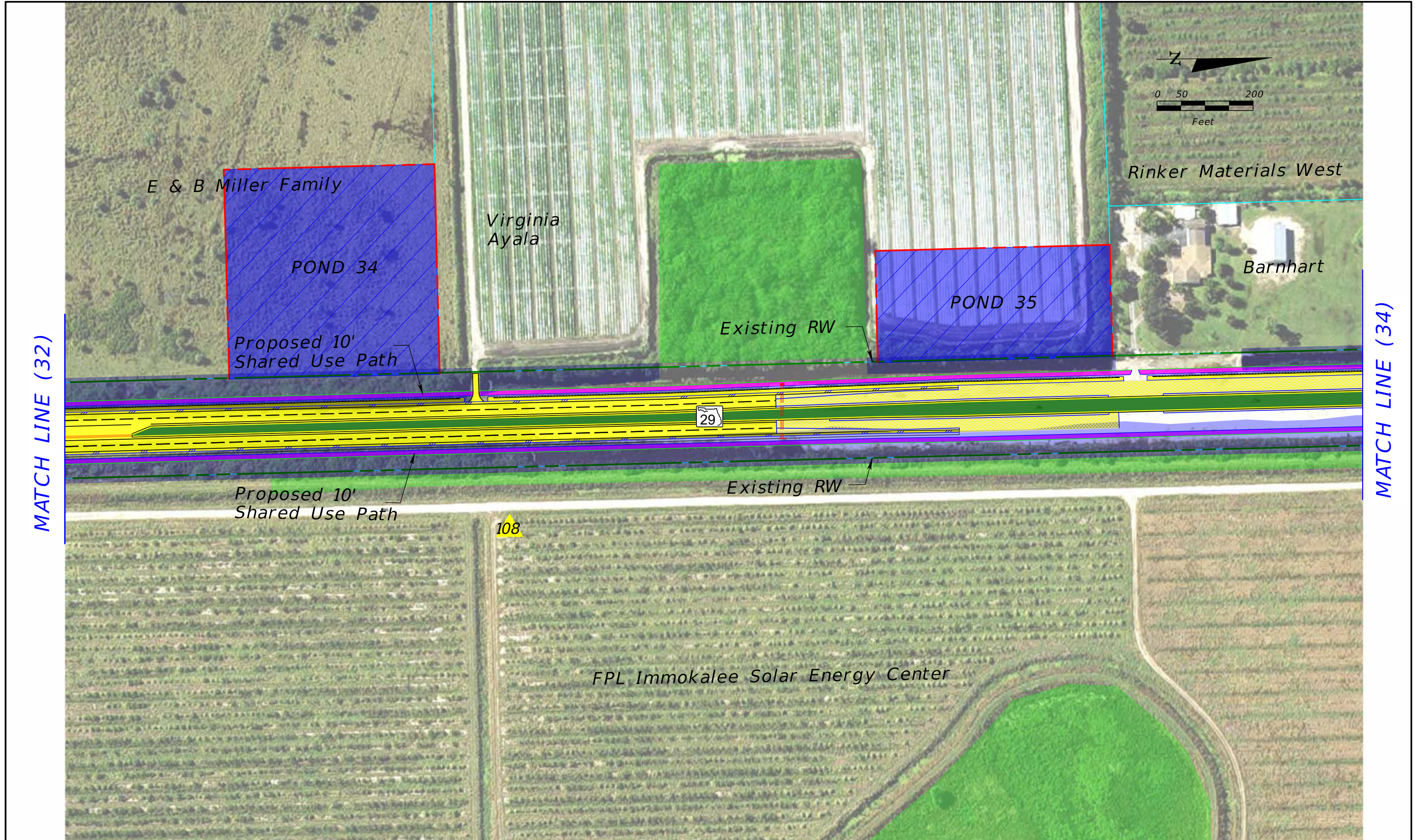
SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Traffic Signal
	Wetland		Proposed Pavement
			Proposed Median/Border
			Proposed Sidewalks/Shared Use Path
			Proposed Traffic Separator
			Existing Drainage Structure
			Proposed Guardrail
			Potential Business Relocation
			Potential Contamination (Low)
			Potential Contamination (Medium or High)

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 Tampa, Florida 33607
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Preferred Alternative

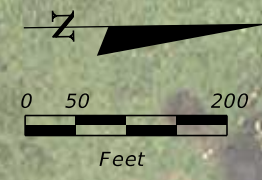
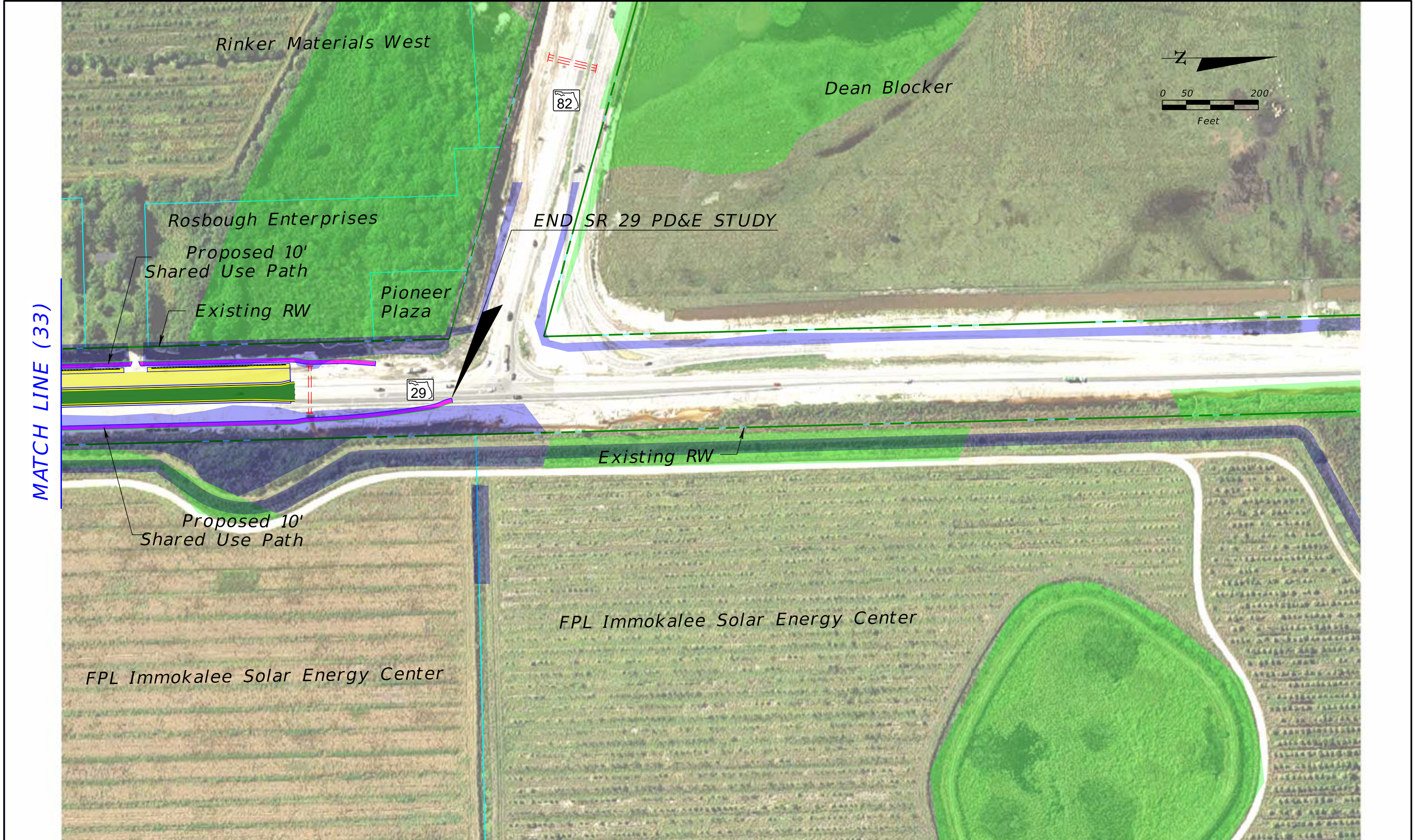
Sheet No.
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Legend			
	Existing Right-of-Way		Proposed Pond
	Parcels		Potential Pond
	Proposed Right-of-Way		Potential Floodplain Compensation
	Water/Canal		Proposed Pavement
	Wetland		Proposed Median/Border
	Traffic Signal		Proposed Sidewalks/ Shared Use Path
	Existing Drainage Structure		Proposed Traffic Separator
	Proposed Guardrail		Potential Business Relocation
	Potential Contamination (Low)		Potential Contamination (Medium or High)

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Preferred Alternative



SR 29 PD&E Study
 From Oil Well Road to SR 82
 FPID NO: 417540 1 22 01 / FAP NO: 3911 022 P

Legend			
Existing Right-of-Way	Proposed Pond	Proposed Pavement	Proposed Guardrail
Parcels	Potential Pond	Proposed Median/Border	Potential Business Relocation
Proposed Right-of-Way	Potential Floodplain Compensation	Proposed Sidewalks/ Shared Use Path	Potential Contamination (Low)
Water/Canal	Traffic Signal	Proposed Traffic Separator	Potential Contamination (Medium or High)
Wetland		Existing Drainage Structure	

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APPENDIX L

***Additional Section 4(f): Airport Park and
Airport Conservation Easement***

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) DETERMINATION OF APPLICABILITY

650-050-45
Environmental
Management
01/19

Project Name:	State Road (SR) 29 from Oil Well Road to SR 82		
FM#:	417540-1-22-01	ETDM#:	3752
FAP#:	3911 022 P		
Project Review Date:	5/10/2019		
FDOT District:	1		
County(ies):	Collier		

Project Description including Section 4(f) Specific Information:

SR 29 is designated as an Emerging Strategic Intermodal System (SIS) and is a major north-south corridor in Collier County. The project extends from Oil Well Road to SR 82 and is approximately 15.6 miles in length. The project proposes to widen existing SR 29 from two lanes to four lanes from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road (CR 29A) to SR 82, as well as include a new four-lane alignment from north of Seminole Crossing Trail to north of Westclox Street/New Market Road (CR 29A) (Central Alternative #2). Based on comments received at the Public Hearing held on November 15, 2018 and further coordination with Collier County, the Central Alternative #2 alignment was shifted to the east to avoid impacts to Immokalee Airport Park. The shifted Central Alternative #2 (Preferred Alternative) is now anticipated to impact 5.49 acres of the Immokalee Airport Conservation Easement.

Type of Property

Check all that apply:

- Public Parks and Recreation Areas
- Wildlife and Waterfowl Refuges
- Historic Sites

Description of Property: The Immokalee Airport Conservation Easement, totaling 154.28 acres, located along the western edge of the Immokalee Regional Airport property, is owned by Collier County and the easement is managed by the Florida Fish and Wildlife Conservation Commission (FWC). The deed granted by Collier County to the FWC establishes the purpose of the Immokalee Airport Conservation Easement along with the Immokalee Regional Airport Upland Management Area (which includes the easement area) (see Attachment 1). The referenced management plan (included as part of the Gopher Tortoise Incidental Take Permit issued by FWC to the airport) is provided as Attachment 2; the management plan may be found specifically within Attachment 2 on pp. 45-47 of this pdf document. The Federal Aviation Administration (FAA), in response to their review of the project Environmental Assessment, indicated that airport use is the primary purpose of the Immokalee Airport Conservation Easement; conservation use is ancillary as this area was designated for mitigation as part of the Gopher Tortoise Incidental Take Permit. FAA also requested to serve as the Official With Jurisdiction (OWJ) since the conservation easement is located on Immokalee Regional Airport property (see Attachment 3). The Florida Department of Transportation (FDOT) Office of Environmental Management (OEM) and legal staff reviewed the deed and management plan along with correspondence received from the FAA as part of their review of the Environmental Assessment. The FDOT OEM and legal staff determined that the easement serves as conservation for the airport property permit. The FDOT OEM and legal staff additionally agreed that the FAA is the OWJ over the conservation easement and concurs with FAA's determination that the primary purpose of the land is airport use. Therefore, Section 4(f) does not apply.

Criteria of Selected Property Type(s):

- Public Parks and Recreation Areas**
 - Must be publicly owned which refers to ownership by local, state or federal government
 - Ownership can also include permanent easements and long-term lease agreements
 - Must be open to the public during normal hours of operation
 - The major purpose must be for park or recreation activities
 - Must be designated or function as a significant park or recreational area.
 - Applies to the entire park or recreation area not just a specific feature

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) DETERMINATION OF APPLICABILITY

Wildlife and Waterfowl Refuge

- Must be publicly owned which refers to ownership by local, state or federal government;
 - Ownership can also include permanent easements and long-term lease agreements;
- Must be open to the public but **refuges are able to restrict access for the protection of refuge habitat and species;**
- The major purpose must be for wildlife and waterfowl refuges;
- Must be designated or function as a significant as a wildlife and waterfowl refuges; -
 - Applies to the entire wildlife and waterfowl refuges not just a specific feature

Historic Sites- includes historic buildings, historic transportation facilities, archeological sites, traditional cultural places, historic & archeological districts and historic trails.

- Must be of national, state or local significance and it must be eligible for listing or is listed in the National Register of Historic Places (NRHP); or
- If a site is determined not to be eligible OEM may determine that the application of Section 4(f) is otherwise appropriate when an official (such as the Mayor, president of a local historic society) provides information to support that the historic site is of local importance.

Does the identified resource meet all of the criteria for the selected property type?

Yes, continue to complete the form

No, STOP Section 4(f) does not apply

Identify the Official(s) with Jurisdiction (OWJ) contacted: _____

Date correspondence sent to the OWJ: [Click here to enter a date.](#)

Has the Official(s) with Jurisdiction (OWJ) responded?

Yes No

Has the 30-day response period passed since the initial OWJ correspondence was sent?

Yes No

Please answer the questions below about the resource:

Note: A potential source for this information can include the property management plan, resource website and/or communications with the OWJ (be sure to document these communications in writing).

What is the size and location of the property (include a map of the resource)?

Who/what organization owns/manages the property?

What is the primary function (activities, features and attributes) within the meaning of Section 4(f) of the facility or property?

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) DETERMINATION OF APPLICABILITY

650-050-45
Environmental
Management
01/19

█
Please describe the location of available appurtenances and facilities (e.g. tennis courts, pools, shelter houses, sports fields, beaches) on the property:

█
What is the function of/or the available activities on the property?

█
Access and Usage of the property by the Public:

█
Relationship to other similarly used lands/facilities in the vicinity:

█
Are there any unusual characteristics of the property that either limit or enhance the value of the resource? If so please explain:

█
Describe project activities that could potentially “use” the resource:

█
If applicable, give a general description of the history of the Historic Site, Archaeological Site or Historic District:

█
Based on the above information the recommended type of documentation for this property is:

Select the appropriate documentation (i.e. No Use, Exception, de minimis approval, etc.): Choose an Item

Reason the selected level is appropriate:

█
Supporting Documentation

The following items **must** be attached to this form:

1. A map of the resource based on the guidelines in Part 2, Chapter 7 of the PD&E Manual, including the proposed alternative being evaluated.
2. Statement of Significance from OWJ or FDOT’s presumption of significance.
3. Determination of Eligibility or Listing in the National Register of Historic Places, Archaeological Site (*include criterion of eligibility*) or a Historic District if applicable.

Signatures

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 14, 2016, and executed by FHWA and FDOT.

Adam Purcell, AECOM
Preparer

5/10/2019
Date

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) DETERMINATION OF APPLICABILITY

650-050-45
Environmental
Management
01/19

Gwen G. Pipkin
Environmental Manager, or designee

5/10/2019
Date

**OEM
Concurrence:**


OEM Subject Matter Expert

5/20/2019
Date

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) NO USE DETERMINATION

650-050-49
Environmental
Management
01/19

Name:	State Road (SR) 29 from Oil Well Road to SR 82		
FM#:	417540-1-22-01	ETDM#:	3752
FAP#:	3911 022 P		
Project Review Date:	5/10/2019		
FDOT District:	1		
County(ies):	Collier		

Project Description including Section 4(f) Specific Information:

SR 29 is designated as an Emerging Strategic Intermodal System (SIS) and is a major north-south corridor in Collier County. The project extends from Oil Well Road to SR 82 and is approximately 15.6 miles in length (see Attachment 2, Location Map). The project section of SR 29 specifically traverses the unincorporated community of Immokalee in eastern Collier County. SR 29 will be widened from two lanes to four lanes from Oil Well Road to south of Farm Worker Way and from north of Westclox Street/New Market Road (CR 29A) to SR 82, as well as include the addition of a new four-lane alignment from north of Seminole Crossing Trail to north of Westclox Street/New Market Road (CR 29A) (Central Alternative #2). Based on comments received at the Public Hearing held on November 15, 2018 and further coordination with Collier County, the Central Alternative #2 alignment was shifted to the east to avoid all impacts to Immokalee Airport Park (see Attachment 2, Preferred Alternative). The shifted Central Alternative #2 serves as the Preferred Alternative.

Type of Property: Public Parks and Recreation Areas

Description of Property: The Immokalee Airport Park, totaling 5.1 acres, is owned and operated by Collier County as a public recreational resource, which is open and free to the public. The Immokalee Airport Park occupies part of a Collier County owned parcel that includes the Immokalee Airport as identified by the Collier County Property Appraiser. The park is located immediately outside and adjacent to the airport as shown in the Immokalee Airport Master Plan. Additionally, the boundary of the park is defined by an airport security fence that limits access north of Airport Access Road. Access can only be gained by traveling on New Market Road and is provided on the north side of the facility through a single gate located adjacent to the gravel parking area and is not accessible from SR 29. Airport Park includes an amphitheater, children's playground, picnic pavilions, restrooms, and open space containing picnic tables and benches. On June 6, 2013, FHWA found that protection under Section 4(f) of the US Transportation Act of 1966, as amended and implemented by 23 CFR 774, is applicable to Immokalee Airport Park.

Establishing Section 4(f) Use of the Property

Will the property be "used" as defined in **Section 4(f)** Resources chapter of the FDOT PD&E Manual? Examples of a "use" include but are not limited to acquiring right of way, new easements, and temporary occupancy?

Yes

No

An explanation of the relationship between the Section 4(f) property and the project:

The Preferred Alternative (Central Alternative #2) will pass to the east of Immokalee Airport Park avoiding any permanent acquisition. There will be no temporary adverse occupancies and no proximity impacts from the project to the park which significantly impair the protected functions (see Attachment 2, Preferred Alternative). While the park is identified as a noise sensitive site in the Noise Study Report prepared for this project in July 2018, the future traffic noise levels with the proposed roadway improvements are not predicted to approach, meet, or exceed the Activity Category C Noise Abatement Criteria at the park; therefore, no constructive use is anticipated. The existing access road and the gravel parking area will be maintained; no recreational activities, facilities, or features within the bounds of the park will be impacted. The Preferred Alternative will include pedestrian and bicycle facilities along SR 29 and installation of a signalized crosswalk at the new intersection of SR 29 and CR 846, providing additional pedestrian and bicycle access to the park. No use of the park will occur.

FLORIDA DEPARTMENT OF TRANSPORTATION
SECTION 4(F) NO USE DETERMINATION

650-050-49
Environmental
Management
01/19

Documentation

The following items **must** be attached to this form to ensure proper documentation of the Section 4(f) No Use:

1. DOA package (if used)
2. Required communications with the OWJ

Signatures

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 14, 2016, and executed by FHWA and FDOT.

Adam Purcell, AECOM
Preparer

5/10/2019
Date

Gwen G. Pipkin
Environmental Manager, or designee

5/10/2019
Date

**OEM
Concurrence:**


OEM Subject Matter Expert

5/20/2019
Date

**OEM
Approval:**


Director of OEM, or designee

5/20/19
Date

APPENDIX M

STOF Concurrence Letter

SEMINOLE TRIBE OF FLORIDA
TRIBAL HISTORIC PRESERVATION OFFICE
AH-TAH-THI-KI MUSEUM

TRIBAL HISTORIC
PRESERVATION OFFICE

SEMINOLE TRIBE OF FLORIDA
AH-TAH-THI-KI MUSEUM

30290 JOSIE BILLIE HIGHWAY
PMB 1004
CLEWISTON, FL 33440

THPO PHONE: (863) 983-6549
MUSEUM PHONE: (863) 902-1113
FAX: (863) 902-1117

THPO WEBSITE: WWW.STOFTHPO.COM
MUSEUM WEBSITE: WWW.AHTAHTHIKI.COM



TRIBAL OFFICERS

MARCELLUS W. OSCEOLA JR.
CHAIRMAN

MITCHELL CYPRESS
VICE CHAIRMAN

LAVONNE ROSE
SECRETARY

PETER A. HAHN
TREASURER

February 19, 2020

Gwen G. Pipkin, CPM
District Environmental Manager and ETDM Coordinator
Florida Department of Transportation District One
Phone: 863.519.2375
Email: gwen.pipkin@dot.state.fl.us

Subject: FDOT SR 29 from Oil Well Road to South of SR 82, Collier County FL
THPO #: 0031312

Dear Ms. Pipkin,

Thank you for contacting the Seminole Tribe of Florida – Tribal Historic Preservation Office (STOF-THPO) regarding the FDOT SR 29 from Oil Well Road to South of SR 82, Collier County FL. The proposed undertaking does fall within the STOF Area of Interest. We have reviewed the documents provided and completed our assessment pursuant to Section 106 of the National Historic Preservation Act and its implementing authority, 36 CFR 800. We have no objections to the project at this time. However, please notify us if any archaeological, historical, or burial resources are inadvertently discovered.

If you have any further questions, please do not hesitate to contact us.

Sincerely,

Victoria L. Menchaca, MA, Compliance Review Specialist
STOF-THPO, Compliance Review Section
30290 Josie Billie Hwy, PMB 1004
Clewiston, FL 33440
Office: 863-983-6549 ext 12216
Email: victoriamentchaca@semtribe.com
Web: www.stofthpo.com

APPENDIX N

***FWC Concurrence Letter
for Second NRE Addendum***



September 4, 2019

**Florida Fish
and Wildlife
Conservation
Commission**

Commissioners
Robert A. Spottswood
Chairman
Key West

Michael W. Sole
Vice Chairman
Tequesta

Rodney Barreto
Coral Gables

Steven Hudson
Fort Lauderdale

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Sonya Rood
St. Augustine

Office of the
Executive Director
Eric Sutton
Executive Director

Thomas H. Eason, Ph.D.
Assistant Executive Director

Jennifer Fitzwater
Chief of Staff

850-487-3796
850-921-5786 FAX

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620 South Meridian Street
Tallahassee, Florida
32399-1600
Voice: 850-488-4676

Hearing/speech-impaired:
800-955-8771 (T)
800 955-8770 (V)

MyFWC.com

Mr. Jonathon A. Bennett
Environmental Project Manager
Florida Department of Transportation (FDOT) District 1
801 N. Broadway Avenue
Bartow, FL 33830
Jonathon.Bennett@dot.state.fl.us

Re: SR 29 from Oil Well Road to SR 82, Collier County, Natural Resources
Evaluation Report, 2nd Addendum

Dear Mr. Bennett:

The Florida Fish and Wildlife Conservation Commission (FWC) staff has reviewed the 2nd Addendum to the Natural Resources Evaluation Report (NRE) for the above-referenced project, and finds that our August 21, 2018, comments (enclosed) on the NRE and NRE Addendum remain applicable. The preferred alignment has been shifted eastward, and now will require use of an additional 1.04 acres (total of 5.49 acres) of the FWC-held Immokalee Airport Conservation Easement. The FDOT has committed to provide the FWC with compensatory land acquisition, and we look forward to working with you on this endeavor. Also enclosed is our Conservation Easement Acceptance and Release Guidelines for your consideration.

Thank you for the opportunity to review the 2nd Addendum to the NRE for the SR 29 from Oil Well Road to SR 82 project in Collier County. If you need further assistance, please do not hesitate to contact our office by email at FWCConservationPlanningServices@MyFWC.com. If you have specific technical questions regarding the content of this letter, contact Brian Barnett at (772) 579-9746 or email brian.barnett@MyFWC.com.

Sincerely,

Jennifer D. Goff, Director
Office of Conservation Planning Services

jdg/bb
ENV 1-13-2
SR 29 from Oil Well Road to SR 82 Addendum_40082_090419

Enclosures



**Florida Fish
and Wildlife
Conservation
Commission**

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800-955-8771 (T)
800 955-8770 (V)

MyFWC.com

August 21, 2018

Mr. Jonathon A. Bennett
Environmental Project Manager
Florida Department of Transportation (FDOT) District 1
801 N. Broadway Avenue
Bartow, FL 33830
Jonathon.Bennett@dot.state.fl.us

Re: SR 29 from Oil Well Road to SR 82, Collier County, Natural Resources
Evaluation Report, File Number 417540-1-22-01

Dear Mr. Bennett:

The Florida Fish and Wildlife Conservation Commission (FWC) staff has reviewed the Natural Resources Evaluation Report (NRE) and the NRE Addendum for the above-referenced project. The NRE was prepared as part of the Project Development and Environment Study for the proposed project. Since 2005, we have been involved in the review of this project via the Efficient Transportation Decision Making process as ETDM 3752, and through meetings and correspondence with FDOT District 1 and environmental resource agency staffs. We provide the following comments and recommendations for your consideration in accordance with Chapter 379, Florida Statutes and Rule 68A-27, Florida Administrative Code (F.A.C.).

Project Description

The project involves the widening of SR 29 from two lanes to four lanes between Oil Well Road and SR 82, a distance of approximately 15.6 miles, and including a new four-lane roadway bypassing the downtown area of Immokalee. The two build alternatives under consideration differ only in their alignment of the Immokalee bypass near the Immokalee Regional Airport. The Central Alternative #1 Revised runs to the west of the airport through developed land within Immokalee, while Central Alternative #2 runs through the Upland Management Area on the west side of airport property where the FWC holds a conservation easement associated with Gopher Tortoise (*Gopherus polyphemus*) Incidental Take Permit No. COL 36, and which is managed to benefit the resident Florida scrub-jays (*Aphelocoma coerulescens*). Central Alternative #2 would result in 4.45 acres of direct impact to this conservation easement. The project area is dominated by agricultural land use (pasture, rangeland, and citrus) with urban land use within the City of Immokalee. Natural land cover includes some pine flatwoods and several forested and herbaceous wetlands. The Big Cypress Area of Critical State Concern borders the east side of SR 29 in the southern portion of the project area.

Potentially Affected Resources

The NRE evaluated potential project impacts to 18 wildlife species classified under the Endangered Species Act as Federally Endangered (FE) or Threatened (FT), or by the State of Florida as Threatened (ST). Listed species were evaluated based on range and

potential appropriate habitat or because the project is within a U.S. Fish and Wildlife Service (USFWS) Consultation Area. Included were: eastern indigo snake (*Drymarchon corais couperi*, FT), American alligator (*Alligator mississippiensis*, FT based on similarity of appearance to American crocodile, *Crocodylus acutus*), Audubon's crested caracara (*Polyborus plancus audubonii*, FT), Everglade snail kite (*Rostrhamus sociabilis plumbeus*, FE), Florida grasshopper sparrow (*Ammodramus savannarum floridanus*, FE), Florida scrub-jay (FT), red-cockaded woodpecker (*Picoides borealis*, FE), wood stork (*Mycteria americana*, FT), Florida panther (*Puma concolor coryi*, FE), Florida bonneted bat (*Eumops floridanus*, FE), gopher tortoise (ST), Florida burrowing owl (*Athene cunicularia floridana*, ST), southeastern American kestrel (*Falco sparverius paulus*, ST), Florida sandhill crane (*Antigone canadensis pratensis*, ST), little blue heron (*Egretta caerulea*, ST), tricolored heron (*Egretta tricolor*, ST), roseate spoonbill (*Platalea ajaja*, ST), and Big Cypress fox squirrel (*Sciurus niger avicennia*, ST). Also evaluated were the bald eagle (*Haliaeetus leucocephalus*), which was delisted by state and federal agencies, but this species remains protected under state rule in Section 68A-16.002, F.A.C., and by the federal Bald and Golden Eagle Protection Act (16 U.S.C. 668-668d); the osprey (*Pandion haliaetus*), which is protected under the Migratory Bird Treaty Act (16 U.S.C. 703-712); and the Florida black bear (*Ursus americanus floridanus*), which is protected in Section 68A-4.009 F.A.C.

Comments and Recommendations

Due to the lack of both appropriate habitat and observation during on-site surveys, project biologists made a finding of "no effect" for the red-cockaded woodpecker and Florida grasshopper sparrow. For the other federally listed species and the gopher tortoise, the biologist's findings were "may affect, but is not likely to adversely affect". The other state-listed species were given a "no adverse effect anticipated" determination. With adherence to the project commitments, we agree with these determinations.

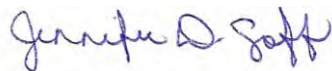
We support the project commitments for protected species, which include the following:

1. The FDOT will perform updated wildlife surveys for the species discussed in the NRE and other wildlife species during the project design phase to ascertain the involvement, if any, of listed/protected species.
2. The FDOT will coordinate further with the FWC during the project design phase for impacts associated with state-listed wildlife species.
3. A Section 7 Consultation with the USFWS will be completed during project design and permitting for the panther, scrub-jay, crested caracara, and wood stork. Appropriate mitigation will be completed for habitat impacts to these species.
4. A wildlife crossing will be constructed near the Owl Hammock curve, which has a high number of panther road kills.
5. The Standard Protection Measures for the Eastern Indigo Snake will be followed during construction.
6. For gopher tortoise burrows that cannot be avoided, the tortoises will be relocated per current FWC guidelines. For gopher tortoise survey methodology and permitting guidance, we recommend that FDOT refer to the FWC's Gopher Tortoise Permitting Guidelines (Revised January 2017) at (<http://www.myfwc.com/license/wildlife/gopher-tortoise-permits/>).

7. Should the Central Alternative #2 be selected for construction, FDOT will provide compensatory land acquisition to mitigate the loss of land within FWC's Immokalee Regional Airport Conservation Easement. As stated in the NRE Addendum, FWC has identified six priority parcels contiguous to the Platt Branch Wildlife and Environmental Area in Highlands County as preferred potential site options for mitigation.
8. The FDOT will resurvey the project limits for the presence of bald eagle nests prior to construction commencement. If a bald eagle nest is identified within the 660-foot construction buffer zone of the project area, the FDOT will coordinate with the FWS (as applicable) to secure all necessary approvals regarding this species prior to project construction.
9. The FDOT will resurvey the project limits for the presence of active osprey nests prior to construction commencement. If an active osprey nest is identified within the project area, the FDOT will coordinate with the FWC (as applicable) to secure all necessary approvals regarding this species prior to project construction.
10. The FDOT will follow the FDOT Supplemental Standard Specification 7-1.4.1 Additional Requirements for the Florida Black Bear to minimize human-bear interactions associated with construction sites during project construction.
11. Wetland impacts resulting from construction of this project will be mitigated pursuant to Section 373.4137, F.S., to satisfy all mitigation requirements of Part IV of Chapter 373, F.S., and 33 U.S.C. §1344. Compensatory mitigation for this project will be completed through the use of mitigation banks and any other mitigation options that satisfy state and federal requirements.
12. During the construction phase of this project, the FDOT will implement the Standard Specifications for Road and Bridge Construction and other best management practices to avoid, where possible, and otherwise minimize adverse impacts to wetlands and water quality within the project limits to the maximum extent practicable.

Thank you for the opportunity to review the NRE for the SR 29 from Oil Well Road to SR 82 project in Collier County. If you need further assistance, please do not hesitate to contact our office by email at FWCConservationPlanningServices@MyFWC.com. If you have specific technical questions regarding the content of this letter, contact Brian Barnett at (772) 579-9746 or email brian.barnett@MyFWC.com.

Sincerely,



Jennifer D. Goff, Director
Office of Conservation Planning Services

Florida Fish and Wildlife Conservation Commission
620 South Meridian Street, Tallahassee, FL 32399

Policy, Position Statement, or Guideline (PPG)



TYPE OF PPG: Guidelines for Accepting or Releasing Perpetual Conservation Easements

ORIGIN: Division of Habitat and Species Conservation

APPROVAL AUTHORITY: Executive Director

EFFECTIVE DATE: June 4, 2019

INTENT OF PERPETUAL CONSERVATION EASEMENTS

Acceptance of conservation easements by the Florida Fish and Wildlife Conservation Commission (FWC) is a transfer of property rights intended to protect and conserve habitat for wildlife in perpetuity. Perpetual easements are commonly provided for mitigation and conservation purposes to offset impacts. When a landowner grants FWC an easement, it is understood that the transfer of title interest is permanent, and that both parties to the easement understand the intent is for permanent conservation. As stewards for wildlife, FWC accepts these permanent easements with the understanding that the habitat will be permanently protected from development or as otherwise specified in the easement.

PURPOSE OF THE GUIDELINES

The following guidelines are to guide Florida Fish and Wildlife Conservation Commission (FWC) staff in the review, approval, monitoring, and tracking process for the acceptance of a conservation easement by the FWC on private or public land (real property) for the conservation of fish and wildlife habitat to further FWC's mission and purpose. It is meant to provide a standard set of criteria to consider across FWC, except where existing protocol exists.

Similarly, these guidelines establish criteria for FWC to consider when evaluating requests for release of conservation easements, or a part thereof, granted to FWC through a gift, donation, or any other conveyance for the purposes of conservation and/or mitigation of impacts to fish and wildlife and their habitat, where a release or a replacement protocol has not previously been approved, or as provided in a Commission-approved management plan specific to the release of conservation easements. These guidelines provide a standard set of criteria to consider across FWC programs. These criteria and procedures for releasing and replacing conservation easements where FWC is the grantee, are meant to provide for consistent, responsive decisions to landowners requesting release of an easement.

A. CONSERVATION EASEMENT ACCEPTANCE GUIDELINES

Conservation easements convey an interest in land from the landowner to the grantee accepting the easement, and as such, are a powerful tool to protect wildlife habitat. However, without proper vetting, an easement can become a burden on the holder. In order to meet its mission, FWC accepts easements that provide enduring benefits to conservation, and for which the continual management is ensured. Factors that staff must consider before recommending acceptance of an easement include:

1. Whether the easement is located in one of FWC's Priority Habitat Areas. Conservation easements proposed for lands outside FWC Priority Habitat Areas may be considered with documentation by FWC staff that the easement would provide perpetual enduring benefits for fish and wildlife resources. Forty (40) acres is the minimum size of contiguous acres that FWC will typically accept under easement; properties smaller than 40 contiguous acres require Acquisition and Restoration Council review to qualify for the tax exemption under s. 196.26, F.S. Smaller parcels may be accepted on a case-by-case basis with justification.
2. Whether the property is likely to have consistent, appropriate management. Staff should consider factors such as: whether the landowner has a management plan, if a trust or some other financial product exists to provide management funding into the future, and if the easement location is adjacent to property being managed by a governmental agency that is willing to undertake the management to protect the conservation benefits of the property.
3. Whether the FWC program recommending the easement has the structure and resources necessary to ensure monitoring and compliance. Conservation easements include terms specific to the property, landowner, or purpose for which the easement was granted. When FWC accepts an easement, it is accepting the responsibility of monitoring the property for compliance, and, if that compliance is not occurring, either accepting the responsibility for management or for enforcement of the terms of the easement.

Since easement requirements are different depending on the program accepting them, each program desiring to accept easements shall create an easement protocol, which shall be submitted to the HSC Division Director or designee(s) for approval. Conservation easement protocols shall adhere to the requirements and guidance described in these guidelines and shall explain the details of how the easement program will be implemented. Elements to be addressed include, but are not limited to, the identification of staff to run the program, program purpose and objectives, selection criteria, success measures, reporting requirements, and internal tracking and reporting. The Conservation Easement Protocol Form (available on [SharePoint](#)) shall be used to prepare and submit all protocols. Identified staff will be responsible for implementing all aspects of each approved protocol, including processing individual easements, tracking compliance, entering appropriate data into a conservation easement database, and providing relevant program status information for annual reviews. The Land Conservation Planning subsection (LCP) in the Division of Habitat and Species Conservation shall periodically review the status of all conservation easement protocols and institute changes as needed based on the respective annual program review reports. Specific characteristics of conservation easement programs that may deviate from these guidelines must be approved by the LCP.

All easements accepted by the agency shall be included in a geodatabase managed by the LCP. The LCP shall review title documents and be included and have commenting authority in the creation of easements that will be held by the agency. Initial site visit and subsequent site reviews shall be the responsibility of the Division originating the easement, with reports provided to LCP.

1. Staff **shall require** the following (referenced forms available on [SharePoint](#)) in order to start a review of a potential conservation easement:
 - a. FWC Conservation Easement Application: Submittal of an FWC Conservation Easement Application by the landowner and, if applicable, an FWC permit application.
 - b. Baseline Report: Completion of a baseline easement report and resource inventory or equivalent acceptable to FWC that documents the condition and type of habitat along with any improvements on the property.
 - c. Management Plan: Include existing habitat management plan, Landowner Assistance Program (LAP) Conservation Plan, Forest Stewardship Plan, grazing plan or equivalent. The habitat management plan accepted by FWC must include a commitment by the landowner to manage the property in perpetuity in compliance with the habitat management plan to ensure the long-term conservation of the habitat and associated fish and wildlife resources.
 - d. Endowment: The landowner must provide an endowment or equivalent FWC-approved financial assurance instrument such as FWC Species Permitting - Financial Assurance Guidelines for funding perpetual management and monitoring requirements of the proposed conservation easement. Financial assurance requirements can be waived for properties that otherwise meet FWC conservation goals.
 - e. Due Diligence: The landowner is responsible for all costs of the conservation easement grant or donation including a Title Commitment and Policy (TCP), Environmental Site Assessment (ESA), Property Boundary Survey (PBS), documentary stamp taxes, and recording fees. A waiver of the TCP and ESA or PBS may be approved by the HSC Division Director according to each conservation easement program protocol. *
 - f. No subdivision: Landowners must agree in the conservation easement documents that they will not subdivide the land held under the easement.

*Due diligence products (title work and policies, boundary surveys, Environmental Site Assessment, etc.) are to be ordered from companies on the State of Florida's [approved list of contractors](#), but may be contracted from companies not on the State's list if approved by the FWC Land Conservation and Planning Administrator.

2. Conservation Easement Application Review
 - a. Responsibilities of the Conservation Easement Project Coordinator
 - i. Each application shall be assigned to a Conservation Easement Project Coordinator who shall coordinate the preparation of the Conservation Easement Application.

- ii. The Coordinator shall document the submittal of a Conservation Easement Application by the landowner along with a copy of the recorded deed verifying ownership in the landowner's name, tax ID number of proposed conservation easement parcel, property appraiser aerial photo and/or tax map of proposed donation parcel, and a legal description and/or existing survey of the proposed conservation easement parcel.
 - iii. The Coordinator shall coordinate completion of a Conservation Geographic Information Systems (GIS) Environmental Resource Analysis of the property and obtain a shapefile or digitized boundary of the property.
 - iv. The Coordinator shall distribute the FWC Conservation GIS Environmental Resource Analysis and Conservation Easement Application for the proposed conservation easement to the appropriate FWC region and Division or Office for evaluation of the potential long-term benefits and to ensure consistency with FWC's Conservation Easement Guidelines.
 - b. Each Protocol shall state which staff are responsible for document review, who shall respond to and correspond with the applicant regarding application or due diligence deficiencies, and how the final recommendation of approval or denial shall be processed and provided to the HSC Division Director.
 - c. The HSC Division Director, in consultation with the Executive Director, shall approve or deny Conservation Easement requests.
3. Upon approval, FWC staff will proceed with coordination and preparation of the conservation easement and associated documents, obtain FWC legal approval of the easement and associated documents, and then forward them to the landowner for execution. FWC staff will include an easement acceptance page for agency signature prior to the easement being recorded in the county where the property is located. This is to make sure that FWC agrees and accepts the responsibility of monitoring and enforcing the easement into the future.
- a. Conservation easements shall be treated as contracts and routed, and a copy of the recorded easement archived in FWC's Contracts archive, unless an alternative routing and archiving process is created specifically for easements.
 - b. The Executive Director or Designee shall sign for acceptance of the easement.
 - c. In certain instances, FWC may negotiate conservation easements that may be held by other agencies or organizations. In those cases, each of those agencies or organizations policies for review and approval of conservation easements would also apply.

B. CONSERVATION EASEMENT RELEASE GUIDELINES

Conservation easements are an interest in land held for the public and a conservation purpose and are supposed to protect these interests in perpetuity. However, when landowners request the release of conservation easements, commission staff shall use the following guidelines to determine whether release is appropriate. Requestors shall provide compensation that provides a net conservation benefit. Following the guidelines will ensure that the release meets the requirement of a net conservation benefit, minimizes risks to the agency, and is treated consistently with other release requests.

1. Release Requests: Requestors must provide mitigation to permanently offset the impacts to the habitat/species due to their requested alteration to the conservation easement, even if mitigation requirements have been met for species impacts.
 - a. Avoidance – Landowner must avoid development within a conservation easement unless no other practical and prudent alternative is available, and all steps to minimize impacts as set forth below are implemented. A request to release and replace a conservation easement must include a comparison of the social, economic, and environmental effects of the alternative locations considered for the development impact and why these alternatives were not practical and/or prudent.
 - b. Minimization – Landowners requesting to release and replace a conservation easement, or part thereof, must show that adverse impacts to lands under the conservation easement will be minimized through reasonable measures where applicable by: locating the project in areas where less adverse impacts are expected, such as areas which have already been altered/impacted and are less sensitive than other areas; avoiding significant wildlife habitats, natural aquatic areas, wetlands, or other valuable natural resources; selecting areas to minimize impact to native habitat; employing best management practices in construction and operation activities; designing access roads and site preparation to avoid interference with hydrologic conditions that benefit wildlife and their habitat and reduce impacts on other wildlife resources; selecting areas that will not increase undesirable human activities on the lands under a conservation easement; and generally, not adversely impacting the habitat and species management on such lands.
2. Ideal replacement properties are the same habitat type or habitat that supports the species for which the easement was originally given and are contiguous to the effected parcel or in the same FWC region as the original easement. Replacement habitat shall be in the same condition as the easement property or better. Additional acreage may be required when the easement property being released fragments existing land under an easement, and/or is in better condition than the replacement property.
3. Compensation – If the easement replacement or modification request is accepted by FWC, monetary compensation plus habitat replacement resulting in a net conservation benefit must be received by FWC in conjunction with easement release/replacement acceptance. For example, if a landowner requests to be released from 10 ac of a 100 ac easement, the landowner must pay FWC an amount not less than the fair market value of the 10 ac, plus provide a property interest in similar or like habitat replacement according to the following criteria.
 - a. The applicant will pay the FWC an amount not less than the fair market value of the interest acquired in the parcel on which the linear or non-linear facility and related appurtenances will be located.

- i. Funds provided to FWC or a 3rd party non-governmental organization for management and/or in-kind services in lieu of habitat replacement, will not be considered as a “net conservation benefit.”
 - b. The applicant will provide to FWC the appropriate measure of additional land necessary to offset the actual acres of habitat proposed for release. FWC permits may also be required if impacts to protected species are likely by the proposed work.
 - i. Where habitat and wildlife corridor(s) will still exist (post- easement modification), the preferred “net conservation benefit” is 1.5 times the released acreage of like-habitat contiguous to the easement or lands managed by FWC (or other approved public agency) that yields a conservation benefit to wildlife (refer to the Conservation Easement Acceptance Guidelines above for easement acceptance criteria).
 - ii. Where habitat and wildlife corridor(s) will not exist (post- easement modification), the preferred “net conservation benefit” is 3 times the released acreage of like-habitat contiguous to the easement or lands managed by FWC (or other approved public agency) that yields a conservation benefit to wildlife (refer to the Conservation Easement Acceptance Guidelines above for easement acceptance criteria).
 - iii. In both scenarios, FWC will consider the replacement habitat ratio with the quality of the release habitat compared to replacement habitat, adjacency to other managed protected lands, and wildlife occurrences on the proposed property for release and replacement property.
 - iv. Requestor shall work with staff to determine suitable replacement property.
 - c. Compensation requirements specific to linear facilities – Only after all efforts to avoid and minimize impacts (see 1a and b above) to the lands protected under the conservation easement have been exhausted, FWC will consider easement modification requests for linear facilities and related appurtenances (e.g., electric, telecommunications or pipeline transmission and distribution facilities) for the purposes of providing services for public benefit based on the following criteria.
 - i. If the end result **will not** result in a permanent loss of habitat and the land will continue to provide wildlife habitat and corridors which retain prohibitions on development and conversion, the request must include a proposal for the compensation described in a. The existing easement must also be modified, and the allowable future use of the linear facility must be incorporated into the existing easement.
 - ii. If the end result **will** result in a loss of habitat or impact to the wildlife, the request must include a proposal for compensation described in a. *and* b. The existing easement must also be modified, and the allowable future use of the linear facility must be incorporated into the existing easement.

4. Violations - In cases where there has been a violation of the conservation easement, a release request will only be considered if the landowner provides compensation at a minimum as listed in both 3a and 3bii above.
5. Temporary impacts - Where the landowner is requesting a release due to an impact that is temporary and does not result in permanent loss of habitat, FWC and the landowner may agree to a temporary easement modification with appropriate mitigation for the habitat impacts, in lieu of release.
6. Incentive-based easements - FWC will not consider a release if the easement is provided through an FWC conservation incentive or regulatory program and the landowner has received any portion of the incentive or benefit (e.g., 3rd party mitigation income, or conservation incentive payments) under that easement from that program. If the landowner has not received any portion of the incentive or benefit (e.g., 3rd party mitigation income, or conservation incentive payments), FWC will consider the release in accordance with 1 above.

C. APPLICABLE CONSTITUTIONAL PROVISIONS, STATUTES AND RULES

1. Article XII, Section 28, Florida Constitution; Sections 193.501, 196.26, 259.105, 379.212
2. 704.06 Florida Statutes (F.S.).
3. FWC Internal Management Policies and Procedures (IMPP) 4.2. Contract Routing

D. DEFINITIONS

- **Conservation Easement Project Coordinator** - Conservation Easement Project Coordinators are the lead staff within each respective program office designated for coordinating evaluation and consideration of conservation easements.
- **Conservation easement tracking system** – LCP shall develop and maintain a centralized Geographic Information Systems database to record specific data sets related to all conservation easements held by FWC. This database shall be used for documenting, managing and monitoring conservation easements.
- **FWC Priority Habitat Areas:** FWC priority conservation areas are those areas identified by FWC as meriting strategic conservation priority and include, but are not limited to, Strategic Habitat Conservation Areas (SHCA), Critical Lands and Waters Identification Project (CLIP Priority I and II areas), Florida Forever Project Boundaries, other approved Public Conservation Acquisition Project Boundaries, FWC Florida Forever List Lands, FWC Optimum Conservation Planning Boundaries, FWC Wildlife Conservation Prioritization and Recovery (WCPR) Focal Species Habitat, FWC Landowner Assistance Program (LAP) Focal Areas, U.S. Fish and Wildlife Service's (USFWS) Critical Habitat, FWC Critical Wildlife Areas, USFWS or FWC Conservation Banks, FWC Mitigation Banks, Wetland Mitigation Banks, Species Specific Management Plans and Priority Habitats (for example; Bald Eagle Protection Sites and Gopher Tortoise Priority Habitats and Recipient Sites), and State Wildlife Action Plan Priority Habitats.

- **Net conservation benefit** - any effective action or transaction which promotes the overall characteristics of protected land under a conservation easement to benefit wildlife and their habitat. It is compensation over and above the market value of the affected parcel to offset any requested use or activity which would preclude or affect, in whole or in part, current or future uses of the natural resource lands. With regard to these guidelines, net conservation benefit shall not be solely monetary compensation, but shall include monetary compensation and at least 1.5 acres for every acre released of wildlife habitat in similar or better condition.
- **Perpetual conservation easement** – means a right or interest in real property which is appropriate to retaining land or water areas predominantly in their natural, scenic, open, agricultural, or wooded condition; retaining such areas as suitable habitat for fish, plants, or wildlife in perpetuity and without an expiration date. Because perpetual conservation easements are binding on future owners, the resource values of these properties are protected in perpetuity

E. IMPLEMENTATION PROCEDURES

Once reviewed by the appropriate program/contract manager where the easement applies, the program/contract manager must consult with the Legal Office for consistency with these guidelines and other applicable laws and rules prior to presenting to their Division for approval. Once the release or modification is approved, the amended conservation easement or release approval letter must be routed through LCP and the legal office and signed by the Executive Director or their delegate.

APPROVED:

Thomas H. Egan

Executive Director or Designee

6/4/19

Date

APPENDIX O

Public Hearing Transcript and Certification

PUBLIC HEARING TRANSCRIPT CERTIFICATION


I hereby certify that on November 15, 2018, beginning at 6:00 p.m., I presided over a Public Hearing for the following project:

**SR 29 Project Development and Environment (PD&E) Study
from Oil Well Road to SR 82
Collier County, Florida
FPID No: 417540-1-22-01**

I further certify that the subject public hearing was conducted relative to the economic and social effects of the location and design concept for the subject project and its impact on the environment, that a transcript was made and the document attached herein is a full, true, and complete transcript of what was said at the hearing, and that the Florida Department of Transportation has considered the social, economic, and environmental effects of the proposed improvement and is of the opinion that it is properly located and should be constructed.



Gwen G. Pipkin
FDOT Hearing Moderator



Date

FDOT Meeting
November 15, 2018

Florida Department of Transportation

SR 29 Immokalee PD&E Study

from Oil Well Road to SR 82

TRANSCRIPT OF PROCEEDINGS

DATE TAKEN: November 15, 2018

TIME: 6:00 p.m. - 7:09 p.m.

PLACE: CareerSource Southwest Florida
750 South 5th Street
Immokalee, Florida 34142

Stenographically Reported by:
Terri L. Lewis
U.S. Legal Support, Inc.

ERRATA SHEET

1

2 IN RE:

3 DEPOSITION OF:

4 TAKEN:

5 PAGE #	LINE #	CHANGE	REASON
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Under penalty of perjury, I declare that I have read my deposition and that it is true and correct subject to any changes in form or substance entered here.

19

20

Signature William G. Howell

21

22

23

24

25



700 East Dania Beach Boulevard, First Floor
Dania Beach, Florida 33004
Toll Free: 888.311.4240
Email: SoutheastProduction@uslegalsupport.com

January 5, 2019

Cella Molnar & Associates, Inc.
Attn: Brianna Wagner
400 North Ashley Drive
Suite 1900
Tampa, FL 33602

Case: **FDOT Public Meeting**
Witness: **Attachment, OX SHIPPED 11/29/2018 (MH) and Public Hearing**
Date Taken: **11/15/2018**
Ref. No: **1825206**

With regard to the deposition transcript referenced above, please be advised that the following circumstance applies, as indicated:

 X The attached errata sheet was received by our office and should be included in the above-referenced deposition transcript at the appropriate page number.

 The witness did not appear for reading and signing of the deposition transcript, and no returned signature page and/or errata sheet has been otherwise received by our office as of the date of this correspondence.

Other: _____

CC:

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ATTACHMENT

DESCRIPTION	PAGE
Audio/Visual Transcript	9

* * * * *

1 P R O C E E D I N G S

2 MS. PIPKIN: Okay. It looks like most
3 everybody over here is ready to go.

4 Good evening. The Florida Department of
5 Transportation would like to welcome you to the
6 public hearing for the Project Development and
7 Environment, or PD&E study, for the State Road 29
8 project.

9 My name is Gwen Pipkin, and I am the project
10 manager for the State Road 29 project. This public
11 hearing is being held for Financial Project
12 No. 417540-1-22-01 and Federal Aid Project
13 No. 391-022-P.

14 This public hearing is being held at
15 CareerSource Southwest Florida, 750 South 5th
16 Street, Immokalee, Florida, on Thursday,
17 November 15th, 2018, at 6 p.m.

18 This PD&E study has been conducted by
19 District 1 in compliance with all applicable
20 federal environmental laws and pursuant to 23
21 U.S.C. 327 and the implementing memorandum of
22 understanding between the DOT and the Federal
23 Highway Administration signed on December 14th,
24 2016. The FDOT Office of Environmental Management
25 in Tallahassee is the approving authority.

1 This project is proposed as the widening of
2 State Road 29 from two to four lanes from Oil Well
3 Road to south of State Road 82, a distance of
4 approximately 16 miles. It includes a section of
5 new alignment along the west side of the Immokalee
6 Regional Airport that then turns northwest to
7 parallel Madison Avenue and New Market Road on 29.

8 This hearing is being held to provide you with
9 the opportunity to comment on this project.

10 Now, I would like to introduce you to the
11 people with name tags, anyone who has one of these
12 name tags, who can assist you with the review of
13 the maps on display.

14 The FDOT right-of-way acquisition and
15 relocation program has a table in the back, along
16 with the traffic noise reevaluation. They are
17 staff from the Florida Department of Transportation
18 and from the consultant team who assisted the
19 department with this study.

20 I would also like to introduce elected or
21 appointed public officials who took time out from
22 their busy schedules to attend our public hearing
23 tonight. And we have one, Ms. Christie Betancourt,
24 who is the operations manager for the Immokalee
25 CRA, sitting over here trying to hide in the

1 corner.

2 And is there anyone else that has shown up
3 that would like me to be -- tell everyone who you
4 are? No one else?

5 (No response.)

6 MS. PIPKIN: Okay.

7 The purpose of this public hearing is to share
8 information with the general public about the
9 proposed improvement and the conceptual design, all
10 alternatives under study, and the potential
11 beneficial and adverse social, economic, and
12 environmental impacts on the community. The public
13 hearing also serves as an official forum providing
14 an opportunity for members of the public to express
15 their opinions regarding the project. Public
16 participation at this hearing is encouraged and
17 solicited without regard to race, color, national
18 origin, age, sex, religion, disability, or family
19 status.

20 There are three primary components to
21 tonight's hearing: First of all, the open house
22 which you just participated in. It occurred prior
23 to the official presentation, which we are now in.
24 You are invited to review the project displays,
25 speak directly with any of the members who have the

1 tag on, and you could also have provided your
2 comments in the comment box or to the court
3 reporter.

4 In a few minutes we're going to show the video
5 presentation, which is the second portion of our
6 presentation. It will explain the purpose and
7 need, show you all the study alternatives, discuss
8 the potential impacts, both beneficial and adverse,
9 and propose methods to mitigate the potential
10 adverse impacts.

11 Following the video, there will be a short
12 intermission which will give you another
13 opportunity to look at the displays as well as
14 discuss anything that came up while you were
15 watching the video with the project team members.
16 You can share your questions and ask them any
17 questions at that time.

18 Third, a formal comment period will follow the
19 intermission where you will have the opportunity to
20 provide formal statements at the microphone, which
21 we have over here, or you may provide your comments
22 directly to the court reporter, which is over here,
23 or you can provide them in writing. We have those
24 stations across the back back there where there's
25 the comment boxes.

1 An important part that you need to remember is
2 that your comments should be for or against the
3 project, and the other important part of the
4 testimony is why you are for or against the
5 project. If you have any other comments, we also
6 welcome those as well.

7 So we are here tonight to present to you and
8 explain the engineering and environmental studies
9 that have been conducted to date. There's a bunch
10 of them. We're also here to give you an
11 opportunity to publicly and officially comment on
12 this project.

13 We bring a proposed project to a public
14 hearing to solicit your views and comments. We
15 want to hear from people with local knowledge, and
16 we want to make sure that we hear what you like and
17 don't like about the proposed improvements. It is
18 important that interested people like you have an
19 opportunity to become fully aware of this project.
20 It is also important that you express your views at
21 this stage of the project when the flexibility
22 still exists to incorporate these views into the
23 study documents. Final decisions will be made
24 using these documents.

25 Now, when you arrived this evening, you were

1 offered a handout containing information about the
2 project and a comment sheet, which is this. If you
3 did not receive those, you can pick one up at the
4 back table where you came in, the registration
5 table.

6 Now, another important thing is if you choose
7 not to speak for the public record this evening,
8 you can still make comments for the record. You
9 can complete the comment form tonight and drop it
10 in the comment boxes, which are the clear plastic
11 boxes on the back table back there.

12 This hearing will officially remain open until
13 November 26th, 2018. Anyone wishing to make
14 comments which will become part of the official
15 transcript of the hearing has until this date to
16 write the department. You may also send your
17 comments by email to the address listed on the
18 poster board, which is my name and address. Those
19 comments will be received -- must be received and
20 postmarked by November 26th as well and will be the
21 same as if you gave your comment at the microphone
22 here this evening.

23 After tonight's hearing, the department will
24 compile your comments, and together with the
25 engineering and environmental work that has been

1 done, make a final recommendation that we will
2 submit to DOT's Office of Environmental Management
3 for approval, and we will publish that approval of
4 the recommended alternative in the Naples Daily
5 News and the Immokalee Bulletin and post the
6 approval on the project website.

7 Now we want to begin the audio/visual
8 presentation.

9 (Audio/Visual Presentation played. See
10 attached transcript.)

11 MS. PIPKIN: Okay. We're going to have an
12 intermission in just a minute, but first I want to
13 describe how you can sign up to give your oral
14 testimony.

15 Lisa over there is holding up these comment
16 cards. If you wish to speak this evening, please
17 see her to obtain one of these cards, fill it out,
18 and return it to her. I will call on you to speak
19 at the microphone in the order in which she
20 receives the cards back.

21 Now, we're going to take a 15-minute
22 intermission so that that gives you time to fill
23 out the cards and ask any potential questions you
24 may have based on what you saw in the video.

25 The time now is 6:35, so we will reconvene at,

1 what is that, 6:50. You're welcome to go look and
2 talk.

3 (A brief recess was had.)

4 MS. PIPKIN: Ladies and gentlemen, we have
5 about two minutes before we start taking public
6 comments, if you'd like to wander back to your
7 seats.

8 Okay. My watch says it's now 6:50, so we will
9 start the public testimony portion of the hearing.
10 If you are still holding a speaker card and did not
11 give it back to Lisa, would you please return it to
12 her. If you have not received the speaker card and
13 you still think you would like to speak, you can
14 still get one from her, or I have them up here.

15 We will now call upon those who have turned in
16 a speaker card. When you come forward to the
17 microphone here, if you would, please state your
18 name and address. If you represent an
19 organization, municipality, or other public body,
20 please provide that information as well.

21 We ask that you limit your input to three
22 minutes. If you have additional comments, you can
23 provide them directly to the court reporter or you
24 can send them in writing. Please come to the court
25 reporter or the microphone and be sure you speak

1 clearly so we can get your comment well recorded.

2 The first card I have is Antonia Rios. If you
3 will please come to the microphone.

4 MS. RIOS: My name is Antonio.

5 THE INTERPRETER: (For Ms. Rios) Good
6 afternoon. My name is Antonia Rios. She is coming
7 with a group of other habitants of the region.
8 They're seeing a lot of accidents on the road,
9 existing conditions, and they are really worried
10 about it. She wants to introduce Maura, Martha,
11 Lusvi.

12 They started a proposal. They collected
13 signatures. They are not -- they disagree with the
14 roundabout option for the intersection, and they
15 say they prefer a signalized intersection
16 alternative if there is.

17 Since it's an evacuation route, they're
18 concerned about the speed for the evacuation route.
19 Given that it's to be a roundabout, they're
20 concerned it's going to be slower, so that's why
21 they have collected the signatures for FDOT to
22 analyze different alternatives for the
23 intersection.

24 THE INTERPRETER: (For Ms. Diaz) They are
25 concerned about the school buses that have to go

1 through State Road 29 and Carson Street. They say
2 that the school buses take too long to enter the
3 SR 29 to, you know, take the kids to school. They
4 say that the access is too slow.

5 There are no pedestrian crosswalks near the
6 pediatric clinic, okay, and the geriatric clinic,
7 too.

8 MS. CANTER: Hi. I was just concerned about
9 pedestrians. As it is now, I know nobody here has
10 pushed a stroller or tried to push a stroller
11 across 29 from Westclox over to New Market. I was
12 just concerned about that, especially with the
13 roundabout. I understand you're trying to direct
14 traffic to the newer extension or the new road, but
15 we still have all the semi-trucks and field trucks
16 that need to turn on this turnabout. It's a
17 beautiful concept, but the turnabout or the turn
18 now, it's pretty wide, and we still have vehicles
19 turning over when they're fully loaded.

20 What is the turnabout speed going to be? And
21 how far is the pedestrian walk going to be from the
22 actual road, is my question?

23 MS. PIPKIN: Okay. That's something we'll
24 have to look at. And I would like to get a written
25 comment from you, if we can. If you could help

1 with us that, Jorge, to get all this written down
2 on a comment, because we would like to go back and
3 analyze that and provide you with a more detailed
4 response.

5 MS. CANTER: Because I believe that's what the
6 ladies are concerned about now and what could be
7 implemented now, because it's a beautiful concept.
8 And, you know, I know it's going to take a few
9 years, but what can we put there now for
10 pedestrians to safely cross?

11 MS. PIPKIN: Okay.

12 MS. CANTER: That is not being used for that
13 reason. And I know everybody here is aware of
14 Marion E. Fether and how big the community is
15 growing on Eden Park now with the Habitat homes.
16 There's more people trying to make their way across
17 and can't seem to find that.

18 MS. PIPKIN: Okay. That's good. I was just
19 talking about that with my boss, actually, before
20 we came back.

21 MS. CANTER: If vehicles can't get across now,
22 imagine being on foot with a child in hand, your
23 bags, groceries, maybe, even from Winn Dixie and
24 pushing a stroller. I know we have all seen it.

25 And if you-all are having a hard time -- I'm a

1 jogger, and just jogging across is a hassle. I can
2 imagine these ladies pushing strollers across and
3 how dangerous that is now, not two years from now
4 with the turnabouts there.

5 MS. PIPKIN: Well, let us get the full
6 question and all your questions put together, and
7 we will prepare a response for that.

8 We haven't made a complete decision on that
9 intersection yet anyway. There's a process that
10 has to be completed, and we have not done that yet.
11 That will be part of the design phase to analyze
12 that. We'll take that into consideration.

13 MS. CANTER: Thank you.

14 MS. PIPKIN: Gracias.

15 MS. CANTER: I think there's one more comment.

16 MS. PEREZ: (Through the interpreter) Is there
17 any immediate action that you can take to fix the
18 current issue, the existing issue with the traffic
19 conditions, safety?

20 MS. PIPKIN: We'll have to take a look at it
21 and see. Nothing the DOT does is immediate, but we
22 will have to look at that. I think we were talking
23 about looking at that next week and seeing if we
24 could come up with some ideas for interim
25 solutions, but I don't know how fast that will be.

1 We will do the best we can.

2 THE INTERPRETER: (For Ms. Perez) Yeah, she
3 understands that the roundabout proposal is good,
4 but the fact that it's going to take five years to,
5 you know, become a reality, that's what they're
6 concerned about. So they would like FDOT to take
7 some more immediate action to at least alleviate
8 the existing problems in the corridor.

9 MS. PIPKIN: Yes. He's nodding at me back
10 there. So, yes, we're going to look at that.
11 That's way above my head to make that decision, but
12 we're going to see what we can do about that.
13 We'll see if we can get that faster.

14 THE INTERPRETER: (For Ms. Perez) She just
15 would like the FDOT to be a witness of what it
16 takes for them to commute from one place to
17 another, and it's a difficult path right now.

18 MS. PIPKIN: Yes, we have heard that, so we
19 will definitely take a look.

20 Okay. The second card I have is from Danny
21 Gonzalez.

22 MR. GONZALEZ: Good evening. My name is Danny
23 Gonzalez, president of the Chamber of Commerce here
24 in Immokalee.

25 My questions are for this loop they're trying

1 to build. I know that the semis travel a lot to
2 New Market Road but, again, you have to also think
3 about this loop, how the semis are going to be able
4 to get gas on New Market Road. Is there another
5 side road? They're going to get back on the loop.
6 That's one of my concerns.

7 And another concern is the business that you
8 are going to buy for the new road, is that business
9 going to be relocated, or are they going to be --
10 not exist no more?

11 MS. PIPKIN: Normally when we have to hit a
12 business like that, it's more considered a
13 relocation; not to say that we're going to say you
14 have to move here or you have to move there.
15 Normally we will give them enough money to locate.
16 Where they choose to relocate is up to them. We
17 don't dictate that. I think that that gas station
18 could probably go in another place that would be a
19 lot easier to access. I know --

20 MR. GONZALEZ: I mean, a good alternative for
21 that would be on the new road itself.

22 MS. PIPKIN: Possibly.

23 MR. GONZALEZ: I mean, because once the semis
24 get on the loop, there's no access to diesel to get
25 down to Fort Myers.

1 MS. PIPKIN: Right.

2 MR. GONZALEZ: So then, you know, this
3 business that created, you know, selling diesel,
4 he's not going to be any longer in business because
5 it seems like semi's are not going to go through
6 there. So I'm just hoping there's another road on
7 the site of that loop for the gentleman that's
8 selling diesel, because he's going to lose his
9 business. So that's one of my concerns.

10 And another concern is about the airport park.
11 So the airport park is going to be gone, or are you
12 doing it on the side? Because airport park means a
13 lot of people, because they jog there on that area,
14 too.

15 MS. PIPKIN: Yes. The airport park proper
16 we're only taking a sliver from. The piece
17 adjacent to the east of airport park actually
18 belongs to the airport, and they have an agreement
19 with the park that says they can take that back
20 anytime they want to. That's not what the park
21 really wanted to hear. We're still kind of working
22 through some issues with that, but it -- but we're
23 not taking all of the park. We're taking just a
24 sliver of the park.

25 MR. GONZALEZ: So, I mean, if I go to the

1 park -- I would not want to go to the park with all
2 that noise going through there now, going through
3 the semis. So can that park get relocated, or can
4 Collier County get some more land for that park?
5 Because it's going to be kind of awkward. You're
6 going to have an airport with kids, and next to it
7 you're going to have the semis going through that.
8 I mean, that's going to be -- you know, that's
9 going to be kind of scary on the noise.

10 MS. PIPKIN: I think the county wants to
11 relocate it. That's -- if we -- we wouldn't pay to
12 relocate it. They would get some money from us for
13 the piece that we are taking. But that's all that
14 we're going to be allowed to do.

15 MR. GONZALEZ: I mean -- because I have to
16 look out for the community itself, too, because we
17 only have two parks here in Immokalee, and we have
18 over 70,000 people during the season.

19 MS. PIPKIN: I know.

20 MR. GONZALEZ: It just, you know, makes me
21 wonder where they're going to go for a park.

22 Another concern is the time on this. I mean,
23 have you to schedule, like, in segments? Like 82,
24 are they doing it in sections, or this is all one
25 at once?

1 MS. PIPKIN: No.

2 MR. GONZALEZ: How's that going to work?

3 MS. PIPKIN: It's a bunch of different
4 sections. Right now what I have heard in the years
5 I've been down here is the most important piece to
6 the community is the piece from Westclox to 82. We
7 have gut that piece. The video did not reflect it
8 because it was just last week that we found out
9 that we would most likely have all of that piece
10 funded through construction out -- I believe it's
11 in 2026. That's where they're planning to put the
12 construction. It's not in there yet, but we're --

13 MR. GONZALEZ: I hope not, because I don't
14 want to paint my hair by then.

15 Anyway, again, also I'm going to want to speak
16 where they were speaking about on Westclox and 29,
17 I live down the road on Jefferson. Like, in the
18 past month we heard three or four accidents, people
19 crossing over getting hit. That's where I come
20 also, because we've been kind of petitioning that
21 for the past three years. Traffic there is just
22 getting more and more, like anywhere else; traffic
23 is picking up everywhere else.

24 MS. PIPKIN: Right.

25 MR. GONZALEZ: I mean, I'm not complaining.

1 You know, there's business here in town, and the
2 traffic is good, but that section is really, really
3 getting really bad. I mean, you can hear it from
4 the fire department, police department. I mean,
5 maybe there's a solution. Maybe you can get a
6 Collier County policeman during the -- from six in
7 the morning to nine in the morning to patrol that
8 area, be a guard or be something, because -- and
9 then from three to six. It's just bad there.

10 And I see these ladies who are there speaking,
11 I see them cross over going to the clinic. But
12 there's no other way. You have to cross over.

13 MS. PIPKIN: Right. I've seen it, too.

14 MR. GONZALEZ: And I've seen too many
15 accidents happen there. So, please, keep it in
16 mind that that area there needs to not wait five
17 years. And this could be done within a year, have
18 some solutions here, because three or four people
19 got passed away in that area already accident-wise.

20 MS. PIPKIN: That's a high priority for me,
21 personally, because that's something that I don't
22 like to see. We're going to see what we can do.
23 We won't be able to do, like, the full project or
24 anything, but there may be some interim solutions
25 that can happen a lot quicker than whatever that

1 final solution is going to be.

2 MR. GONZALEZ: Well, thank you. And I want to
3 thank FDOT for, you know, reconsidering all this
4 new work here in Immokalee. It's about time. And
5 infrastructure -- you know, we can use
6 infrastructure here in Immokalee. I'm all for it.
7 And the more you get information out, I'm here to
8 help. So thank you again for coming over.

9 MS. PIPKIN: Well, thank you. We're glad
10 we're finally getting to the point where we can get
11 something done. Thank you.

12 And I do have one last card. Lusvi Perez, did
13 you want to say something else? I think she was
14 here with the other women.

15 UNIDENTIFIED SPEAKER: I think she already
16 spoke.

17 MS. PIPKIN: She was with the other group.
18 That's what I thought. Okay. So she doesn't want
19 to say anything else. Okay.

20 I have no more cards, however, if there's
21 anyone else who wants to say anything, this would
22 be the time. You can come up now, if you like.
23 This is the moment to do so.

24 (No response.)

25 MS. PIPKIN: Okay. Seeing no one, the

1 verbatim transcript of this hearing's oral
2 proceedings, together with all the written material
3 received as part of the hearing record and all the
4 studies that you saw on the table back there,
5 displays, and informational material provided at
6 the hearing, will be made part of the project
7 decision-making process and will be available at
8 the district office and at the SWAO office for
9 public review upon request.

10 If anyone wishes to submit written statements
11 or other exhibits in place of or in addition to
12 oral statements, they may do so. Once again,
13 written statements and exhibits will be accepted
14 and recorded as part of this hearing if postmarked
15 by December 26th (sic).

16 Please mail your comments to me, and it should
17 be on the back of the comment page, my address,
18 Gwen G. Pipkin, project manager, Florida Department
19 of Transportation, P.O. Box 1249, Bartow, Florida,
20 33831. This is the same address that appears on
21 the back of the comments sheet.

22 Thank you for attending this public hearing
23 and providing your input into this important
24 project. It is now 7:09. I hereby officially
25 close the public hearing for the State Road 29 PD&E

1 study.

2 Thank you, and have a great evening.

3 (Proceedings concluded at 7:09 p.m.)

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1 STATE OF FLORIDA

2 COUNTY OF COLLIER

3 I, Terri L. Lewis, Court Reporter and Notary
4 Public, do hereby certify that the foregoing proceedings
5 were taken before me at the date and place as stated in
6 the caption hereto on Page 1 hereof; that the foregoing
7 computer-assisted transcription, consisting of pages
8 numbered 2 through 23, inclusive, is a true record of my
9 Stenograph notes taken at said proceedings.

10 Dated this 26th day of November 2018.

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TERRI L. LEWIS, Court Reporter
Notary Public, State of Florida
Commission No: GG 097505
Commission Expires: August 23, 2021

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SR 29 Immokalee PD&E Study from Oil Well Road to SR 82
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1 The Florida Department of Transportation, known as the F-D-O-T, welcomes you to the public
2 hearing for the proposed improvement of State Road 29 from Oil Well Road to State Road 82,
3 extending a distance of approximately 16 miles.

4
5 F-D-O-T, in cooperation with Collier County and the Collier Metropolitan Planning
6 Organization or M-P-O, is conducting a Project Development and Environment or P, D, and E
7 Study to evaluate improvements to State Road 29 in unincorporated Collier County. The
8 purpose of the P, D, and E Study is to evaluate the need for improvements and provide
9 documented environmental and engineering analyses to assist the F-D-O-T in reaching a decision
10 on the location and conceptual design for improvements to State Road 29. The study is
11 developed in compliance with requirements of the National Environmental Policy Act of 1969,
12 which will qualify future phases of this State Road 29 project for federal funding.

13
14 The environmental review, consultation, and other actions required by applicable federal
15 environmental laws for this project are being, or have been, carried out by the F-D-O-T pursuant
16 to 23 United States Code Section 327 and a Memorandum of Understanding dated December
17 14th, 2016 executed by the Federal Highway Administration and F-D-O-T.

18
19 The F-D-O-T began this P, D, and E Study in June 2007 and the study is scheduled to be
20 completed in the spring of 2019. The purpose of this project is to improve traffic operational
21 conditions along the State Road 29 corridor between Oil Well Road and State Road 82 to meet
22 the following needs:

- 23 • Accommodate future growth,
- 24 • Reduce truck traffic in downtown Immokalee,
- 25 • Correct current design deficiencies,
- 26 • Improve mobility and connectivity within the regional transportation network,
- 27 • Enhance the economic competitiveness, and
- 28 • Improve emergency evacuation capabilities.

29

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30 State Road 29 within the project limits is designated as a Strategic Intermodal System or S-I-S
31 Emerging Highway Corridor. S-I-S is our state's network of high priority transportation
32 facilities.

33

34 Presently, State Road 29 is a two-lane undivided roadway with one 12-foot travel lane in each
35 direction from Oil Well Road to 13th Street and from Westclox Street/New Market Road West to
36 SR 82. From 13th Street to North 9th Street, State Road 29 is a four-lane divided roadway with
37 two 12-foot travel lanes in each direction. From North 9th Street to Westclox Street/New Market
38 Road West, State Road 29 is a two-lane divided roadway with one 12-foot travel lane in each
39 direction and a 14-foot two-way left turn lane. The speed limit within the project limits ranges
40 from 35 to 60 miles per hour.

41

42 The existing right-of-way varies from approximately 174 feet to 183 feet from Oil Well Road to
43 Seminole Crossing Trail and from 100 to 200 feet from Seminole Crossing Trail to State Road
44 82.

45

46

PAUSE

47

48 Throughout this P, D, and E Study process, F-D-O-T has looked at many different concepts to
49 widen the existing two lanes of State Road 29 to four lanes through downtown Immokalee, as
50 well as potential new alignments east and west of Immokalee. At each phase of the process, F-
51 D-O-T conducted a public meeting to provide an update on the project's status and to take
52 comments from the public and local agencies. After environmental and engineering analyses and
53 public and agency comments, many of these alternatives were eliminated. The last public
54 meeting was an Alternatives Public Workshop held on November 9th, 2017. At that meeting, we
55 asked for your input on three proposed build alternatives within the project limits – Central
56 Alternative 1 Revised, Central Alternative 2, and Central Alternative 2 Revised. Based on your
57 comments and additional environmental and engineering analyses, we selected Central
58 Alternative 2 as the Recommended Build Alternative for State Road 29.

59

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60 Currently, the majority of the State Road 29 project corridor operates at or above the F-D-O-T
61 Levels of Service C and D adopted for the roadway; only a small segment of the project corridor
62 (from New Market Road to State Road 82) operates below the adopted standard. If no
63 improvements occur to the roadway, the State Road 29 project corridor is anticipated to operate
64 under deficient conditions with most segments operating at Level of Service F by the 2045
65 design year, thus, resulting in an unacceptable Level of Service for the existing roadway.

66

67 This evening the F-D-O-T is presenting two alternatives for State Road 29 within the project
68 limits - the Recommended Build Alternative and the No-Build Alternative. Please review the
69 boards on display this evening showing the alternatives and their impacts in detail.

70

71 The Recommended Alternative or Central Alternative 2, follows existing State Road 29 from
72 the start of the project at Oil Well Road to north of Seminole Crossing Trail. From this point, the
73 Recommended Alternative travels north from State Road 29 on new alignment along the west
74 side of the Immokalee Regional Airport to avoid impacts to the commercial and industrial areas
75 of Immokalee and the State Farmers Market to the west. It then turns to the northwest just past
76 Gopher Ridge Road to parallel Madison Avenue and New Market Road. It then travels along the
77 east side of the Collier Health Services Medical Center and the Florida State University College
78 of Medicine before reconnecting to State Road 29 north of Westclox Street/New Market Road
79 West. The Recommended Alternative then follows existing State Road 29 from north of
80 Westclox Street/New Market Road West to SR 82.

81

82

PAUSE

83

84 From Oil Well Road to south of the Kaicasa Entrance, the proposed typical section consists of a
85 four-lane divided roadway with two 12-foot travel lanes in each direction separated by a 40-foot
86 grassed median and roadside ditches to handle stormwater runoff. This typical section can be
87 constructed within the existing right-of-way, except for stormwater ponds and floodplain
88 compensation sites.

89

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90 From south of the Kaicasa Entrance to north of Seminole Crossing Trail, the proposed typical
91 section consists of a four-lane divided roadway with two 12-foot travel lanes in each direction
92 separated by a 30-foot raised median with a 10-foot shared use path on the west side of the
93 roadway from Farm Worker Way to Seminole Crossing Trail. There are roadside ditches to
94 handle stormwater runoff. This typical section can be constructed within the existing right-of-
95 way with the exception of the canal relocation near Seminole Crossing Trail, and stormwater
96 ponds and floodplain compensation sites.

97

98 The proposed typical section from north of Seminole Crossing Trail to Gopher Ridge Road
99 consists of a four-lane divided roadway with two 11-foot travel lanes in each direction separated
100 by a 22-foot raised median with 7-foot buffered bicycle lanes and 6-foot sidewalks in each
101 direction. There is a closed drainage system with curb and gutter. This typical section can be
102 constructed within the existing right-of-way with the exception of some additional right-of-way
103 needed for a turn lane near 13th Street, plus right-of-way for stormwater ponds.

104

105 From Gopher Ridge Road to State Road 29, the proposed new alignment typical section consists
106 of a four-lane divided roadway with two 12-foot travel lanes in each direction separated by a 30-
107 foot raised median. There are roadside ditches to handle stormwater runoff. This typical section
108 will require 200 feet of right-of-way for the roadway, plus right-of-way for stormwater ponds.

109

110 The proposed typical sections for State Road 29 from north of Westclox Street to south of State
111 Road 82 consist of a four-lane divided roadway with two 12-foot travel lanes in each direction,
112 separated by a 30-foot raised median from north of Westclox Street to Experimental Road, and a
113 40-foot grassed median from Experimental Road to south of State Road 82. A 10-foot shared
114 use path is proposed on the west side of the roadway. There are roadside ditches to handle
115 stormwater runoff. The roadway typical section can be constructed within the existing 200 feet
116 of right-of-way, but additional right-of-way will be required for stormwater ponds and floodplain
117 compensation sites.

118

119 In accordance with Rule 14-97 of the Florida Administrative Code, the proposed improvements
120 will include a median to control the movements or access of turning traffic. SR 29 falls under

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121 access management classification three. These changes are presented in compliance with
122 Section 335.199, Florida Statutes, transportation projects modifying access to adjacent property.

123

124 To comply with the National Environmental Policy Act of 1969 or NEPA (pronounced Nee-pa),
125 F-D-O-T will consider the No Build Alternative as a valid alternative throughout this study
126 process. The No Build Alternative assumes that no major improvements are made to State Road
127 29 within the project limits through the design year of 2045 except for routine maintenance.

128

129 Advantages of the No Build Alternative include:

- 130 • No construction costs,
- 131 • No disruption to traffic due to construction,
- 132 • No disruption to adjacent property owners due to construction,
- 133 • No right-of-way acquisitions or relocations, and
- 134 • No degradation or disruption of natural and other environmental resources due to
135 construction.

136

137 Disadvantages of the No Build Alternative include:

- 138 • Increased traffic congestion causing increased road user costs due to travel delay,
- 139 • Not consistent with the local transportation plans,
- 140 • Increased potential for vehicular crashes due to congested lanes and intersections,
- 141 • Increased emergency vehicle response times,
- 142 • Increased potential for crashes between vehicles and pedestrians/bicyclists due to
143 inadequate sidewalks and bicycle lanes, and
- 144 • Increased vehicle emission pollutants due to higher levels of traffic congestion.

145

146

PAUSE

147

148 This P, D, and E Study evaluated potential environmental impacts associated with the
149 Recommended Alternative. The evaluations are conducted in accordance with the National
150 Environmental Policy Act of 1969, as amended, and other federal requirements.

151

SR 29 Immokalee PD&E Study from Oil Well Road to SR 82
Public Hearing Video Script

152 The evaluation considered the effects of the proposed improvements to State Road 29 on land
153 use, threatened and endangered species, wetlands, floodplains, water quality, noise, air quality,
154 contamination, cultural resources, Section 4(f) resources, farmlands, as well as right-of-way
155 requirements and relocations.

156

157 Land use in the vicinity of the State Road 29 project corridor includes agricultural, residential,
158 industrial, and commercial activities. Commercial and industrial activities also exist near the
159 Immokalee Regional Airport. Existing and future land uses in the project area will continue to
160 be supported as well as enhanced by the proposed project due to improved access for nearby
161 businesses, residents, and agricultural operations.

162

163 Threatened and endangered plant and animal species are afforded special protection under the
164 Endangered Species Act of 1973, as amended, and Florida Statutes. Through evaluation of
165 various datasets and coordination with the US Fish and Wildlife Service and Florida Fish and
166 Wildlife Conservation Commission, a total of 30 federal or state listed protected species were
167 identified as having the potential to occur within the project area. Of these species, the agencies
168 determined that the proposed project may affect, but is not likely to adversely affect, six federal
169 species including: American alligator, Eastern indigo snake, Florida bonneted bat, wood stork,
170 Audubon's crested caracara, and the snail kite. Two federally listed species, the Florida scrub
171 jay and the Florida panther, may be affected and are likely to be adversely affected. One state-
172 listed species, the gopher tortoise may be affected, but is not likely to be adversely affected.

173

174 To comply with Section 7 of the Endangered Species Act of 1973, as amended, the F-D-O-T has
175 committed to re-initiating consultation with both US Fish and Wildlife Service during the
176 project's design and permitting phase for the Florida scrub jay, Florida panther, and all other
177 species for which a may affect, but is not likely to adversely affect determination has been made.

178 As part of the coordination with both US Fish and Wildlife Service and Florida Fish and Wildlife
179 Conservation Commission, the F-D-O-T will implement various measures to minimize and
180 mitigate impacts to any federal or state protected species. Furthermore, the F-D-O-T will
181 continue to consult with environmental agencies in future project phases to meet all
182 environmental permitting and construction requirements.

SR 29 Immokalee PD&E Study from Oil Well Road to SR 82
Public Hearing Video Script

183

184 The project team has evaluated this project for wetlands involvement. The proposed
185 improvements will result in approximately 30 acres of wetland and other surface water impacts.
186 The Recommended Alternative includes all practicable measures to minimize harm to wetlands.
187 Wetland impacts resulting from construction of this project will be mitigated to meet Florida
188 Statutes and the US Code.

189

190 The project has been evaluated for potential floodplain involvement. Even though the project is
191 located within the 100-year floodplain, there will be no significant change in flood risk and
192 minimal impacts are expected to the 100-year floodplain.

193

194 A water quality impact evaluation was performed. The proposed stormwater facilities will be
195 designed to meet the current requirements of the South Florida Water Management District.

196

197 The project team evaluated effects of traffic noise associated with the Recommended
198 Alternative. None of the 100 evaluated noise-sensitive receptor sites along the project corridor
199 are predicted to experience future traffic noise levels that approach, meet, or exceed the Noise
200 Abatement Criteria for their respective Activity Category. None of the evaluated sites will
201 experience a substantial increase in traffic noise above the existing conditions as a result of the
202 proposed project. Therefore, noise abatement measures were not considered for the noise
203 sensitive sites identified adjacent to the Recommended Alternative. Traffic noise specialists are
204 here tonight to answer any questions.

205

206 The project is located in an area which is designated attainment for all of the National Ambient
207 Air Quality Standards under the criteria provided in the Clean Air Act. Therefore, the Clean Air
208 Act conformity requirements do not apply to this project and no significant impact is expected on
209 air quality. This project is expected to improve traffic flow by adding capacity to relieve
210 congestion, which should reduce operational greenhouse gas emissions.

211

212 Minimal short-term air quality effects may be caused from construction of the proposed
213 improvements, such as dust from earthwork or unpaved roads and smoke from open burning.

SR 29 Immokalee PD&E Study from Oil Well Road to SR 82
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214 The F-D-O-T will adhere to all state and local regulations and to the Standard Specifications for
215 Road and Bridge Construction.

216

217 A contamination screening analysis was performed for the project which identified 28 sites with
218 the potential for medium contamination involvement and three sites with the potential for high
219 contamination involvement. The F-D-O-T will further evaluate the potential contamination risks
220 of these sites during the design phase of the project. Before construction, specially trained crews
221 will address contamination in these areas, as required. Locations of these sites are shown on the
222 concept plans displayed this evening.

223

224 A cultural resource assessment survey was conducted in accordance with the National Historic
225 Preservation Act of 1966 and Florida Statutes. Archaeologists and historians identified no
226 archaeological sites and 46 historic resources in the project area. One resource is individually
227 eligible for listing in the National Register of Historic Places - the Immokalee Ice Plant. The
228 State Historic Preservation Officer determined that there would be no adverse effect to the Ice
229 Plant and no significant historic properties identified would be adversely affected by the
230 proposed improvements.

231

232 The project team examined the project area for publicly owned properties that may be affected
233 under Section 4(f) of the Department of Transportation Act of 1966. The Immokalee Airport
234 Park and the Immokalee Airport Conservation Easement are Section 4(f) resources. The
235 Recommended Alternative will require permanent use of approximately 0.27 acre of the
236 Immokalee Airport Park and 4.45 acres of the Immokalee Airport Conservation Easement.
237 These impacts will occur at the edge of each property.

238

239 Based upon the overall minor level of impact and implementation of proposed mitigative actions
240 to address potential impacts, the project will not adversely affect the activities, features, and
241 attributes of each property in meeting its intended Section 4(f) purpose. As such, F-D-O-T is
242 pursuing a Section 4(f) *de minimis* finding for the potential impacts to each resource. This
243 information is available for review at the public hearing tonight. The hearing provides the

SR 29 Immokalee PD&E Study from Oil Well Road to SR 82
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244 opportunity for public comment on the project's use of the Immokalee Airport Park and the
245 Immokalee Airport Conservation Easement.

246

247 As determined by the Natural Resources Conservation Service under the US Department of
248 Agriculture or U-S-D-A, the Recommended Alternative impacts approximately 160.5 acres of
249 farmlands of prime or unique importance. Due to the project effects being below the significance
250 threshold, no further consideration of protection is needed, no additional corridors need to be
251 evaluated, and no additional coordination with the Natural Resources Conservation Service is
252 required.

253

254 Construction of the Recommended Alternative will require additional right-of-way. One of the
255 unavoidable consequences on a project such as this is the necessary relocation of families or
256 businesses. On this project, we anticipate the relocation of one business; however, no residential
257 relocations are expected. All right-of-way acquisition will be conducted in accordance with
258 Florida Statute 339.09 and the federal Uniform Relocation Assistance and Real Property
259 Acquisition Policies Act of 1970, commonly known as the Uniform Act. Brochures that describe
260 the F-D-O-T's Relocation Assistance Program and Right of Way Acquisition Program are
261 available. F-D-O-T right-of-way representatives are available this evening to assist you and
262 discuss the program.

263

264 An evaluation matrix showing a detailed comparison of the Recommended Alternative and the
265 No Build Alternative is provided in the project handout and is on display here this evening. The
266 matrix shows potential effects to the social, cultural, natural, and physical environments, and
267 identifies preliminary costs.

268

269 The estimated cost for the Recommended Alternative includes \$16.4 million dollars for design,
270 \$1.8 million dollars for wetland mitigation, \$4.4 million dollars for wildlife habitat mitigation,
271 \$227 thousand dollars for relocation of intelligent transportation system facilities, \$18.3 million
272 dollars for right-of-way acquisition, \$109.2 million dollars for construction, and \$16.4 million
273 dollars for construction engineering and inspection. The total estimated cost for the
274 Recommended Alternative is \$166.7 million in 2018 dollars.

SR 29 Immokalee PD&E Study from Oil Well Road to SR 82
Public Hearing Video Script

275

276 The proposed improvements to State Road 29 are consistent with the Collier M-P-O's 2040 Long
277 Range Transportation Plan and Transportation Improvement Program or T-I-P. F-D-O-T's
278 Adopted Five-Year Work Program currently includes funding for the design of segments of State
279 Road 29 from Sunniland Nursery Road to State Road 82, but does not include funding for right-
280 of-way acquisition or construction. In addition, funding for future phases of the segment of State
281 Road 29 from Oil Well Road to Sunniland Nursery Road is not included in the T-I-P or Five-
282 Year Work Program; efforts are currently underway to ensure that this segment is funded.

283

284 Your comments are a very important part of the decision-making process. This public hearing is
285 your opportunity to provide comments on the Recommended Alternative and the No Build
286 Alternative. In a few minutes, you will have the opportunity to provide oral comments that will
287 be transcribed by a court reporter. Also, you can submit written comments in the comment boxes
288 provided this evening or send them to F-D-O-T later via mail or email. Comments must be
289 postmarked by November 26th, 2018 so that we can complete our study documentation.

290

291 You may send comments to Gwen G. Pipkin, Project Manager, at the address shown here or by
292 email. You may also visit the project website at www.sr29collier.com and provide comments by
293 November 26th, 2018. Comments will be evaluated and, where feasible, may be incorporated
294 into the conceptual design. A final determination of the Recommended Alternative will be
295 submitted to the F-D-O-T Office of Environmental Management for approval, which is expected
296 in the spring of next year.

297 F-D-O-T representatives are here this evening to answer your questions, listen to your
298 comments, and seek your input. They are wearing name badges for identification. We invite
299 you to speak with them and to review the aerial photos, poster board displays, and project
300 information handout.

301

302

PAUSE

303

304 This public hearing is held in accordance with the Federal Highway Act of 1968, as amended;
305 Chapter 23 United States Code 128; Title 40, Code of Federal Regulations, Parts 1500 to 1508;

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Public Hearing Video Script

306 Title 23, Code of Federal Regulations, Part 771; Section 120.525, Florida Statutes; Section
307 335.199, Florida Statutes; Section 339.155, Florida Statutes; Rule 14-97 of the Florida
308 Administrative Code; and the National Environmental Policy Act of 1969, as amended. It also
309 fulfills the requirements of Executive Orders 11 9 88 "Floodplain Management" and 11 9 90
310 pertaining to "Protection of Wetlands", whereas the opportunity for early public review and
311 comment is offered for projects proposed to be located in floodplains and wetlands. Floodplain
312 involvement and wetland impacts are associated with this project.

313

314 The hearing was advertised and is held consistent with federal and state requirements. The
315 hearing is conducted in accordance with the Americans with Disabilities Act of 1990 and with
316 Title VI of the Civil Rights Act of 1964 and related statutes. Anyone who feels that they have
317 been discriminated against with regard to race, color, religion, sex, age, national origin,
318 disability, or family status may complete one of the forms located at the sign-in table and mail
319 the completed form to the address listed on the poster board.

320

321 Thank you for your interest and participation in the State Road 29 Project Development and
322 Environment Study public hearing and for taking the time to join us this evening.

SR 29 IMMOKALEE MODERATOR SCRIPT

GOOD EVENING, THE FLORIDA DEPARTMENT OF TRANSPORTATION WOULD LIKE TO WELCOME YOU TO THE PUBLIC HEARING FOR THE PROJECT DEVELOPMENT AND ENVIRONMENT, OR PD&E, STUDY FOR STATE ROAD 29. MY NAME IS GWEN PIPKIN, AND I AM THE PROJECT MANAGER FOR THE FLORIDA DEPARTMENT OF TRANSPORTATION, DISTRICT ONE. THIS PUBLIC HEARING IS FOR FINANCIAL MANAGEMENT PROJECT NUMBER 417540-1-22-01 AND FEDERAL AID PROJECT NUMBER 3911-022-P.

THIS PUBLIC HEARING IS BEING HELD AT CAREERSOURCE SOUTHWEST FLORIDA, 750 SOUTH 5TH STREET, IMMOKALEE, FLORIDA ON THURSDAY, NOVEMBER 15, 2018, AT 6 P.M. THIS PD&E STUDY HAS BEEN CONDUCTED BY FDOT DISTRICT ONE IN COMPLIANCE WITH ALL APPLICABLE FEDERAL ENVIRONMENTAL LAWS AND PURSUANT TO 23 U.S.C. § 327 AND THE IMPLEMENTING MEMORANDUM OF UNDERSTANDING BETWEEN THE FDOT AND THE FEDERAL HIGHWAY ADMINISTRATION SIGNED ON DECEMBER 14, 2016. THE FDOT OFFICE OF ENVIRONMENTAL MANAGEMENT IN TALLAHASSEE IS THE APPROVING AUTHORITY.

THIS PROJECT IS DESCRIBED AS THE PROPOSED WIDENING OF STATE ROAD 29 FROM TWO TO FOUR LANES FROM OIL WELL ROAD TO SOUTH OF STATE ROAD 82, A DISTANCE OF ABOUT 16 MILES, AND INCLUDES A SECTION OF NEW ALIGNMENT ALONG THE WEST SIDE OF THE IMMOKALEE REGIONAL AIRPORT THAT THEN TURNS NORTHWEST TO PARALLEL MADISON AVENUE AND NEW MARKET ROAD TO SR 29. THIS HEARING IS BEING HELD TO

PROVIDE YOU WITH THE OPPORTUNITY TO COMMENT ON THIS PROJECT.

NOW I WOULD LIKE TO NOTE THE PEOPLE WITH NAME TAGS WHO CAN ASSIST YOU WITH YOUR REVIEW OF THE MAPS ON DISPLAY, THE FDOT RIGHT OF WAY ACQUISITION AND RELOCATION PROGRAM, ACCESS MANAGEMENT, AND THE TRAFFIC NOISE EVALUATION PROCESS. THEY ARE STAFF FROM THE FLORIDA DEPARTMENT OF TRANSPORTATION AND FROM THE CONSULTANT TEAM WHO ASSISTED THE DEPARTMENT WITH THIS STUDY.

I WOULD ALSO LIKE TO INTRODUCE ELECTED PUBLIC OFFICIALS WHO TOOK TIME OUT OF THEIR BUSY SCHEDULES TO ATTEND OUR PUBLIC HEARING.

(PAUSE)

FURTHER, I WOULD LIKE TO ACKNOWLEDGE ANY OTHER PUBLIC OFFICIALS WITH US THIS EVENING. **(PAUSE)**

THE PURPOSE OF THIS PUBLIC HEARING IS TO SHARE INFORMATION WITH THE GENERAL PUBLIC ABOUT THE PROPOSED IMPROVEMENT; ITS CONCEPTUAL DESIGN; ALL ALTERNATIVES UNDER STUDY; AND THE POTENTIAL BENEFICIAL AND ADVERSE SOCIAL, ECONOMIC, AND

ENVIRONMENTAL IMPACTS ON THE COMMUNITY. THE PUBLIC HEARING ALSO SERVES AS AN OFFICIAL FORUM PROVIDING AN OPPORTUNITY FOR MEMBERS OF THE PUBLIC TO EXPRESS THEIR OPINIONS REGARDING THE PROJECT. PUBLIC PARTICIPATION AT THIS HEARING IS ENCOURAGED AND SOLICITED WITHOUT REGARD TO RACE, COLOR, NATIONAL ORIGIN, AGE, SEX, RELIGION, DISABILITY, OR FAMILY STATUS.

THERE ARE THREE PRIMARY COMPONENTS TO TONIGHT'S HEARING:

- FIRST, THE OPEN HOUSE, WHICH OCCURRED PRIOR TO THIS PRESENTATION WHERE YOU WERE INVITED TO VIEW THE PROJECT DISPLAYS AND TO SPEAK DIRECTLY WITH THE PROJECT TEAM AND PROVIDE YOUR COMMENTS IN WRITING OR TO THE COURT REPORTER.
- SECOND, WE WILL SHOW A VIDEO PRESENTATION, WHICH WILL EXPLAIN THE PROJECT PURPOSE AND NEED; STUDY ALTERNATIVES; POTENTIAL IMPACTS, BOTH BENEFICIAL AND ADVERSE; AND PROPOSED METHODS TO MITIGATE ADVERSE PROJECT IMPACTS. FOLLOWING THE VIDEO, THERE WILL BE A SHORT INTERMISSION WHICH WILL GIVE YOU ANOTHER OPPORTUNITY TO LOOK AT THE DISPLAYS AS WELL AS DISCUSS THE PROJECT WITH PROJECT TEAM MEMBERS AND SHARE YOUR QUESTIONS AND CONCERNS WITH THEM.
- THIRD, A FORMAL COMMENT PERIOD WILL FOLLOW THE INTERMISSION WHERE YOU WILL HAVE THE OPPORTUNITY TO PROVIDE ORAL

STATEMENTS AT THE MICROPHONE, OR YOU MAY PROVIDE YOUR COMMENTS DIRECTLY TO THE COURT REPORTER OR IN WRITING. THESE COMMENTS SHOULD BE "FOR" OR "AGAINST" THE PROJECT. AN IMPORTANT PART OF THIS TESTIMONY IS THE REASON WHY YOU ARE "FOR" OR "AGAINST" THE PROJECT.

(PAUSE)

WE ARE HERE TONIGHT TO PRESENT TO YOU AND EXPLAIN THE ENGINEERING AND ENVIRONMENTAL STUDIES THAT HAVE BEEN CONDUCTED TO DATE. WE ARE ALSO HERE TO GIVE YOU AN OPPORTUNITY TO PUBLICLY AND OFFICIALLY COMMENT ON THE PROJECT.

WE BRING A PROPOSED PROJECT TO A PUBLIC HEARING TO SOLICIT YOUR VIEWS AND COMMENTS. WE WANT TO HEAR FROM PEOPLE WITH LOCAL KNOWLEDGE, AND WE WANT TO HEAR WHAT YOU LIKE AND DON' T LIKE ABOUT THE PROPOSED IMPROVEMENTS.

IT'S IMPORTANT THAT INTERESTED PEOPLE LIKE YOU HAVE AN OPPORTUNITY TO BECOME FULLY AWARE OF THIS PROJECT. IT IS ALSO IMPORTANT THAT YOU EXPRESS YOUR VIEWS AT THIS STAGE OF THE PROJECT WHEN THE FLEXIBILITY STILL EXISTS TO INCORPORATE THOSE VIEWS INTO THE STUDY DOCUMENTS. FINAL DECISIONS ARE MADE USING THESE DOCUMENTS.

WHEN YOU ARRIVED THIS EVENING, YOU WERE OFFERED A HANDOUT CONTAINING INFORMATION ABOUT THE PROPOSED PROJECT AND A COMMENT SHEET **(HOLD THEM UP)**. IF YOU DID NOT RECEIVE THEM, PLEASE PICK UP A COPY AT THE REGISTRATION TABLE WHERE YOU SIGNED IN.

IF YOU CHOOSE NOT TO SPEAK THIS EVENING, BUT WOULD LIKE TO MAKE COMMENTS FOR THE RECORD, YOU CAN COMPLETE THE COMMENT SHEET TONIGHT AND DROP IT IN THE COMMENT BOX. THIS HEARING WILL REMAIN OPEN UNTIL NOVEMBER 26, 2018. ANYONE WISHING TO MAKE COMMENTS, WHICH WILL BECOME A PART OF THE OFFICIAL TRANSCRIPT OF THE HEARING, HAS UNTIL THIS DATE TO WRITE THE DEPARTMENT. YOU MAY ALSO SEND YOUR COMMENTS BY E-MAIL TO THE ADDRESS LISTED ON THE POSTER BOARD OR SUBMIT A COMMENT USING THE PROJECT WEBSITE, WWW.SR29COLLIER.COM. THOSE COMMENTS RECEIVED AND POSTMARKED BY NOVEMBER 26, 2018 WILL BE THE SAME AS IF YOU GAVE YOUR COMMENTS AT THE MICROPHONE HERE THIS EVENING.

AFTER TONIGHT'S HEARING, THE DEPARTMENT WILL COMPILE YOUR COMMENTS AND, TOGETHER WITH THE ENGINEERING AND ENVIRONMENTAL WORK THAT HAS BEEN DONE, MAKE A FINAL RECOMMENDATION THAT WE WILL SUBMIT TO FDOT'S OFFICE OF ENVIRONMENTAL MANAGEMENT FOR APPROVAL. WE WILL PUBLISH APPROVAL OF THE RECOMMENDED ALTERNATIVE IN THE *NAPLES DAILY NEWS* AND *THE IMMOKALEE BULLETIN*, AND POST THE APPROVAL ON THE PROJECT WEBSITE.

WE WILL NOW BEGIN THE AUDIOVISUAL PRESENTATION.

AUDIO VISUAL PRESENTATION

(AFTER THE VIDEO PRESENTATION)

WE WILL HAVE THE INTERMISSION IN JUST A MOMENT.

FIRST, I WOULD LIKE TO DESCRIBE HOW YOU CAN SIGN UP TO GIVE YOUR ORAL TESTIMONY.

KRIS CELLA IS HOLDING SPEAKER CARDS. IF YOU WISH TO SPEAK THIS EVENING, PLEASE OBTAIN A SPEAKER CARD; COMPLETE THE INFORMATION ON THE CARD BY PRINTING NEATLY; THEN, RETURN THE CARD TO KRIS. I WILL CALL ON YOU TO SPEAK IN THE ORDER THAT THE CARDS ARE RETURNED.

WE WILL NOW TAKE A 15-MINUTE INTERMISSION SO YOU CAN REVIEW THE DISPLAYS AND TALK WITH DEPARTMENT REPRESENTATIVES BEFORE THE TESTIMONY PORTION OF THE HEARING. THE TIME IS 6:-----

WE WILL RECONVENE THE HEARING AT 6:----- .

INTERMISSION - 15 MINUTES

PUBLIC TESTIMONY

LADIES AND GENTLEMEN, WE WILL NOW BEGIN THE PUBLIC TESTIMONY PORTION OF THE HEARING. IF YOU ARE HOLDING A SPEAKER CARD, PLEASE GIVE IT TO A MEMBER OF THE PROJECT TEAM. IF YOU HAVE NOT RECEIVED A SPEAKER CARD AND WISH TO SPEAK, PLEASE RAISE YOUR HAND SO YOU CAN RECEIVE A CARD TO FILL OUT.

WE WILL NOW CALL UPON THOSE WHO HAVE TURNED IN A SPEAKER CARD. WHEN YOU COME FORWARD, PLEASE STATE YOUR NAME AND ADDRESS. IF YOU REPRESENT AN ORGANIZATION, MUNICIPALITY, OR OTHER PUBLIC BODY, PLEASE PROVIDE THAT INFORMATION AS WELL. IN ORDER TO GIVE EVERYONE AN OPPORTUNITY TO SPEAK, WE ASK THAT YOU LIMIT YOUR INPUT TO 3 MINUTES. IF YOU HAVE ADDITIONAL COMMENTS, YOU CAN PROVIDE THEM DIRECTLY TO THE COURT REPORTER OR IN WRITING. PLEASE COME TO THE MICROPHONE SO THE COURT REPORTER WILL BE ABLE TO GET A COMPLETE RECORD OF YOUR COMMENTS.

NOW, THE PUBLIC TESTIMONY PORTION OF OUR HEARING. THE FIRST CARD I HAVE IS - - - - -

CLOSING

IS THERE ANYONE WHO HAS NOT SPOKEN WHO WOULD LIKE TO SPEAK? IF SO, STATE YOUR NAME AND ADDRESS AND COMPLETE A SPEAKER CARD AFTER YOU HAVE GIVEN YOUR STATEMENT FOR THE PUBLIC RECORD.

(SEEING NO ONE...)

THE VERBATIM TRANSCRIPT OF THIS HEARING'S ORAL PROCEEDINGS, TOGETHER WITH ALL WRITTEN MATERIAL RECEIVED AS PART OF THE HEARING RECORD AND ALL STUDIES, DISPLAYS, AND INFORMATIONAL MATERIAL PROVIDED AT THE HEARING WILL BE MADE A PART OF THE PROJECT DECISION-MAKING PROCESS AND WILL BE AVAILABLE AT THE DISTRICT OFFICE FOR PUBLIC REVIEW UPON REQUEST.

IF ANYONE WISHES TO SUBMIT WRITTEN STATEMENTS OR OTHER EXHIBITS IN PLACE OF OR IN ADDITION TO ORAL STATEMENTS, THEY MAY DO SO. ONCE AGAIN, WRITTEN STATEMENTS AND EXHIBITS WILL BE ACCEPTED AND RECORDED AS PART OF THIS HEARING IF POSTMARKED BY NOVEMBER 26, 2018.

PLEASE MAIL YOUR STATEMENTS TO ME, GWEN G. PIPKIN, PROJECT MANAGER, FLORIDA DEPARTMENT OF TRANSPORTATION, POST OFFICE BOX 1249, BARTOW, FLORIDA 33831. THIS IS THE SAME ADDRESS THAT APPEARS ON THE BACK OF THE COMMENT SHEET.

THANK YOU FOR ATTENDING THIS PUBLIC HEARING AND FOR PROVIDING YOUR INPUT INTO THIS PROJECT. IT IS NOW **(STATE THE TIME)**. I HEREBY OFFICIALLY CLOSE THE PUBLIC HEARING FOR THE STATE ROAD 29 PD&E STUDY. THANK YOU AGAIN AND HAVE A GOOD EVENING.

APPENDIX P

***Typical Section Package –
CR 846 to SR 82 Refinements***

Preferred Alternative Typical Sections

Within the project limits, SR 29 has been divided into the following eight typical sections:

From Oil Well Road to South of Kaicasa Entrance

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 40-foot median). There is an open drainage system, and the design speed is 65 mph.

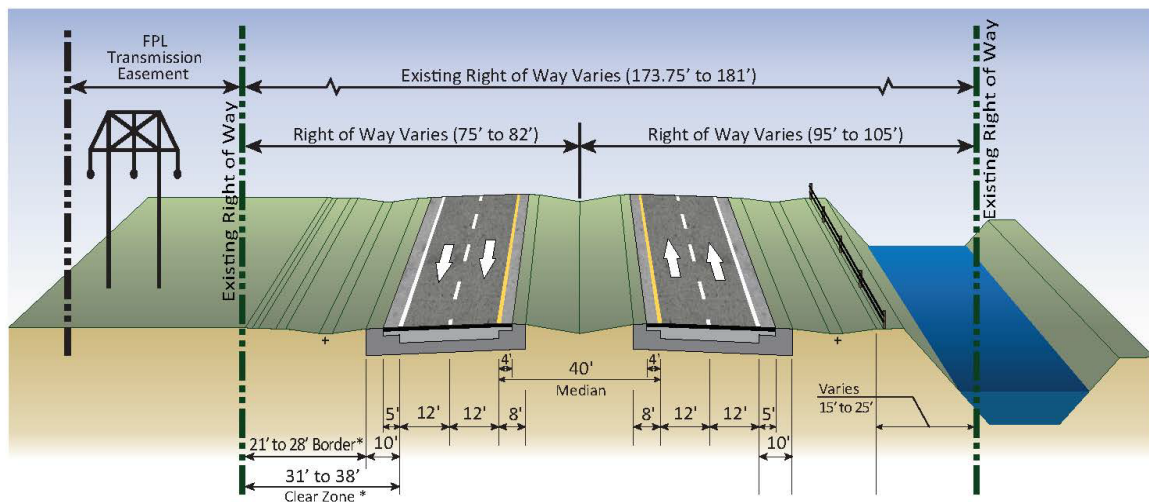
The existing ROW varies from 173.75 feet to 181 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.1** depicts this typical section.

From South of Kaicasa Entrance to North of Seminole Crossing Trail

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 30-foot median), with a 10-foot shared use path on the west side of the corridor from Farm Worker Way to Seminole Crossing Trail. There is an open drainage system, and the design speed is 55 mph.

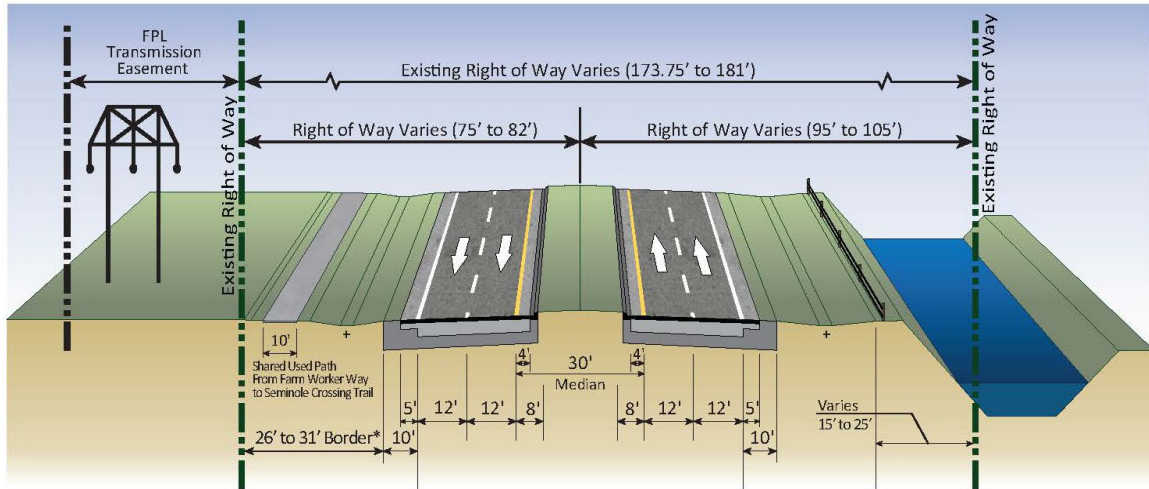
The existing ROW varies from 173.75 feet to 181 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits, except for the canal relocation near Seminole Crossing Trail. **Figure 1.2** depicts this typical section.

Figure 1.1
SR 29 Typical Section from Oil Well Road to South of Kaicasa Entrance



* Clear Zone and Border Width Require Design Variations
+ DBI if warranted

Figure 1.2
SR 29 Typical Section from South of Kaicasa Entrance to North of Seminole Crossing Trail



* Border Width Requires Design Variation
 + DBI, as warranted

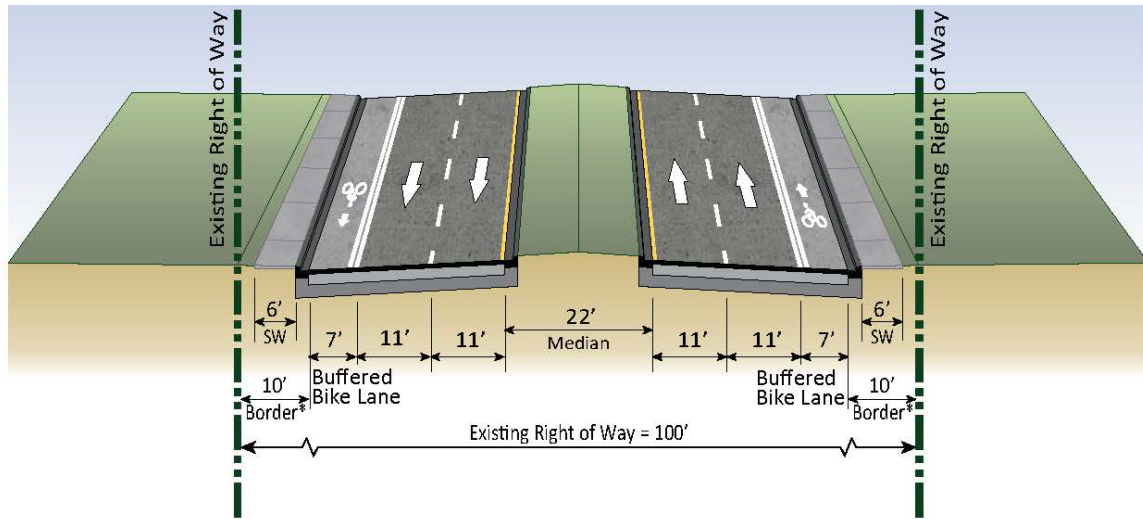
From North of Seminole Crossing Trail to South of CR 846

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 11-foot lanes in each direction and a 22-foot median), with 7-foot buffered bicycle lanes and 6-foot sidewalks in each direction. There is a closed drainage system with curb and gutter, and the design speed is 45 mph.

The existing ROW is 100 feet. The ROW width needed for this typical section can mostly be accommodated within the existing ROW limits, except for some additional ROW needed for a turn lane near 13th Street. **Figure 1.3** depicts this typical section.

(This space intentionally left blank)

Figure 1.3
SR 29 Typical Section from North of Seminole Crossing Trail to South of CR 846



* 10' Border Width Requires Design Variation Where Constrained by 100' Existing ROW

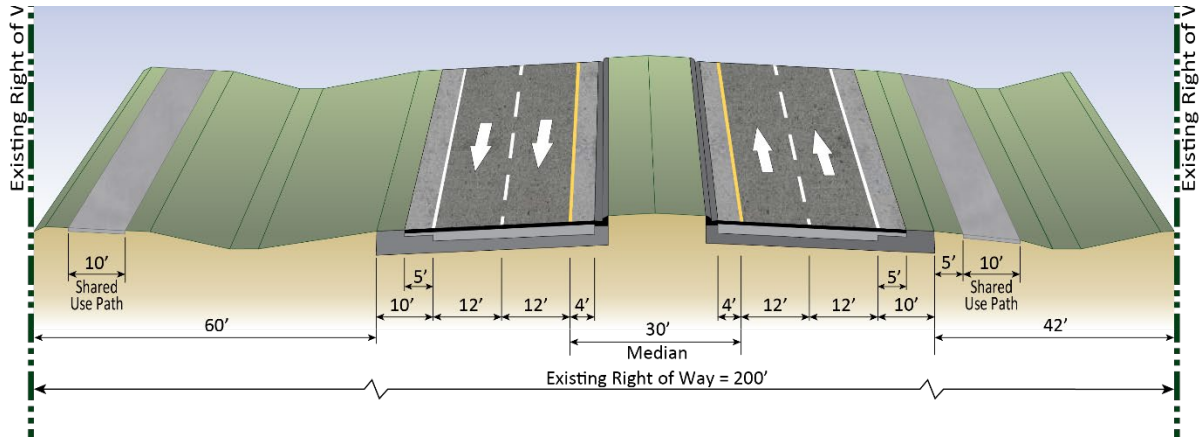
From South of Westclox Street/New Market Road W to Heritage Boulevard

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 30-foot median), with a 10-foot shared use path on both sides of the corridor. There is an open drainage system, and the design speed will be 55 mph when the SR 29 Bypass is constructed.

The existing ROW is 200 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.4** depicts this typical section.

(This space intentionally left blank)

Figure 1.4
SR 29 Typical Section from South of Westclox Street/New Market Road W to Heritage Boulevard

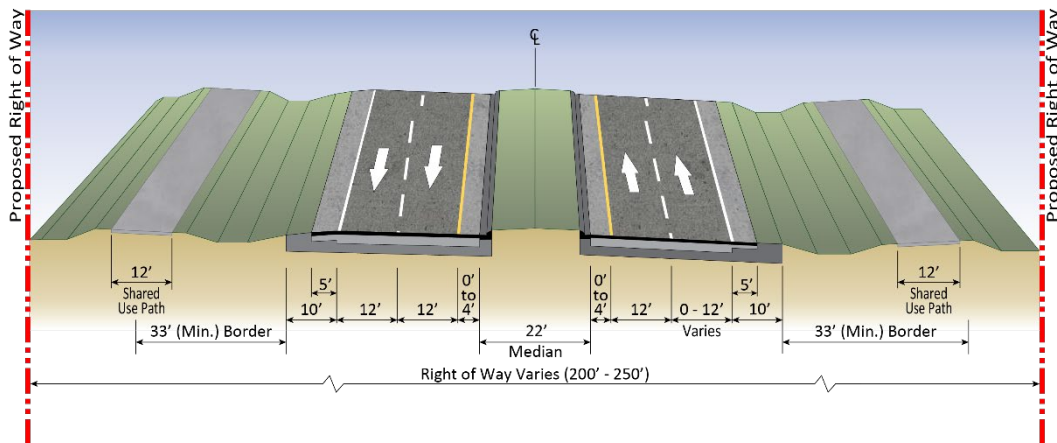


From Heritage Boulevard to SR 29 Bypass Junction

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 22-foot to 30-foot median), with a 12-foot shared use path on both sides of the corridor. There is an open drainage system, and the design speed is 45 mph.

The ROW width needed for this typical section is 200 to 250 feet. **Figure 1.5** depicts this typical section.

Figure 1.5
SR 29 Typical Section from Heritage Boulevard to SR 29 Bypass Junction

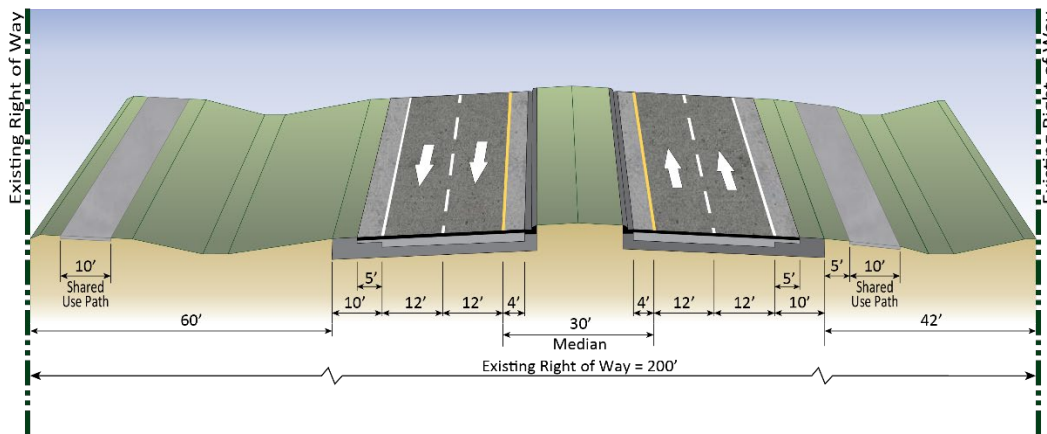


From Experimental Road to South of SR 82

The existing 2-lane undivided roadway is widened to a 4-lane divided typical section (two (2) 12-foot lanes in each direction and a 40-foot median), with a 10-foot shared use path on both sides of the corridor. There is an open drainage system, and the design speed is 55 mph.

The existing ROW is 200 feet. The ROW width needed for this typical section can be accommodated within the existing ROW limits. **Figure 1.6** depicts this typical section.

Figure 1.6
SR 29 Typical Section from Experimental Road to South of SR 82



SR 29 Bypass Portion

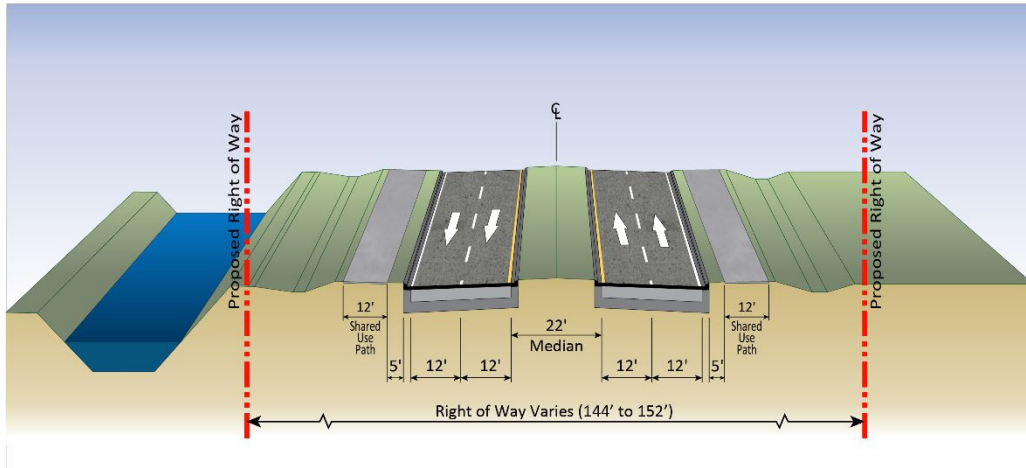
Within the project limits, the proposed SR 29 Bypass portion of the Preferred Alternative from CR 846 to the SR 29 Bypass junction with SR 29 north of Westclox Street/New Market Road W can be divided into the following two typical sections:

From South of CR 846 to Gopher Ridge Road

A 4-lane divided typical section (two (2) 12-foot travel lanes in each direction and a 22-foot median) is proposed, with a 12-foot shared use path in each direction. There is an open drainage system, and the design speed is 45 mph.

The ROW width needed for this typical section is 144 to 152 feet. **Figure 1.7** depicts this typical section.

Figure 1.7
SR 29 Bypass Typical Section from South of CR 846 to Gopher Ridge Road

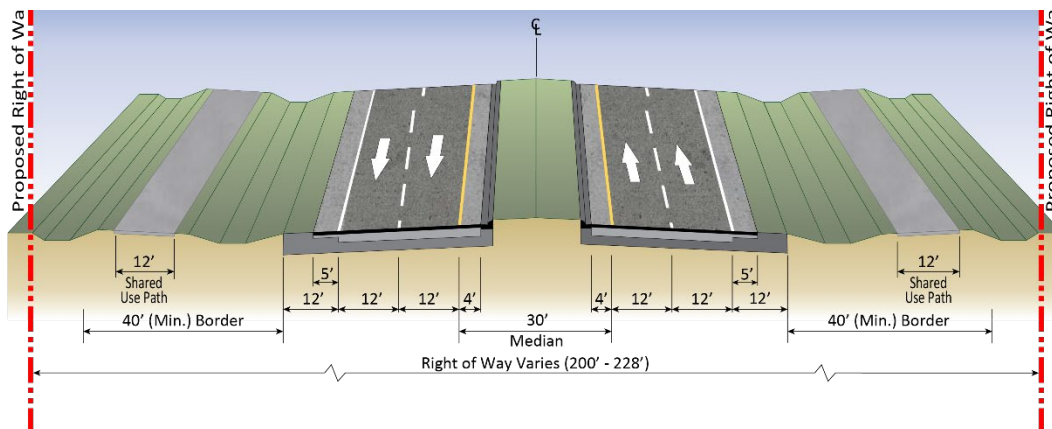


From Gopher Ridge Road to Experimental Road

A 4-lane divided typical section (two (2) 12-foot travel lanes in each direction and a 30-foot median) is proposed, with 12-foot shared use paths on both sides of the corridor. There is an open drainage system, and the design speed is 50 to 55 mph.

The ROW width needed for this typical section is 200 to 228 feet. **Figure 1.8** depicts this typical section.

Figure 1.8
SR 29 Bypass Typical Section from Gopher Ridge Road to Experimental Road



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APPENDIX Q

***Collier County Parks & Recreation
Administration Concurrence Letter***

Request for concurrence, Airport Park boundary, FDOT District One, SR 29 PD&E Study, 417540-1

Trinity Scott <Trinity.Scott@colliercountyfl.gov>

Tue 3/5/2024 4:26 PM

To: Marino, Matthew <Matthew.Marino@dot.state.fl.us>

Cc: James, Jeffrey W <Jeffrey.James@dot.state.fl.us>; Randi Swinderman <Randi.Swinderman@colliercountyfl.gov>; James Hanrahan <James.Hanrahan2@colliercountyfl.gov>; Tanya Williams <Tanya.Williams@colliercountyfl.gov>; Ed Finn <Ed.Finn@colliercountyfl.gov>; Olema Edwards <Olema.Edwards@colliercountyfl.gov>; Warren, Kimberly <Kimberly.Warren@dot.state.fl.us>; Amy Patterson <Amy.Patterson@colliercountyfl.gov>; Jeanne Marcella <Jeanne.Marcella@colliercountyfl.gov>

 1 attachments (1 MB)

Image_001.pdf;

EXTERNAL SENDER: Use caution with links and attachments.

Mr. Marino,

Attached please find the signed letter for concurrence, as discussed with Ms. Warren, Airport Road provides access to the park and based on the preliminary plans that we have reviewed, the park will have access from the new loop road.

Should you have any questions, please contact me.

Best Regards,

Trinity Scott, Department Head
Collier County Transportation Management Services Department
2885 South Horseshoe Drive
Naples, Florida 34104
(239) 252-5873
Trinity.Scott@colliercountyfl.gov

Under Florida Law, e-mail addresses are public records. If you do not want your e-mail address released in response to a public records request, do not send electronic mail to this entity. Instead, contact this office by telephone or in writing.



Florida Department of Transportation

RON DESANTIS
GOVERNOR

801 North Broadway Avenue
Bartow, FL 33830

JARED W. PERDUE, P.E.
SECRETARY

February 14, 2024

Olema Edwards, Director
Collier County Parks & Recreation Administration
15000 Livingston Road
Naples, FL 34109

**RE: Project Development and Environment (PD&E) Study
SR 29 from Oil Well Road to SR 82
Collier County, Florida
Financial Project ID No.: 417540-1**

Dear Olema Edwards:

The Florida Department of Transportation (FDOT), District One, is conducting a Project Development and Environment (PD&E) Study, in accordance with the National Environmental Policy Act (NEPA), in unincorporated Collier County, Florida. As part of the study, FDOT is proposing to add a new four-lane roadway bypassing the downtown area of unincorporated Immokalee. Section 4(f) of the Department of Transportation Act of 1966 (49 U.S.C., Section 303) governs the use of publicly owned parks, recreation areas, wildlife and waterfowl refuges, and public and private historic sites for U.S. Department of Transportation projects. FDOT has previously determined that Airport Park, located west of Airways Road and north of SR 29, qualifies as a "Section 4(f) protected property."

FDOT identified Airport Park as publicly owned and managed by your agency and as a recreational resource potentially protected under Section 4(f) of the US Department of Transportation Act of 1966. Based on exhibits from the Collier County Airport Authority's Airport Master Plan (**Figure 1**) and coordination with Parks & Recreation Administration on January 22, 2024, the FDOT has determined the park is 3.09 acres with a boundary as depicted in **Figure 2**. The 3.09-acre park boundary includes the recreational area shown in blue (2.09 acres) and the parking area shown in red (1.0 acre).

Section 4(f) of the US Department of Transportation Act of 1966 requires USDOT agencies (and their legally authorized designees) to make specific findings when a USDOT-funded or approved transportation project requires the use of land from a Section 4(f) protected property. FDOT reviewed Airport Park for significance and has determined that it plays an important role in meeting Collier County's recreational and park objectives. Therefore, Airport Park is considered significant under Section 4(f).

As the OWJ over Airport Park, we are requesting your concurrence with the determination that the 3.09-acre Airport Park boundary as depicted in **Figure 2** is accurate. If you concur, please sign and date the concurrence block below and return it to me at the address shown in the letterhead, or preferably by e-mail to Jeffrey.James@dot.state.fl.us as soon as possible. FDOT will assume your concurrence with the determinations of this letter should there be no response within 30 days.

SR 29 from Oil Well Road to SR 82
Collier County
FPID No.: 417540-1
February 14, 2024
Page 2 of 4

Sincerely,



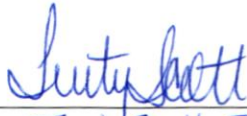
Jeffrey James
Environmental Manager

CC:

James Hanrahan, Assistant Director, Parks & Recreation Administration
Randi Swinderman, Regional Manager, Region 3, Parks & Recreation Administration
Kim Warren, Patel, Greene & Associates
Matt Marino, FDOT District One Cultural Resource Coordinator

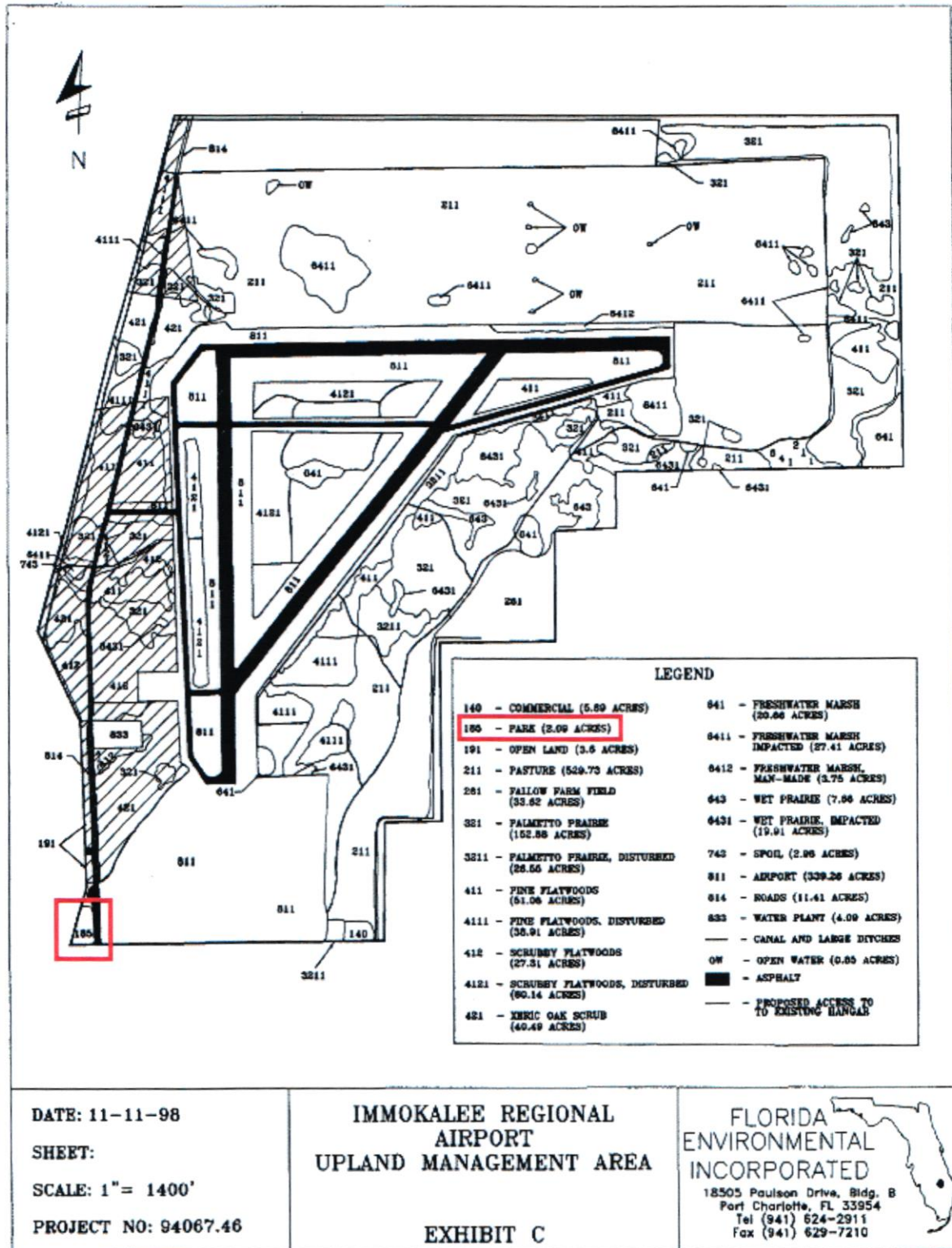
Enclosures: Figures

Collier County concurs with the boundary determination made regarding **Airport Park**



Signature *Trinity Scott, Transportation Management Services*
Collier County Parks Director (or designee) *Department Head*

3/5/2024
Date



DATE: 11-11-98
 SHEET:
 SCALE: 1" = 1400'
 PROJECT NO: 94067.46

IMMOKALEE REGIONAL
 AIRPORT
 UPLAND MANAGEMENT AREA
 EXHIBIT C

FLORIDA ENVIRONMENTAL INCORPORATED
 18505 Paulson Drive, Bldg. B
 Fort Charlotte, FL 33954
 Tel (941) 624-2911
 Fax (941) 629-7210

Figure 1. Excerpt from Collier County Airport Authority's Airport Master Plan with 2.09-acre Airport Park called out (does not include 1.0-acre parking area).



Figure 2. The 3.09-acre Airport Park boundary as understood by FDOT includes the recreational area shown in blue (2.09 acres) and the parking area shown in red (1.0 acre).

APPENDIX R

***SHPO Concurrence Letter
for CRAS Addendum***



Florida Department of Transportation

RON DESANTIS
GOVERNOR

801 N. Broadway Avenue
Bartow, FL 33830

JARED W. PERDUE, P.E.
SECRETARY

February 23, 2024

Alissa S. Lotane,
Director and State Historic Preservation Officer
Florida Division of Historical Resources
Florida Department of State
R.A. Gray Building
500 South Bronough Street
Tallahassee, Florida 32399-0250

Attn: Ms. Alyssa McManus, Transportation Compliance Review Program

RE: Cultural Resource Assessment Survey Addendum Report
State Road (SR) 29 from Oil Well Road to State Road (SR) 82
Collier County, Florida
Financial Project No.: 417540-1-22-01

Dear Ms. Lotane,

Enclosed please find one copy of the report titled *Cultural Resource Assessment Survey Addendum Report State Road (SR) 29 from Oil Well Road to State Road (SR) 82, Collier County, Florida*. The Florida Department of Transportation (FDOT), District One is conducting a Project Development and Environment (PD&E) Study in accordance with the National Environmental Policy Act (NEPA), to assess the need for capacity and traffic operational improvements along a two-lane undivided section of State Road (SR) 29 extending 15.6 miles from Oil Well Road (southern terminus) to SR 82 (northern terminus) in unincorporated Collier County, Florida. The project proposes to widen existing two-lane undivided sections of SR 29 to four lanes, as well as add a new four-lane roadway bypassing the downtown area of Immokalee.

A PD&E Public Hearing was held on November 15, 2018, to present the Preferred Alternative and provide the public with the opportunity to review project documents and provide comments. After the Public Hearing was held, a decision was made to place the project on hold in order to complete consultation with the United States Fish and Wildlife Service (USFWS) prior to completing the PD&E study. During this time, the Moving Florida Forward infrastructure initiative was approved (January 2023) providing funding to advance construction of the northern portion of the project from County Road (CR) 846 to State Road (SR 82), hereinafter referred to as the proposed project. Therefore, design for this portion of the project is proceeding concurrent with the PD&E study. Refinements to the Preferred Alternative have been made to meet the FDOT Design Manual (FDM) requirements. The refinements also include the identification of pond sites, or stormwater management facilities (SMF), necessary to accommodate stormwater runoff. This Cultural Resource Assessment Survey (CRAS) Addendum is an addendum to the original PD&E CRAS (SR 29 from Oil Well Road (CR 858) to SR 82 (Survey No. 25332) that was prepared in 2018 and specifically addresses the design refinements for the portion of the project from CR 846 to SR 82. The design refinements include the following.

CR 846 to SR 29 Bypass Junction: The proposed new signalized intersection at CR 846 and the proposed intersection at Gopher Ridge Road have been revised to a roundabout at these locations. The proposed right-of-way (ROW) requirement previously varied from 108 feet to 200 feet and has been increased to varying from 144 feet to 250 feet. The two 11-foot travel lanes in each direction have been increased to 12-foot travel lanes in each direction from CR 846 to Gopher Ridge Road. The 6-foot sidewalk and 7-foot buffered bicycle lanes in each direction have been replaced with 12-foot shared use paths from CR 846 to Gopher Ridge Road. Twelve-foot shared use paths have been added to both sides of the corridor from Gopher Ridge Road to the SR 29 Bypass Junction. As a result of criteria updates, the proposed design speeds, ranging from 45-50 miles per hour (mph), have been updated and range from 45-55 mph. Three SMFs have been identified. The three proposed SMFs will require approximately 22 acres of offsite ROW. Stormwater runoff will be conveyed to the proposed SMFs by an open drainage system within the existing mainline right-of-way.

North of New Market Road West to SR 82: The currently existing signalized intersection at New Market Road West and SR 29 has been revised to a roundabout at this location. A 10-foot shared use path has been added on the east side of the roadway from north of New Market Road West to SR 82, thus providing a 10-foot shared use path on both sides of the corridor. The mainline roadway improvements required for the proposed project will not require any additional ROW. As a result of criteria updates, the proposed design speeds, ranging from 50-60 mph, have been unified at 55 mph. Six SMFs have been identified. The six proposed SMFs will require approximately 20.3 acres of offsite ROW. Stormwater runoff will be conveyed to the proposed SMFs by an open drainage system within the existing mainline ROW.

Based on the scale and nature of the activities, the project has a limited potential for any indirect (visual or audible) or cumulative effects outside the immediate footprint of construction. Because of the project type and location of the proposed work, the archaeological APE is defined as the footprint of construction of the pond sites and within the corridor. The historical/architectural APE includes the archaeological APE and adjacent parcels up to 200-feet from edge of existing ROW and 100 feet from preferred pond sites. This APE remains in-keeping with the 2018 CRAS (Janus Research 2018; Survey No. 25332) which was submitted to the State Historic Preservation Officer (SHPO) in 2018 and the SHPO concurred with the survey findings (SHPO File No. 2018-3480).

This CRAS was conducted in accordance with the requirements set forth in Section 106 of the National Historic Preservation Act of 1966, as amended, found in 36 CFR Part 800 (Protection of Historic Properties). The studies also comply with Chapter 267 of the Florida Statutes and Rule Chapter 1A-46, Florida Administrative Code and Section 267.12, Florida Statutes, Chapter 1A-32. All work was performed in accordance with Part 2, Chapter 8 of FDOT's PD&E Manual (2023 revision), FDOT's Cultural Resources Management Handbook, and the standards stipulated in the Florida Division of Historical Resources' (FDHR) *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The Principal Investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 FR 44716-42). This study also complies with Public Law 113-287 (Title 54 U.S.C.), which incorporates the provisions of the National Historic Preservation Act of 1966, as amended, and the Archeological and Historic Preservation Act of 1974, as amended.

Archaeological background research indicated that no previously recorded historic or pre-Contact archaeological sites were identified within the APE and no sites were recorded within the general vicinity. A review of relevant site information for environmentally similar areas within Collier County and the surrounding region indicated a low to moderate potential for the occurrence of pre-Contact sites within the APE. The background indicated that pre-Contact sites, if present, would most likely be small lithic or

artifact scatters. As a result of the archaeological field survey, no archaeological sites were identified within the APE.

Historical background research revealed that 17 historic resources were previously recorded within the APE (8CR01087, 8CR01184, 8CR01185, 8CR01187, 8CR01188, 8CR01189, 8CR01236, 8CR01237, 8CR01238, 8CR01309, 8CR01333, 8CR01334, 8CR01368, 8CR01370, 8CR01496, 8CR01498, 8CR01500). These include 11 buildings, three linear resources, one culvert, one building complex resource group, and one designed historic landscape that have been determined ineligible for listing in the NRHP by the SHPO. In addition to these 17 resources, during the historic/architectural field survey, new resources were identified. These include two buildings, a ca. 1971 Mid-Century Modern style building (8CR01645) and a ca. 1970 Masonry Vernacular style building (8CR01646), which were recorded and evaluated within the APE. Overall, the newly identified buildings lack sufficient architectural features, and are not significant embodiments of a type, period, or method of construction. In addition, a 0.25-mile segment of SR 29 (8CR01309) was updated within the APE to account for the modifications extending the northern portion of the original study corridor to SR 82. The linear resource is a common example of a four-lane divided roadway found throughout Collier County and is the same design as the segments that were previously recorded and evaluated as ineligible by the SHPO within the APE to the south. Background research did not reveal any historic associations with significant persons and/or events. Thus, the resources do not appear eligible for listing in the NRHP, either individually or as a part of a historic district. The remaining 16 historic resources were previously recorded (8CR01087, 8CR01184, 8CR01185, 8CR01187, 8CR01188, 8CR01189, 8CR01236, 8CR01237, 8CR01238, 8CR01333, 8CR01334, 8CR01368, 8CR01370, 8CR01496, 8CR01498, 8CR01500) within the APE and were not re-evaluated since the SHPO already determined they were ineligible for listing in the NRHP, and no significant changes were observed during the field survey. Of the 12 extant historic resources, two previously recorded historic resources, the boundary of Immokalee Regional Airport (8CR01087) and Eutopia Canal (8CR01498), are located within the proposed pond sites, Ponds 501B and 501C. In addition, Madison Avenue Canal (8CR01368) is located adjacent to Ponds 502A and 502B and the Eutopia Canal (8CR01498) is located adjacent to Pond 501C. SR 29 (8CR01309) is located adjacent to six proposed pond sites (601A, 602B, 603/604B, 605A, 606A, and 607A).

Based on the results of this study, it is the opinion of the District that the proposed undertaking will result in *no historic properties affected*.

I respectfully request your concurrence with the findings of the enclosed report.

If you have any questions or need further assistance, please contact me at (863) 519-2625 or Jeffrey.James@dot.state.fl.us.

Sincerely,




Jeffrey James
Environmental Manager
FDOT, District One

Enclosures: one original copy of the CRAS Report (February 2024), three (3) FMSF forms, one completed Survey Log, GIS shapefiles

The Florida State Historic Preservation Officer finds the attached Cultural Resource Assessment Survey Report complete and sufficient and concurs / does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR Project File Number 202400888. Or, the SHPO finds the attached document contains _____ insufficient information.

In accordance with the *Programmatic Agreement Among the FHWA, the FDOT, the ACHP, and the SHPO Regarding Implementation of the Federal-Aid Highway Program in Florida (2023 PA)*, and appended materials, if providing concurrence with a finding of **No Historic Properties Affected** for a whole project, or to **No Adverse Effect** on a specific historic property, SHPO shall presume that FDOT may pursue a *de minimis* use of the affected historic property in accordance with Section 4(f) as set forth within *23 C.F.R. Part 774* and its implementing authorities, as amended, and that their concurrence as the official with jurisdiction (OWJ) over the historic property is granted.

SHPO Comments:


Alissa S. Lotane, Director
Florida Division of Historical Resources

3/21/24
Date

APPENDIX S

FWS RAI and FDOT OEM Responses

From: [Bennett, Jonathon](#)
To: [Garrett, Harrison](#)
Cc: [Peters, Lauren](#); [Clark, Thu-Huong](#); [Cornwell, Katasha](#); [James, Jeffrey W](#); [Kimberly Warren](#)
Subject: RE: [EXTERNAL] 417540-1 SR 29 Immokalee Formal Consultation Request
Date: Tuesday, December 14, 2021 2:30:06 PM

Harrison,

We are going to have an internal discussion on the 12/16th after that we will find a time that works for OEM to discuss further.

Thank you,

Jonathon A. Bennett

Environmental Project Manager

ETDM Coordinator

Florida Department of Transportation District One
801 North Broadway Avenue | Bartow, Florida 33830

PH: (863) 519-2495 EMAIL: Jonathon.Bennett@dot.state.fl.us



From: Garrett, Harrison <Harrison.Garrett@dot.state.fl.us>
Sent: Tuesday, December 14, 2021 1:37 PM
To: Bennett, Jonathon <Jonathon.Bennett@dot.state.fl.us>
Cc: Peters, Lauren <Lauren.Peters@dot.state.fl.us>; Clark, Thu-Huong <Thu-Huong.Clark@dot.state.fl.us>; Cornwell, Katasha <Katasha.Cornwell@dot.state.fl.us>
Subject: FW: [EXTERNAL] 417540-1 SR 29 Immokalee Formal Consultation Request

Jonathon,

Let's get a meeting scheduled between D-1 and OEM for us to get some more background here and for us to discuss moving forward before we schedule that meeting with John Wrublik. From OEM please include Katasha, Thu, and myself.

Harrison Garrett, MSW

Project Delivery Coordinator
Florida Department of Transportation
Office of Environmental Management
605 Suwannee Street, MS-37
Tallahassee, FL 32399-0450
PH: 850-414-4943
Harrison.Garrett@dot.state.fl.us

From: Wrublik, John <john_wrublik@fws.gov>

Sent: Tuesday, December 14, 2021 9:47 AM

To: Garrett, Harrison <Harrison.Garrett@dot.state.fl.us>

Cc: Bennett, Jonathon <Jonathon.Bennett@dot.state.fl.us>; Peters, Lauren <Lauren.Peters@dot.state.fl.us>; Clark, Thu-Huong <Thu-Huong.Clark@dot.state.fl.us>; Kimberly Warren <kwarren@rkk.com>; Turner, Jonathan <Jonathan.Turner@dot.state.fl.us>; Rivera, Jose J <jose_rivera@fws.gov>; Carey, Robert L <robert_carey@fws.gov>; Dryden, Kim <kim_dryden@fws.gov>

Subject: Re: [EXTERNAL] 417540-1 SR 29 Immokalee Formal Consultation Request

EXTERNAL SENDER: Use caution with links and attachments.

Harrison,

I am sorry that I have not yet been able to respond to your consultation request for the State Road 29 from State Road 82 to Oil Well Road (SR 29 bypass) project. I have been out of the office on use or lose annual leave for a good part of November, and I will be off as well starting this Friday until January 3, 2022 for the holidays. I have also been swamped with other work for the few days I have been in the office. I wanted to give you a heads up regarding a serious issue that could affect your project. As reported in the Natural Resource Addendum (NREA) that you provided for the project, the SR 29 bypass project as currently proposed would affect Florida scrub-jay habitat with the areas denoted as the Collier Property and the Upland Management Area. The latter area is protected through conservation easement and was set aside as a mitigation area for the Florida scrub-jay in the Service's 1998 biological opinion to the Federal Aviation Administration (Service Log Number 4-197-F-556) for development activities at the Immokalee Airport. Consequently, the State Road 29 bypass as currently proposed, would affect lands that are protected for conservation purposes. More importantly, the project has the potential to extirpate the last remaining known Florida scrub-jays in Collier County. As such, it seems unlikely that the Service could support the project as currently designed. I plan to meet with my management to discuss the project, although due to the upcoming holidays, we will probably not be able to meet internally until after everyone returns from the holiday break. After this meeting, I will schedule a meeting with you and the appropriate FDOT staff from District 1 so that we can discuss these issues further.

John

John M. Wrublik
U.S. Fish and Wildlife Service
1339 20th Street
Vero Beach, Florida 32960
Office: (772) 469-4282
Fax: (772) 562-4288
email: John_Wrublik@fws.gov

NOTE: This email correspondence and any attachments to and from this sender is subject to the Freedom of

Information Act (FOIA) and may be disclosed to third parties.

From: harrison.garrett@dot.state.fl.us <harrison.garrett@dot.state.fl.us>

Sent: Wednesday, November 17, 2021 1:38 PM

To: Wrublik, John <john_wrublik@fws.gov>; Rivera, Jose J <jose_rivera@fws.gov>

Cc: jonathon.bennett@dot.state.fl.us <jonathon.bennett@dot.state.fl.us>;

lauren.peters@dot.state.fl.us <lauren.peters@dot.state.fl.us>; thu-huong.clark@dot.state.fl.us <thu-huong.clark@dot.state.fl.us>; kwarren@rkk.com <kwarren@rkk.com>;

jonathan.turner@dot.state.fl.us <jonathan.turner@dot.state.fl.us>

Subject: [EXTERNAL] 417540-1 SR 29 Immokalee Formal Consultation Request

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.



Garrett, Harrison sent you a secure message

[Access message](#)

John/Jose

Please find attached transmittal letter requesting formal consultation and the Biological Addendum. I understand that John will be on leave through 11/30/2021. In that case we would like to respectfully request to have the consultation processed to ensure all documentation and information has been received to ensure John has everything at his disposal upon his return. Please let me know if there is anything further I can do to assist with this request.

Secured by Accellion



Attachments expire on Dec 01, 2021



1 document

21-09-22_FWS_NRE_FormalConsultation_Transmital.doc



1 PDF

SR 29 PD Full clean document.pdf

This message requires that you sign in to access the message and any file attachments.

From: Kuhn-Hendricks, Katlin <Katlin.Kuhn-Hendricks@dot.state.fl.us>

Sent: Tuesday, December 19, 2023 10:00 AM

To: john_wrublik@fws.gov

Cc: robert_carey@fws.gov; Cantrell, Mark A <mark_a_cantrell@fws.gov>; Marshall, Jennifer <Jennifer.Marshall@dot.state.fl.us>; Cornwell, Katasha <Katasha.Cornwell@dot.state.fl.us>; Clark, Thu-Huong <Thu-Huong.Clark@dot.state.fl.us>; Turner, Jonathan <Jonathan.Turner@dot.state.fl.us>; Horne, Abra <Abra.Horne@dot.state.fl.us>; James, Jeffrey W <Jeffrey.James@dot.state.fl.us>; Setchell, Brent <Brent.Setchell@dot.state.fl.us>

Subject: FPID 417540-1 SR 29 from SR 82 to Oil Well Rd - USFWS RAI Response

Good morning,

The FDOT is providing the attached documents in response to the USFWS RAI for the above-mentioned project. Please let me know if you have any questions.

Thank you,

Katlin Kuhn-Hendricks
Project Delivery Coordinator
Office of Environmental Management
Florida Department of Transportation
605 Suwannee Street, MS-37
Tallahassee, FL 32399
Office: (850) 414-5226
Email: Katlin.Kuhn-Hendricks@dot.state.fl.us

Email from John Wrublik to Thu-Huong Clark (22-05-24, 11:28 AM)

Federally listed species expected to be adversely affected by the project

The Service finds that the Project will result in adverse effects to the endangered Florida panther (*Puma concolor coryi*), and Florida bonneted bat (*Eumops floridanus*) and the threatened Florida scrub-jay (*Aphelocoma coerulescens*) and Audubon’s crested caracara (*Polyborus plancus audubonii*).

As a result of a meeting between the U.S. Fish and Wildlife Service (USFWS) and the Florida Department of Transportation (FDOT) on November 6, 2023, it was agreed that a Biological Opinion (BO) would be issued for the northern portion of State Road (SR) 29 from south of the County Road (CR) 846 Intersection to SR 82, which is currently funded for construction beginning in fiscal year (FY) 2027. The FDOT anticipates that the BO will also provide review of the effects determinations for documented species with a “No Effect” and concurrence with the effects determinations for documented species with a “may affect, not likely to adversely affect (MANLAA)” determinations to complete consultation for the northern portion (see **Exhibit 1** below) of SR 29. FDOT anticipates pursuing permits for the northern portion of the project in the near future to support the FY 2027 construction schedule.

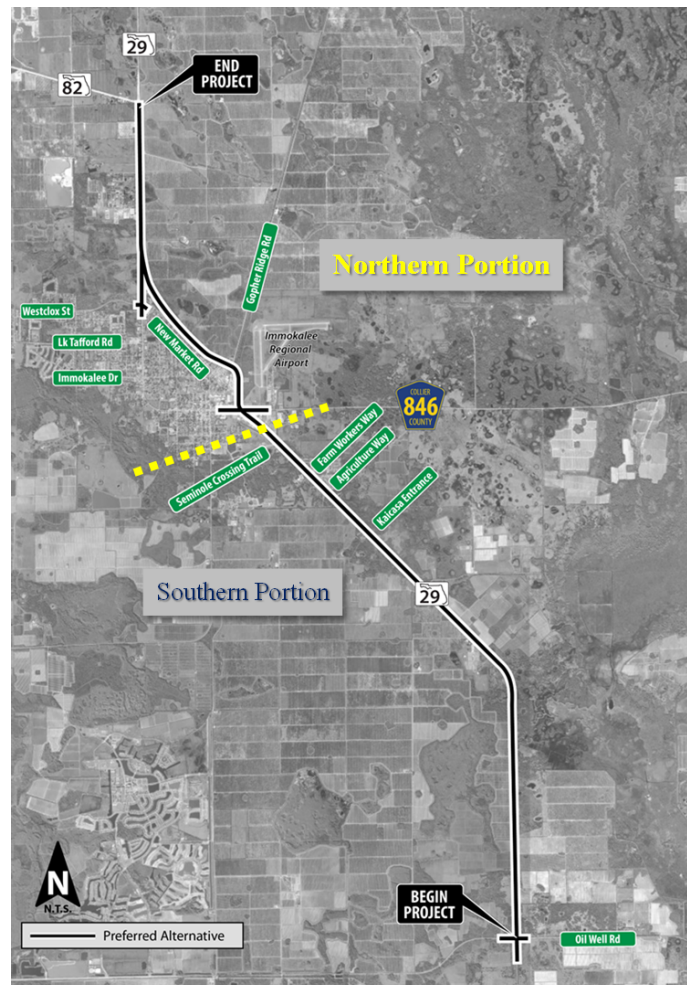


Exhibit 1: Northern Segment versus Southern Segment

The FDOT commits to re-initiate consultation for the southern portion of SR 29 from Oil Well Road to south of the CR 846 intersection during the permitting process for individual design segments.

Based on coordination with the USFWS, the FDOT will re-initiate consultation with the USFWS for the limits of the project from Oil Well Road to south of the CR 846 intersection for the Florida panther, Florida bonneted bat and Audubon's crested caracara. At that time, the FDOT will provide additional information, as needed, that will allow the USFWS to complete their analysis of the project's effects on documented species and complete consultation, for the project in accordance with Section 7 of the Endangered Species Act of 1973, as amended, during the design and permitting project phases.

The responses provided below address the northern portion of SR 29 from the CR 846 intersection to SR 82 only and includes the mainline roadway and stormwater management facilities (SMF)/Floodplain Compensation (FPC) sites which the FDOT design teams have identified.

FLORIDA PANTHER

Comment 1: The new bypass road east of Immokalee represents a potential threat to the Florida panther because panthers could be struck by motor vehicles if they enter or attempt to cross the roadway. The Service notes that a small portion of the bypass footprint is located in the Service's Focus Area for the Florida panther and the remainder of the footprint is located near the Focus area. As such, the Service finds it likely that panthers could occur in the Project area and enter the bypass roadway. As a protective measure, the Service requests that 8 to 10-foot-tall chain link fence be installed within the right-of-way immediately east of the new paved bypass roadway. The purpose of the fence would be to prevent panthers from entering the bypass. Underpass structures and fence along the Eastern right-of-way of the bypass road right-of-way would not be necessary within this section of the Project because much of the lands located west of the proposed bypass contain residential and commercial development and do not contain panther habitat. Please indicate if the FDOT would be willing to install barrier fence along the bypass (as described) in association with the project.

Response 1: *The Department believes the risk to panthers (and motorists) is very small along the new alignment (bypass) portion of SR 29 as there have been no existing panther vehicle collisions along either the existing SR 29 or New Market Road which parallels the new alignment. The new alignment was purposely aligned to be very close to the urbanized area of Immokalee, which deters use by panthers. There are no natural habitats south of the proposed SR 29 alignment attracting panther usage. Only a single telemetry data point documented in 1998 occurred within the bypass footprint or south. However, the Department commits to monitoring this section of the SR 29 bypass as part of the annual hotspots update and if a panther vehicle collision occurs, FDOT will implement best management practices consistent with the Florida Panther Conservation Plan currently being developed in partnership with the USFWS.*

Comment 2: The Service is concerned that the proposed bypass roadway east of Immokalee associated with the Project will provide new access to currently undeveloped lands that may provide habitat for the Florida panther. These lands are located East of the proposed bypass footprint. We find this access is likely to promote commercial and residential development in the Project area that would not be likely to occur but for the new motor vehicle access provided by the bypass. Such development would likely result in the loss of habitat for the panther and ultimately has the potential to affect the survival and recovery of the species. To reduce the potential for indirect habitat loss due to the bypass, the Service requests that the bypass be designated as a limited access road that would allow minimal access to the West of the bypass to tie into the current developed lands within the City of Immokalee, but not allow new access to the East of the bypass. Please indicate if the FDOT would be willing to do this as part of the project as a measure to minimize the Project's adverse impacts to the panther.

Response 2: *The Department commits to implementing best management practices consistent with the Florida Panther Conservation Plan currently being developed in partnership with the USFWS.*

Comment 3: The Service finds that the project is likely to increase the potential for vehicle-related panther mortalities in the Project footprint. In order for the Service to estimate the incidental take of panthers from motor vehicle strikes likely to occur due to the Project, the Service requests an estimate of the current motor vehicle traffic (in Annual Average Daily Traffic [AADT]) and an estimate of motor vehicle traffic (in AADT) expected to occur in the future (2043*) for the following segments of the Project corridor following widening:

- 1) SR 29 project corridor from SR 82 to New Market Road
- 2) SR 29 project corridor from County Road 846 to Oil Well Road.

In addition, we request an estimate of motor vehicle traffic (in AADT) expected to occur in the proposed bypass roadway located East of Immokalee when it opens following construction and, in the future (2043*).

*I used 2043 as the year for the future motor vehicle traffic estimate because this is the year of the future estimate of motor vehicle traffic that FDOT provided for the SR 82 from Hendry County Line to Gator Slough Lane biological opinion. If this is not correct, we leave it to the FDOT to determine the appropriate future year for which an estimate of traffic could be made

Response 3: *The requested traffic information is documented below.*

*The information provided in **Exhibit 2** (next page) addresses the traffic requested in sub-bullet 1 above (SR 29 project corridor from New Market Road to SR 29 Bypass and SR 29 Bypass to SR 82). The table below provides the associated Existing Year (2017), Opening Year (2025) and Design Year (2045) AADT volumes. The segment of the project from the CR 846 intersection to SR 82 is the subject of our request for a Biological Opinion (BO).*

Exhibit 2: Existing and Anticipated AADT Volumes

Roadway Segment	2017	2025	2045
New Market Road to SR 29 Bypass	18,000	12,000	19,000
SR 29 Bypass to SR 82	18,000	25,000	41,000

Comment 4: Proposed Panther Underpass at Owl Hammock – The FDOT has proposed to install an underpass for panthers and other wildlife to cross under the roadway within the SR 29 corridor at Owl Hammock. Please provide the proposed design information for this underpass including the length of the barrier fence associated with the underpass.

Response 4: *The Owl Hammock underpass is located south of the CR 846 intersection and is not located within the limits of the section of SR 29 for which we are requesting a Biological Opinion. However, the FDOT has conducted a Wildlife Crossing Memorandum (June 2022) (Attachment A) to assist with identifying the approximate location of a proposed crossing of the existing SR 29 roadway in the area of Owl Hammock. The Owl Hammock area is located between Oil Well Road and Sunniland Nursery Road. As stated in the June 2022 report (Section 5.5, page 5-2), the preferred location of the crossing is approximately 1,200 feet north of the Gator Slough bridge and falls within the area of recent panther and black bear vehicle collisions, and within a telemetry documented high wildlife usage area.*

FDOT commits to constructing the wildlife crossing in the southern segment. This commitment will not have to wait until that portion of SR 29 is constructed. This crossing will be listed at the annual prioritization meeting of the Panther Conservation Plan to determine priority for available funding. As part of the preferred recommendation, directional fencing associated with the proposed crossing would be consistent with the Florida Panther Conservation Plan and, as appropriate, the Wildlife Crossing Memorandum (June 2022) (Attachment A).

FLORIDA SCRUB-JAY

Comment 1: As a conservation measure to benefit the survival and recovery of the scrub-jay, the FDOT has proposed to compensate for impacts to 2 occupied scrub-jay territories located in lands identified in FDOT’s biological assessment as the Collier Property. The construction of the proposed SR 29 bypass roadway East of Immokalee is expected to result in the loss of two occupied scrub-jay territories that comprise 52.14 acres of habitat. To compensate for the loss of the two occupied territories, the FDOT has proposed to provide at least 104.28 acres of currently occupied scrub-jay habitat at a Service-approved scrub-jay conservation bank.

The Project will also result in the loss of 10.41 acres of potential scrub-jay habitat that is located within a 151.5-acre conservation area at the Immokalee Airport known as the Upland Management Area (UMA). This area was established to protect and manage scrub habitat for the scrub-jay and other scrub dependent species by the Collier County Airport Authority through approval by the Federal Aviation Administration in association with and as documented in the Service’s biological opinion for improvements to the airport dated January 14, 1998. This area was established within the intent that it be protected in perpetuity to benefit the scrub-jay. However, it appears the appropriate protective easement

was not established when the biological opinion was issued. Because the bypass roadway will result in the loss of habitat in a conservation area, we request that the FDOT provide at least 41.64 acres of currently unprotected and occupied scrub-jay habitat as compensation for this loss (i.e., compensation provided at a ratio of 4 acres of occupied scrub-jay habitat protected for every acre of habitat impacted within the existing conservation area). We find this rate of compensation to be appropriate because the impacts are associated with an area that was set aside for conservation purposes in association with a previous Federal action.

In summary, we request that the FDOT provide a total of 145.92 acres of occupied scrub-jay habitat (104.28 associated with the loss of two scrub territories within the Collier Property + 41.64 associated with scrub-jay habitat within UMA = 145.92) as a conservation measure to compensate for the loss of scrub-jay habitat resulting from the Project. Please indicate if this is acceptable to the FDOT.

Response 1: *Based on our review of the Biological Opinion (BO) issued in 1998 by the USFWS for the Immokalee Regional Airport (FWS Log No.: 4-1-97-F-556), the USFWS indicated in the BO Terms and Conditions that the 151.5-acre Upland Management Area (UMA) was to be established for compensation for anticipated loss of occupied scrub-jay habitat. This area is currently under a conservation easement to the Florida Fish and Wildlife Conservation Commission (FWC) (previously the Florida Game and Freshwater Fish Commission) for gopher tortoise conservation. The land was intended to be managed for both species.*

While historically Type I and Type II habitat, the UMA is now in poor condition due to the management plan not being implemented as intended. The plan includes alternating prescribed burns and herbicide use to address nuisance vegetation and overgrowth. Prescribed burns have not been utilized due to airport safety concerns. Accordingly, the habitat has deteriorated. Based upon field observations in 2010, 2011, 2018, and the species-specific survey conducted in October 2020, scrub-jays are not present within the UMA.

The FDOT design teams have identified two new Stormwater Management Facilities (SMF) within scrub-jay habitat:

- *One SMF is located within the limits of the UMA, within an area isolated from the remainder of the UMA by the proposed bypass roadway (**Attachment B: Florida scrub-jay Habitat with Pond Sites and UMA Map**).*
 - *The drainage analysis identified and studied several sites seeking to avoid and/ or minimize impacts to the UMA. It was recommended that locating an SMF in the remainder of that parcel would have the fewest impacts.*
 - *This SMF will result in an additional 5.34 acres of impact, in addition to the previously identified 10.41 acres of impact for the bypass road, for a total of 15.75 acres of impact to the UMA.*
- *The second SMF is located within the portion of the Collier property where two active scrub-jay territories have been identified (**Attachment B: Florida scrub-jay Habitat with Pond Sites and UMA Map**). Since FDOT has proposed a total take of both territories and will mitigate for these impacts as noted in your comment*

above, there will be no additional acreage of impact associated with the SMF in this location.

*The USFWS has proposed a replacement ratio of four acres for each acre impacted within the UMA by the project. Based on discussions with the FFWCC in November 2013 (see **Attachment C: FWWCC Meeting Notes**), they require a replacement ratio of two acres for each acre of UMA habitat impacted. The FDOT proposes to mitigate at a ratio of two acres per one acre of impact for the loss of 52.14 total acres of occupied territory on the Collier property and a ratio of four acres per one acre of impact for the loss of 15.75 acres of the UMA. Therefore, FDOT will provide a total of 167.28 acres of occupied scrub-jay habitat (104.28 associated with the loss of two scrub-jay territories within the Collier Property + an additional 63 acres associated with potential habitat loss within the UMA = 167.28) as a conservation measure to compensate for the loss of scrub-jay habitat resulting from the Project.*

Comment 2: The Service notes that FDOT's SR 29 Project is not located with scrub-jay mitigation service area (see attached map) that contains an approved scrub-jay conservation bank. Consequently, we request that the FDOT provide 145.92 acres of occupied scrub habitat either through the acquisition of credits at the Tippen Bay Scrub-Jay Conservation Bank in DeSoto County (the bank closest to the Project site) or by providing funding to the Service's Florida Scrub-jay Conservation Fund sufficient to acquire 145.92 acres of scrub-jay habitat. Please indicate if this acceptable to the FDOT.

Response 2: *In addition to the two alternative mitigation options identified by the USFWS, the FDOT requests approval to utilize available Florida scrub-jay mitigation credits at the Platt Branch Mitigation Bank in Highlands County. This mitigation bank is approximately the same distance from the project site as the Tippen Bay Scrub-Jay Conservation Bank in DeSoto County. FDOT proposes to provide a total of 167.28 acres of occupied scrub-jay habitat and requests the addition of Platt Branch as a mitigation option. As discussed during the November 6, 2023 meeting, the Platt Branch MOU with USFWS would not need to be modified, but the BO could address this request for out of service area mitigation. The location of the two alternative mitigation options is depicted in **Attachment D: Map of Scrub-Jay Mitigation Sites Depicting Mileage**. The current ledger for Platt Branch showing the proposed deduction of the 167.28 credits is depicted in **Attachment E: Platt Branch Species Credit Ledger**.*

FLORIDA BONNETED BAT

Comment 1: The Service notes that acoustic surveys conducted by FDOT's consultant in the Project footprint recorded the call of a Florida bonneted bat (FBB) within 1 and ½ hours of sunset. This evidence suggests that the FBB is likely to be roosting on the Project site and is reasonably certain to occur. Consequently, the Service finds that the Project may affect and is likely to adversely affect the FBB. The Service notes that the FDOT has determined that the Project may affect, but is not likely to adversely affect the FBB. We recommend that you change your determination for the FBB in association with the Project to may affect, likely to adversely affect. Please let me know if this is acceptable to the FDOT.

Response 1: *The determination for the bonneted bat of "may affect, likely to adversely affect" is acceptable.*

Comment 2: As a conservation measure to benefit the FBB and to help meet the FDOT's and the Federal Highway Administration's responsibilities pursuant to Section 7(a)(1) of the Endangered Species Act of 1973, we request that the FDOT contribute \$10,000.00 to the Service's Florida bonneted bat fund administered by the Wildlife Foundation of Florida. Monies accumulated in the fund are used to support measures that aid in the survival and recovery of FBB. Please indicate if this acceptable to the FDOT.

Response 2: *The FDOT agrees to contribute \$10,000.00 to the USFWS Florida Bonneted Bat Fund (Attachment F: FBB Conservation Fund Fee, page 4).*

AUDUBON'S CRESTED CARACARA

Comment 1: Nest surveys conducted by the FDOT's consultant in association with the Project documented an active nest of the Audubon's crested caracara (caracara) approximately 279 feet west of the Project footprint approximately 1 mile north of Oil Well Road.

The Service has determined that the Project will result in the loss of caracara habitat within the Primary Zone (i.e., all lands within 985 feet) of this nest site and is likely to adversely affect the caracara. As such, we will include the caracara in the biological opinion for the project and conduct the jeopardy analysis associated with project for this species. The Service notes that the FDOT has determined that the Project may affect, but is not likely to adversely affect the caracara. We recommend that you change your determination for the caracara in association with the Project to may affect, likely to adversely affect. Please let me know if this is acceptable to the FDOT.

Response 1: *The subject nest (Station 10) is located south of the CR 846 intersection and is not within the limits of the project segment being advanced. The FDOT will re-initiate ESA Formal Section 7 consultation during design and permitting phase for this subject nest. There is an active nest (Station 1) located in the segment north of the CR 846 intersection, approximately 0.55 miles west of SR 29 and south of SR 82 (Exhibit 3: Caracara Secondary Zone with Pond Sites Location Map). The northernmost portion of this project segment, approximately 0.6 miles (3,100 feet), is within the secondary zone of this nest. The proposed effects determination for the caracara is "may affect, not likely to adversely affect". This is based on the following: the project's location within the nest's secondary zone; the acquisition of high-quality upland and wetland credits, which will also mitigate for the loss of habitat; and FDOT's commitment to implement conservation measures identified on p. 4-3 of the NRE Addendum (listed below).*

Land clearing activities for the project will be conducted outside of the Audubon's crested caracara nesting season (December 1 through April 30) to the greatest extent practicable. Since caracara nesting season is from December 1 through April 30, clearing should be completed between May 1 and November 30. Should it be necessary to conduct land clearing activities within the nesting season, the FDOT or their designated agent will survey suitable caracara nesting habitat to determine if an active caracara nest occurs within or adjacent to the project area. If an active nest is observed within 300 meters (985 feet) of the project area, land clearing within 300 meters (985 feet) of the nest will not occur until monitoring has determined the nest has either been abandoned, or chicks within the nest have fledged and left the nest site.

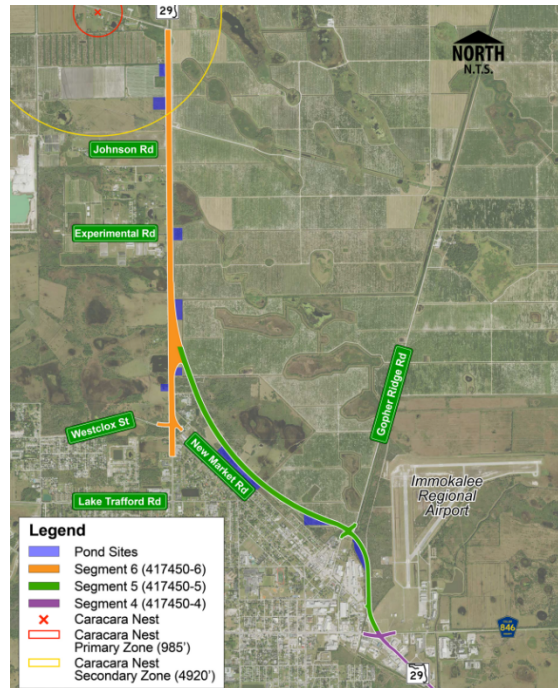


Exhibit 3: Caracara Secondary Zone with Pond Sites Location Map

Comment 2: Also, please provide the total acreage of the Project footprint that occurs within the Primary Zone of this nest and the acreage of each habitat or land cover type that occurs within this acreage.

Response 2: *The subject nest (Station 10) is located south of the CR 846 intersection and is not within the limits of the project segment being advanced.*

Federally listed species not expected to be affected by the project

EASTERN INDIGO SNAKE

Comment 1: The FDOT has determined that the Project may affect but is not likely to adversely affect the threatened Eastern indigo snake (EIS; *Drymarchon corais couperi*). The Service notes that the species was not observed to occur within the Project footprint during pedestrian inspections conducted by FDOT's consultants. Furthermore, the Service does not have records of EIS occurring in or within 0.62 mile of the Project footprint. Consequently, we find that the EIS is not reasonably certain to occur within the Project corridor and is unlikely to be affected by the Project. We recommend that the FDOT change its determination for the EIS from may affect, not likely to adversely affect, to no effect. Please let me know if this is acceptable to FDOT.

Response 1: *The FDOT accepts the effects determination of “no effect” for the Eastern indigo snake.*

Comment 1: I have been reviewing the plans for the SR 29 from SR 82 to Oil Well Road Project provided in FDOT's biological assessment dated December 8, 2021. The plans show approximately 41 sites listed as potential pond sites (stormwater treatment ponds I assume). Can you tell me about how many stormwater treatment ponds will be constructed for the project? Also, did your consultant include stormwater treatment ponds located in the portion of the project footprint in the Service's panther focus area when they calculated the number of panther habitat units (PHUs) impacted by the project and the number of PHUs needed to offset the panther habitat lost due to the project? (see table 3-10 on page 3-73 of your biological assessment). The loss of panther habitat due to the construction of storm water ponds in the panther focus area will need to be accounted for in the PHU calculations before I can finish the biological opinion for the project. If you would like to discuss further, please let me know.

Response 1a: *The first table below (**Exhibit 4**) summarizes the number and size of the recommended stormwater management facilities (pond sites). The second table (**Exhibit 5**) summarizes all project panther habitat impacts, including impacts by roadway and stormwater management facilities associated with the segment of the SR 29 project currently funded for construction, SR 29 from the CR 846 Intersection to SR 82. The stormwater management facilities for the southern sections of the project will be provided during the re-initiation of consultation during their design phases.*

Exhibit 4: Stormwater Management Facilities

<i>Pond Site</i>	<i>Acreage</i>	<i>Secondary Protection Zone</i>
<i>501B</i>	<i>5.5</i>	<i>No</i>
<i>502A</i>	<i>5.59</i>	<i>No</i>
<i>503B</i>	<i>11.16</i>	<i>No</i>
<i>601A</i>	<i>1.5</i>	<i>Yes</i>
<i>602B-1</i>	<i>2.1</i>	<i>Yes</i>
<i>603/604B</i>	<i>5.7</i>	<i>Yes</i>
<i>605A</i>	<i>4.0</i>	<i>Yes</i>
<i>606B</i>	<i>3.1</i>	<i>Yes</i>
<i>607A</i>	<i>2.6</i>	<i>Yes</i>

Exhibit 5: Panther Habitat Unit Assessment Summary Table

Panther Zone	Land Cover	Acres	Habitat Score	Habitat Value	Base Rate	Landscape Multiplier	PHUs Required
Secondary	211 - Improved Pasture	23.44	5.2	121.89	1.98	0.69	166.53
Secondary	213 - Woodland Pasture	1.69	5.7	9.63	1.98	0.69	13.16
Secondary	214 - Row Crops	1.92	4.8	9.22	1.98	0.69	12.6
Secondary	221 - Citrus Groves	7.09	4.7	33.32	1.98	0.69	45.52
Secondary	420 - Upland Hardwood Forest	0.34	9.0	3.06	1.98	0.69	4.18
Secondary	422 - Brazilian Pepper	0.42	3.0	1.26	1.98	0.69	1.72
Secondary	510 - Streams and Waterways	0.66	0.0	0.00	1.98	0.69	0.00
Secondary	530 - Reservoirs	0.10	0.0	0.00	1.98	0.69	0.00
Secondary	814 - Roads and Highways	57.38	0.0	0.00	1.98	0.69	0.00
Secondary Impact Totals		93.04					243.71

Attachment A

Wildlife Crossing Evaluation

SR 29

**from Oil Well Road to Sunniland Nursery Road
(Owl Hammock)
Collier County**

Financial Project ID (FPID) No. 417540-8-52-01

Florida Department of Transportation

District One



The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated May 26, 2022, and executed by FHWA and FDOT.

JUNE 2022

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APPENDICES

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Attachment 2	Detailed Preliminary Cost Estimate

1.0 Introduction and Project Description

The Florida Department of Transportation (FDOT) is conducting an evaluation for opportunities to enhance the passage of wildlife across SR 29 near Owl Hammock in Collier County. A 2.05-mile segment of SR 29 was identified to study locations and design concepts for enhanced wildlife passage. Five alternative locations were evaluated to provide passage across SR 29. The project is located in Collier County, Florida. The project location map, **Figure 1-1**, shows the evaluation area.

The evaluation segment is approximately 3.5 miles north of the intersection of SR 29 and Oil Well Road. Within this evaluation segment, the Barron Canal (Photo 1) is adjacent to SR 29 on the east side of the roadway. Existing wildlife crossings and conservation lands are shown on **Figure 1-1**



Photo 1: Barron Canal

This segment was chosen for evaluation, as the Owl Hammock area is mapped as two Hot Spot areas by the Southwest Florida Roads Panther Hot Spots Mapping Report (PRIT Transportation Subteam, 2020). Hot Spots are assigned to road segments in which multiple panther-vehicle collisions have occurred in clusters. Within this evaluation segment, nine fatal panther-vehicle collisions have occurred.

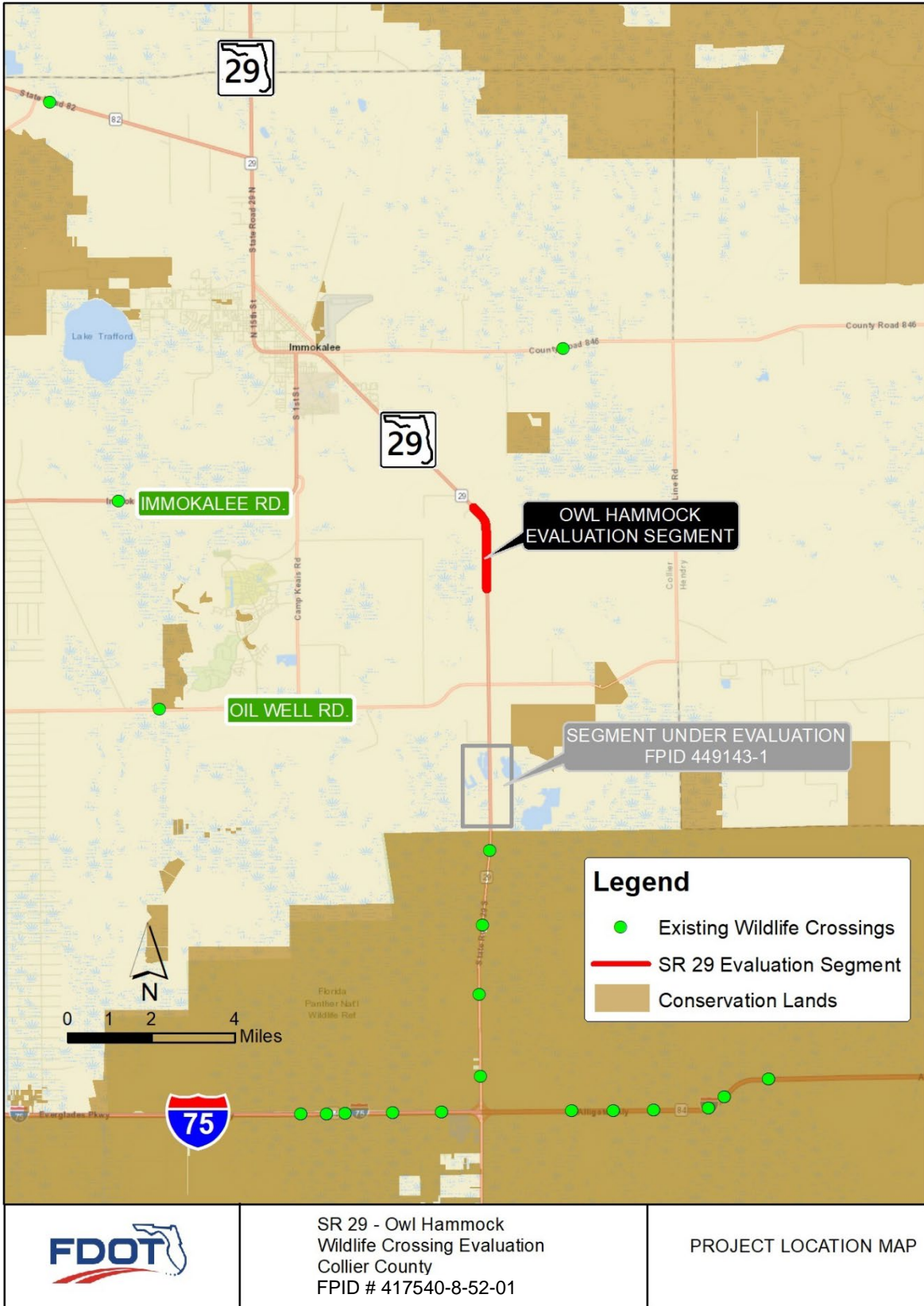


Figure 1-1: Project Location Map

SR 29 from Oil Well Road to Sunniland Nursery Way
 FPID #: 417540-8-52-01

2.0 Existing Conditions

2.1 Roadway

The eastern right-of-way is roughly at the top of the western bank of the Barron Canal, just beyond the SR 29 guardrail. The posted speed limit is 60 mph. Guardrail is present along the east side for the Barron Canal for the entire length of the evaluation segment.

Within the evaluation segment, there are five existing driveways providing direct access to SR 29. These driveways are shown on **Figure 2-1**.

2.2 Structures

The evaluation segment includes one bridge over Gator Slough (Bridge No. 030303). Bridge No. 030303 is a two-span concrete slab structure constructed in 1999.



Photo 2: Gator Slough

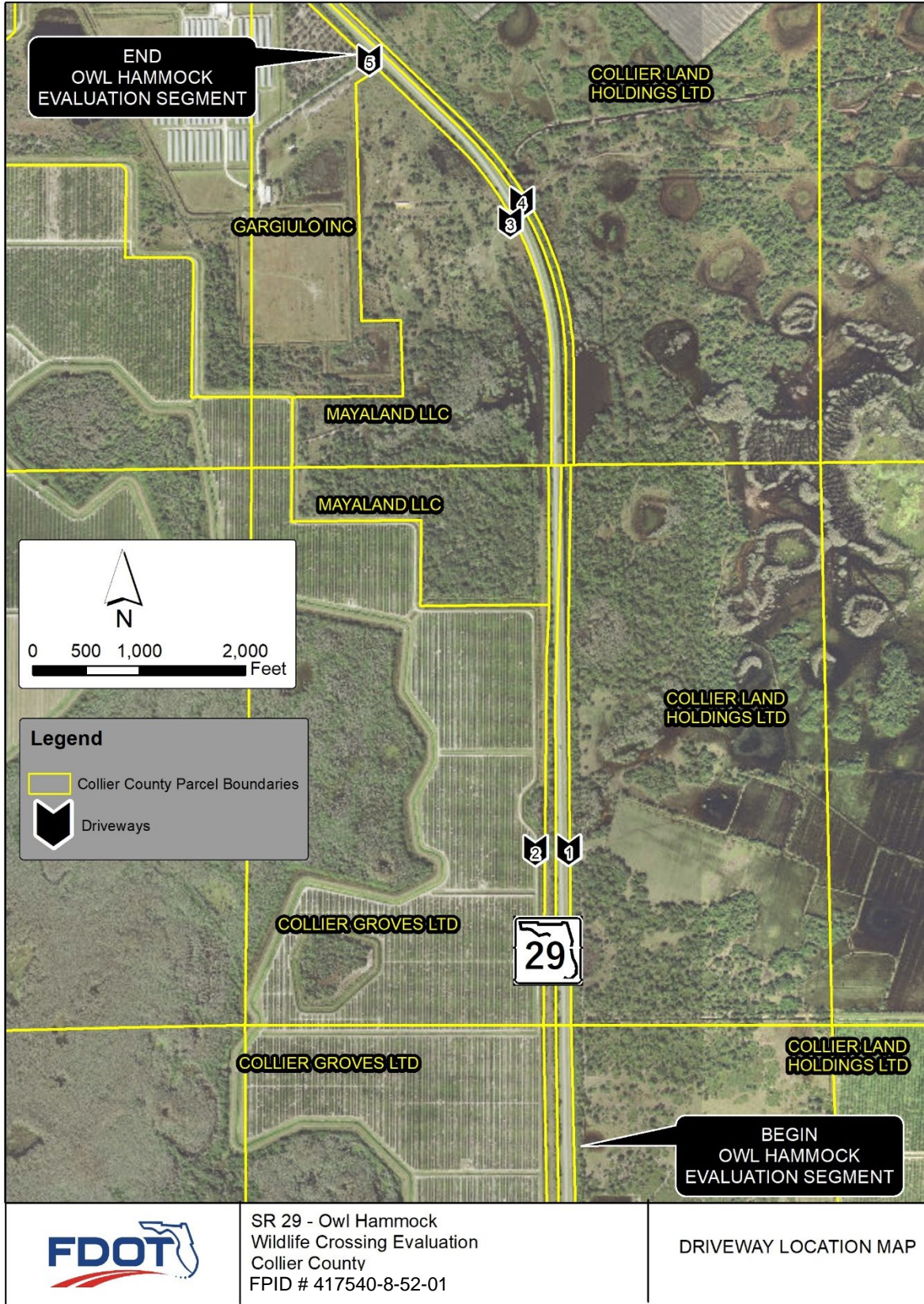


Figure 2-1: Driveway Location Map

SR 29 from Oil Well Road to Sunniland Nursery Way
FPID #: 417540-8-52-01

2.3 Drainage

Existing flow patterns are west to east beneath SR 29 and into the Barron Canal which parallels the east side of SR 29. Stormwater runoff is conveyed into the Barron Canal via three 24-inch cross drains and the bridge over Gator Slough.

The 2012 FEMA floodplain mapping for this area shows published FEMA flood elevations varying between approximately 21.5 ft-NAVD88 and 22.5 ft-NAVD88. The existing ground on the west side of SR 29 where the alignment shift will occur varies between approximately 18 ft-NAVD88 and 21 ft-NAVD88. Based on the size of the floodplain it is anticipated that modeling would be a successful approach to demonstrating no adverse floodplain impacts. As such, floodplain compensation is not considered a cost driver.

2.4 Utilities

There are no major underground utilities. Buried CentruyLink communications lines are present on the east side of SR 29 between the edge of pavement and the guardrail. Overhead utilities owned by Lee County Electric Co-Op are present to the west of SR 29 and outside FDOT right-of-way.



Photo 3: Electric Utility Corridor

3.0 Existing Environmental Conditions

This section presents a description of existing conditions within the evaluation segment, including wetlands, land use and wildlife movements.

3.1 Land Use / Land Cover

The Barron Canal is a significant surface water adjacent to the roadway. The Barron River Canal was originally constructed in the 1920's as a borrow canal to provide fill for construction of the railroad grade between Immokalee and Everglades City.

Figure 3-1 shows the existing land use/land cover map, within the evaluation segment, as mapped by the South Florida Water Management District (SFWMD) Land Use Cover, and Forms Classification System (2016). The forested wetland system associated with Gator Slough on the west side of SR 29 is the most significant wetland system within evaluation segment. Panthers use such large areas, they traverse, hunt, and shelter in many various habitat types, but they prefer mature upland forests such as hardwood hammocks and pinelands, where they hunt for their preferred prey, white-tailed deer and feral hogs.

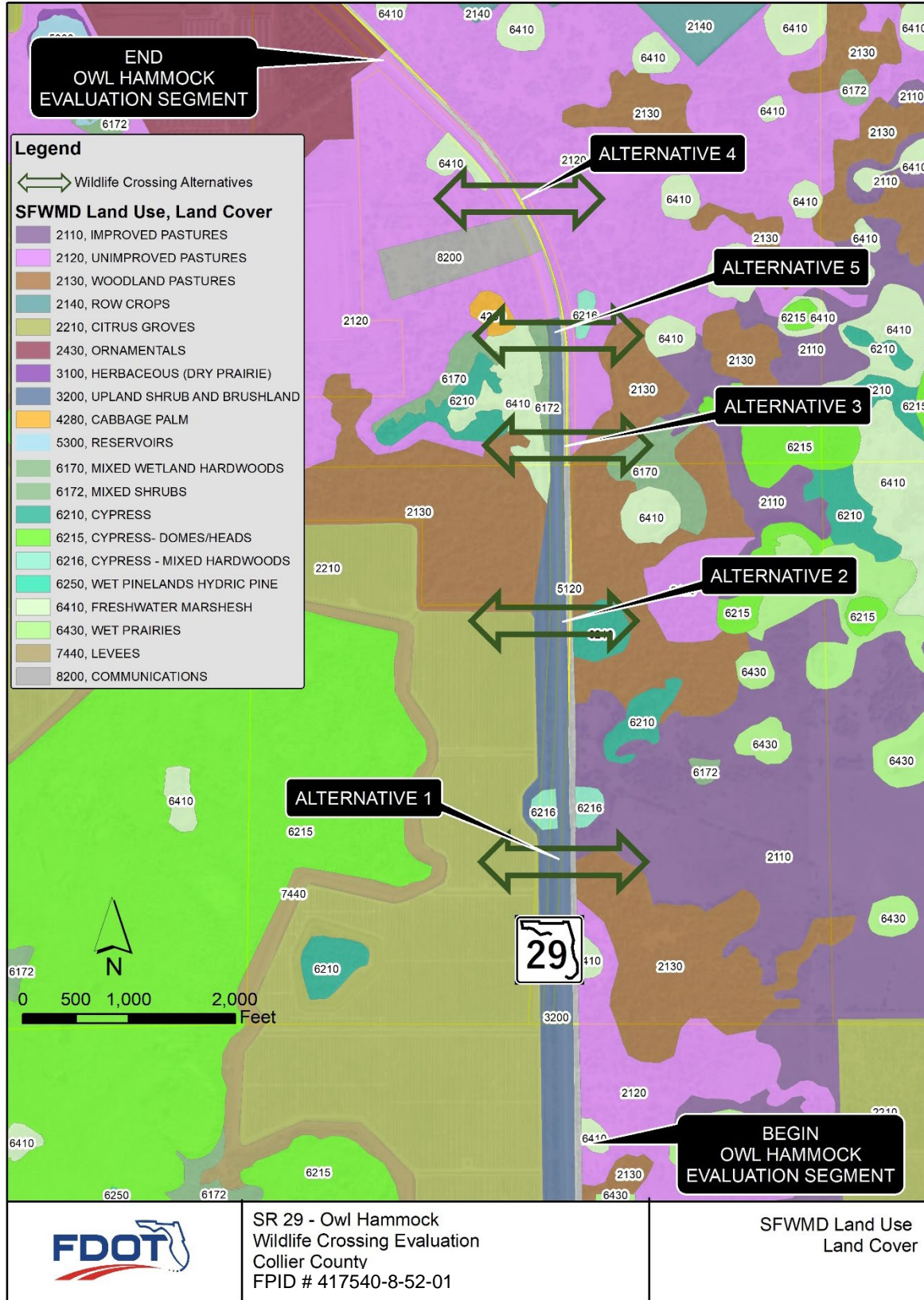


Figure 3-1: Land Use / Land Cover Map

3.2 Eastern Collier County Multiple Species Habitat Conservation Plan

The evaluation segment is within lands included in the Eastern Collier County Multiple Species Habitat Conservation Plan (ECMSHCP). The ECMSHCP proposes compact commercial/residential development and mining on up to 45,000 acres within the area covered by the plan. Conservation elements of the ECMSHCP include maintaining 107,000 acres; a management plan for preserved lands; a mitigation and monitoring plan for measuring success of the ECMSHCP; and contributions to a funding mechanism for conservation activities. If issued, the Incidental Take Permits (ITPs) would cover take incidental to development activities within the ECMSHCP area. The ITPs would also include take incidental to land management activities designed to maintain or improve habitat functions; maintain agriculture operations; maintain drainage infrastructure; control exotic vegetation; and control pests and diseases. Finally, the ITPs would consider long-term effects covering the 50-year life of the permit to include more intense use within the ECMSHCP area and other results of the covered activities. The U.S. Fish and Wildlife Service published a draft environmental impact statement (EIS) for the ECMSHP on October 19, 2018, in the Federal Register (<https://www.regulations.gov/docket/FWS-R4-ES-2018-0079>).

The draft EIS identifies 11 applicants as members of Eastern Collier Property Owners, LLC. These applicants are listed in **Table 3-1**. As the draft EIS was published in 2018, ownership in some parcels within the evaluation segment have changed ownership. As shown in **Figure 2-1**, Collier Land Holdings LTD owns land on the eastern side of SR 29 within the evaluation segment. The Florida Department of State, Division of Corporations, lists Collier Land Holdings LTD as a subsidiary of Collier Enterprises, Inc, which is a member of the Eastern Collier Property Owners, LLC.

Table 3-1: ITP Applicants

Applicants	Incidental Take Permit Application No.
Alico Land Development, Inc	TE05647D-0
Barron Collier Investment, Ltd	TE04440D-0
Collier Enterprises Management, Inc	TE04443D-0
Consolidated Citrus Limited Partnership	TE04471D-0
English Brothers Partnership	TE04152D-0
Half Circle L Ranch, LLP	TE05238D-0
Heller Bros. Packing Corp	TE05668D-0
JB Ranch I, LLC	TE04473D-0
Owl Hammock Immokalee, LLC	TE06114D-0
Pacific Land, Ltd	TE05665D-0
Sunniland Family Limited Partnership	TE04472D-0

As shown in Figure 3-2 from the draft EIS, the ECMSHCP (HCP) proposes “Preserve” lands on both sides of SR 29, in the vicinity of Owl Hammock.

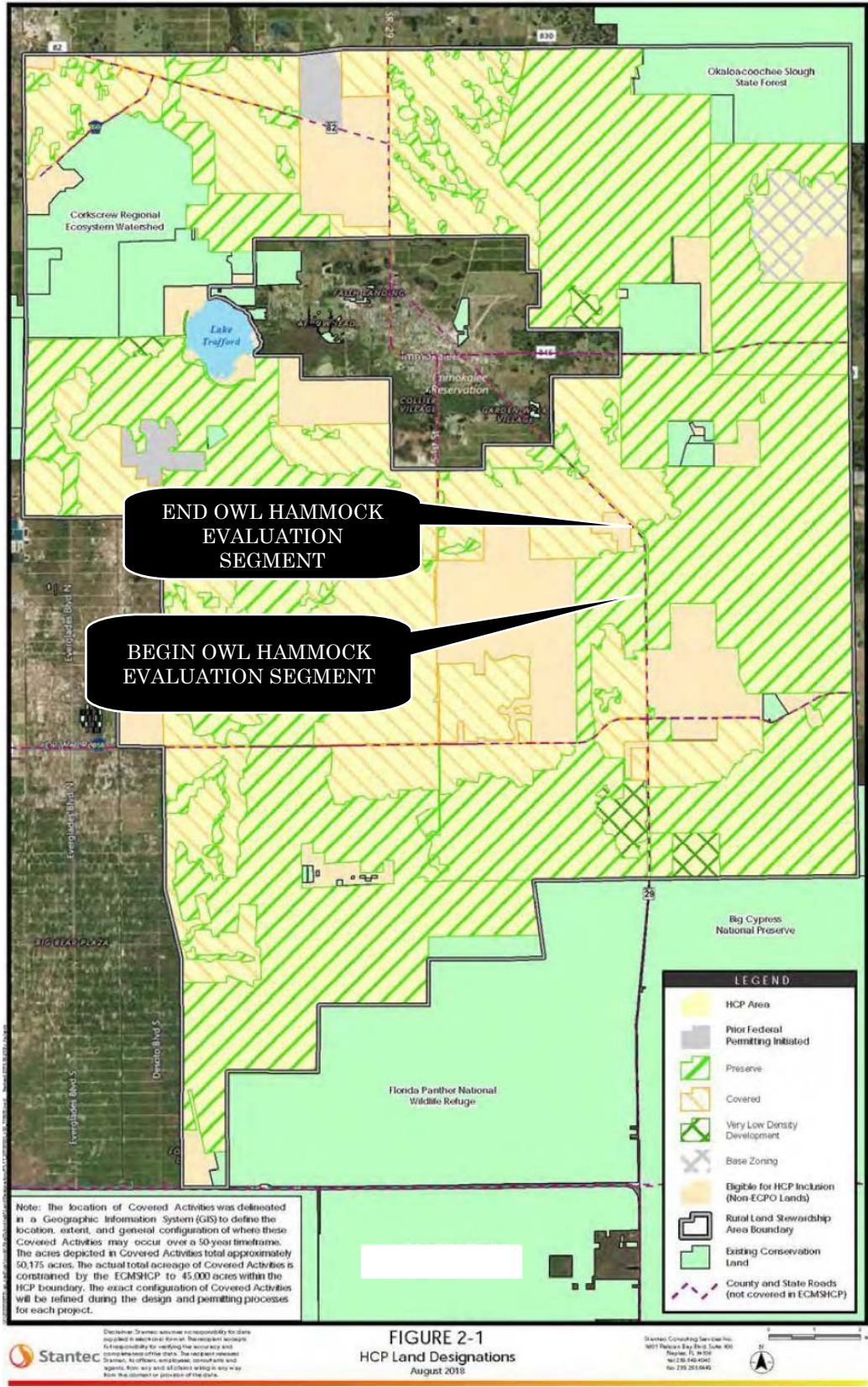


Figure 3-2: HCP Land Designations

3.3 Wildlife Movement

Wildlife cameras were placed at driveways 1 and 3, which cross the Barron Canal. Wildlife cameras were placed on July 15, 2021, and were collected on September 18, 2021. Neither of these cameras collected photographs of wildlife using these driveways during this limited survey period.

A review of available wildlife usage within the evaluation segment was conducted. This dataset includes Florida panther (*Puma concolor coryi*) and Florida black bear (*Ursus americanus floridanus*) radio-telemetry data collected between February 1981-June 2020. **Figure 3-3** shows the telemetry data collected in the vicinity of Owl Hammock.

Table 3-2 shows the collared panthers which utilized the area surrounding Owl Hammock and approximate dates they were in the area.

Table 3-2: Panther Telemetry in Owl Hammock

Panther Number	Approximate date of activity
FP011	1999
FP020	1987
FP031	1993-1994
FP046	1993
FP048	2006
FP052	1993-1994
FP058	1996
FP059	2000-2001
FP065	2002
FP097	2001
FP131	2004-2006
FP135	2006
FP143	2007
FP154	2007
FP185	2011



Figure 3-3: Panther and Black Bear Telemetry

Of particular note was the movement of FP131. Between March 31, 2004, and June 17, 2005, FP131 telemetry data was collected 16 times within two miles of Owl Hammock. When evaluating the timestamps for this telemetry data, FP131 crossed SR 29 at least six times. **Figure 3-4** shows the telemetry data for FP131 near Owl Hammock.

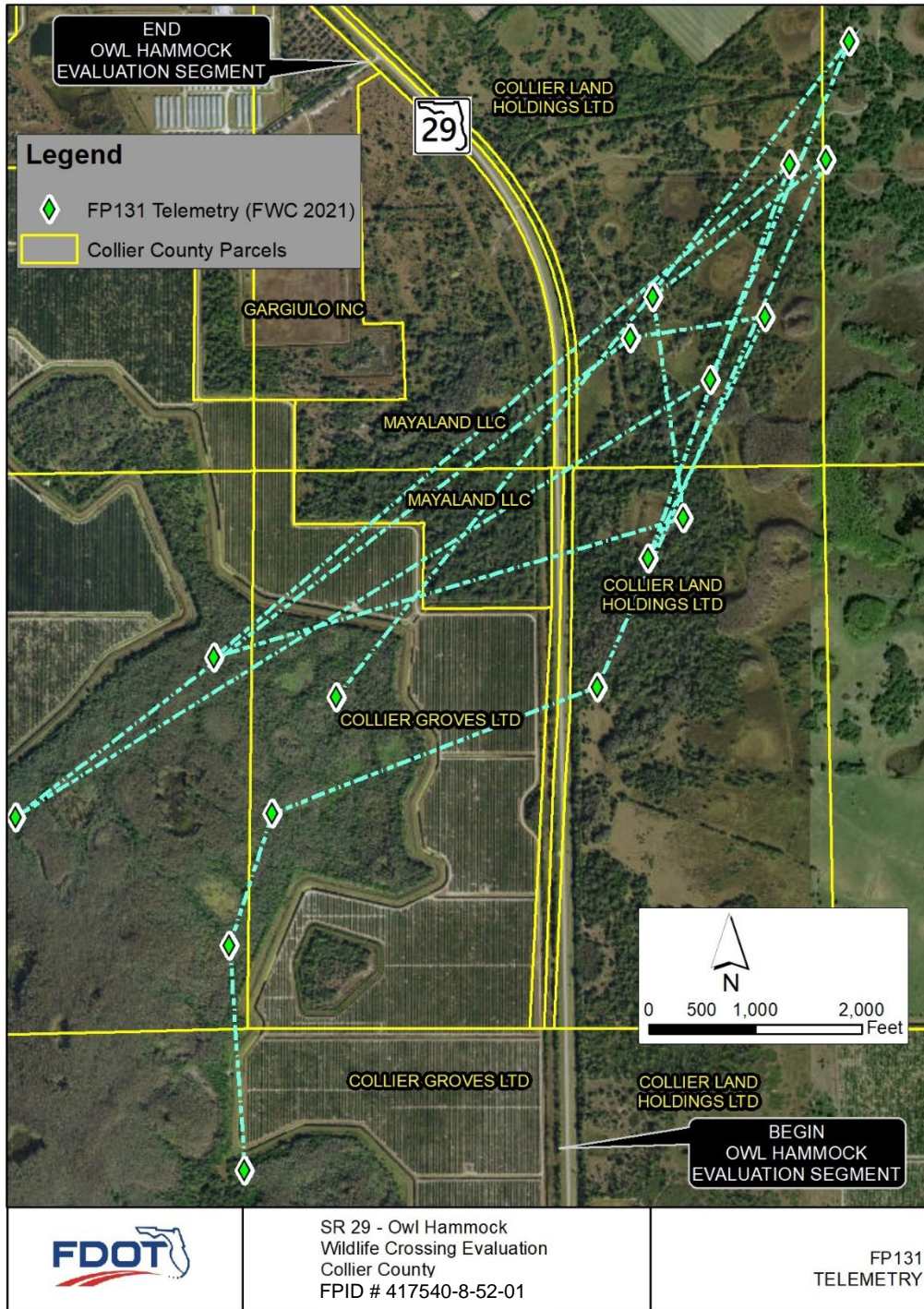


Figure 3-4: Panther FP131 Telemetry

3.4 Wildlife Mortality

Reviewing FWC wildlife mortality data shows that there have been nine Florida panther vehicle collisions in the vicinity of Owl Hammock between 2004 and 2019. There have been four Florida black bear vehicle collisions in the vicinity of Owl Hammock. Near Owl Hammock the horizontal curve of SR 29 is likely a contributing factor to the number of wildlife vehicle collisions. This horizontal curve limits driver visibility. This evaluation segment is in alignment with two Panther Collision Hot Spots. The Hot Spots and wildlife collision data is shown on **Figure 3-5**.

Attachment A

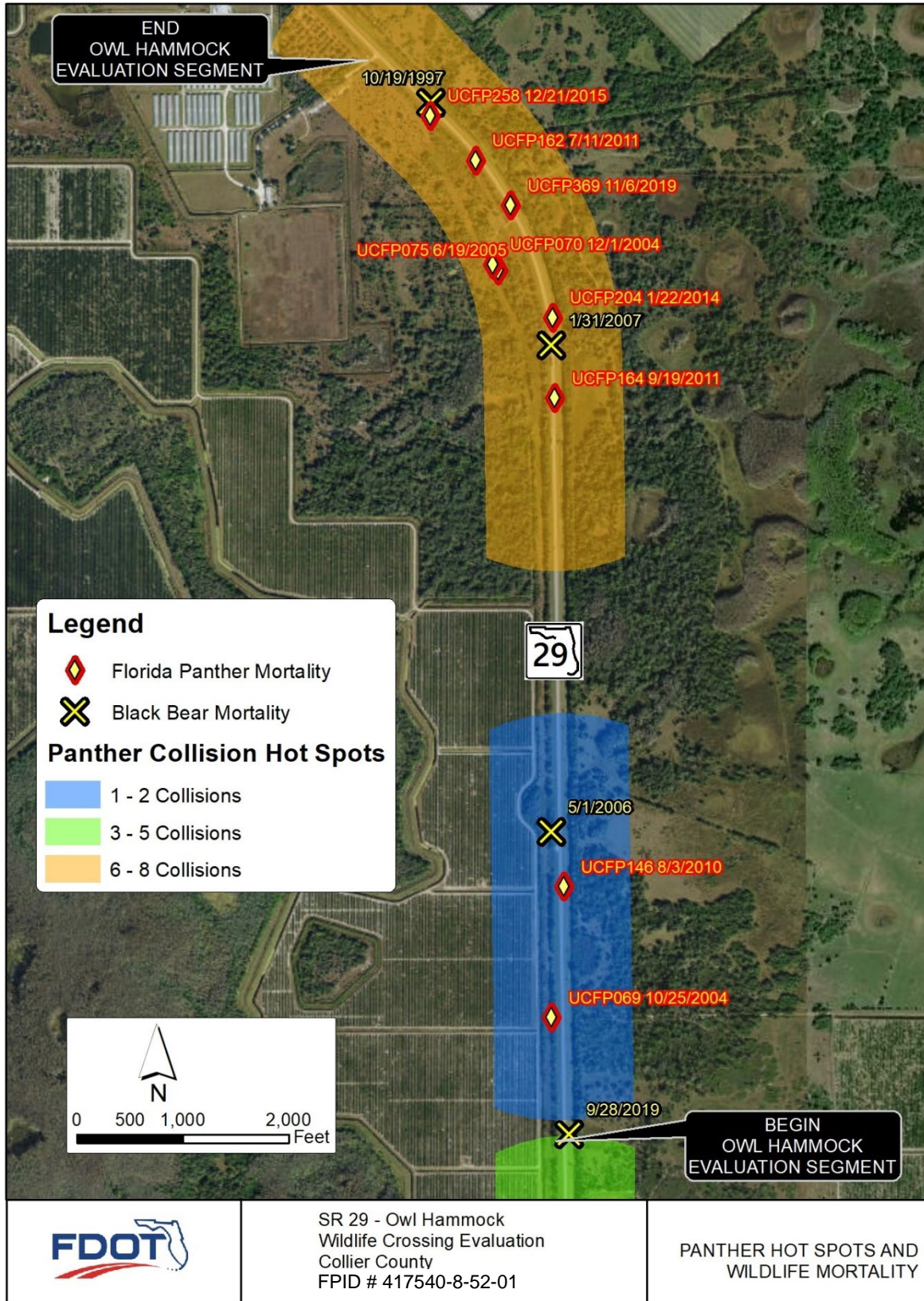


Figure 3-5: Panther Hot Spots and Wildlife Mortality

SR 29 from Oil Well Road to Sunniland Nursery Way
 FPID #: 417540-8-52-01

4.0 Crossing Alternatives

The following sections discuss the feasible wildlife crossings developed in Owl Hammock. Alignments for each option were set with consideration of the existing right-of-way and constructability. The alignments allow for the maintenance of traffic on the existing lanes and minimize the need for extensive traffic control measures and temporary diversions. Horizontal alignment shifts for all options are based on reverse curves with normal crown cross slope for a 65 mph design speed.

The interim construction of any of these wildlife crossing do not preclude the construction of the ultimate four-lane SR 29 typical section, however, minor modifications may be required at the wildlife crossings to accommodate the ultimate typical section. Plan sheets showing details of each of the alternatives evaluated are included in **Attachment A**.

Cross section views of each culvert option show a 72" pipe, which allows flexibility with slope of the culvert to match existing ground. A 10' x 6' box can also be utilized with minor modifications to the cross section and vertical alignment. For this evaluation, the culverts were all placed in uplands with an invert elevation located at least one-half foot above seasonal high water elevation.

All alternatives include wildlife fencing for the entire 2.05-mile length. FDOT wildlife crossing guidelines recommend providing adequate fencing to guide wildlife for a sufficient distance to the wildlife crossing feature. Type B fence, 10 feet in height with three-strand barbed wire, in the Standard Plans Index 550-002 is recommended. **Figure 4-1** shows the alternatives evaluated for Owl Hammock, including the limits of wildlife fencing.

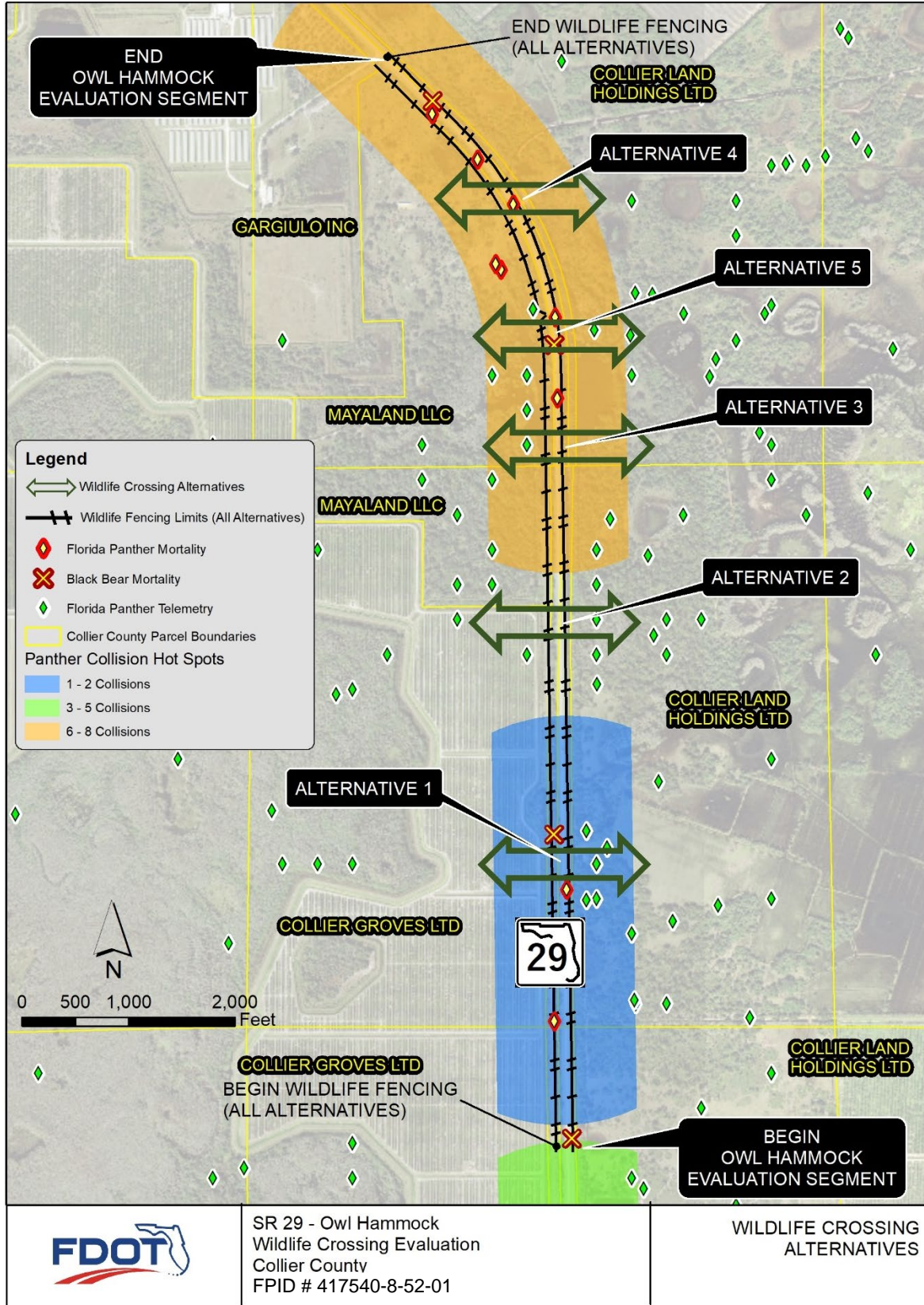


Figure 4-1: Wildlife Crossing Alternatives

4.1 Alternative 1

Alternative 1 is the southernmost alternative evaluated for this segment. Alternative 1 is located within a blue Panther Collision Hot Spot representing two independent panther vehicle collisions. Alternative 1 utilizes a 10-foot x 6-foot box culvert or a 72" pipe placed on western side of SR 29. To accommodate a vertical clearance of 6 feet, the existing SR 29 roadway profile would have to be raised approximately 8 feet at this location (**Figure 4-2**). The vertical alignment is based on maintaining a minimum two feet of cover from the top of the culvert to the bottom of the proposed pavement base.

The cross section at this location includes a shifted two-lane section, with two 12-foot lanes, 8-foot paved shoulders with shoulder gutter and guardrail. These lanes can be utilized as the southbound lanes in the ultimate four-lane condition. MSE wall will be required on the southbound outside shoulders in the ultimate four-lane section. Shoulder gutter and guardrail is utilized to ensure the new alignment ties down within the existing right of way (R/W). Temporary barrier will be required along the west side of the existing SR 29 lanes during construction, and minimal temporary overbuild may be required on the existing northbound shoulder.

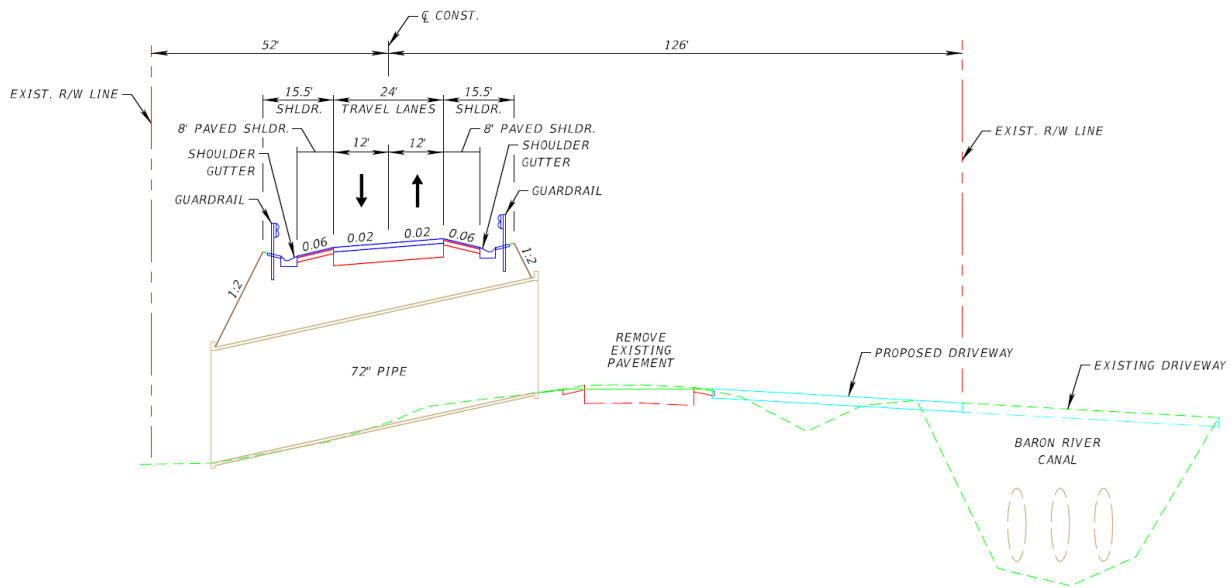


Figure 4-2: Alternative 1 Typical

Alternative 1 would require the extension of a driveway to provide access to land owned by Collier Holdings Ltd. Maintenance of the existing Collier Holdings driveway will require construction of a long frontage drive adjacent to the SR 29 mainline. The connection of this driveway frontage at each end of the new alignment will be challenging for entering and exiting vehicles due to the limited right-of-way available. This driveway connection constrains this alternative. As shown in **Figure 4-3**, wildlife can utilize the existing driveway connection for Collier Holdings Ltd. over the Barron River Canal, eliminating the need for a new bridge, however wildlife gates would be required to channelize wildlife to the proposed box/pipe. An unpaved driveway on the west side of SR 29 that provides access to the powerline easement would also require relocation to the south.

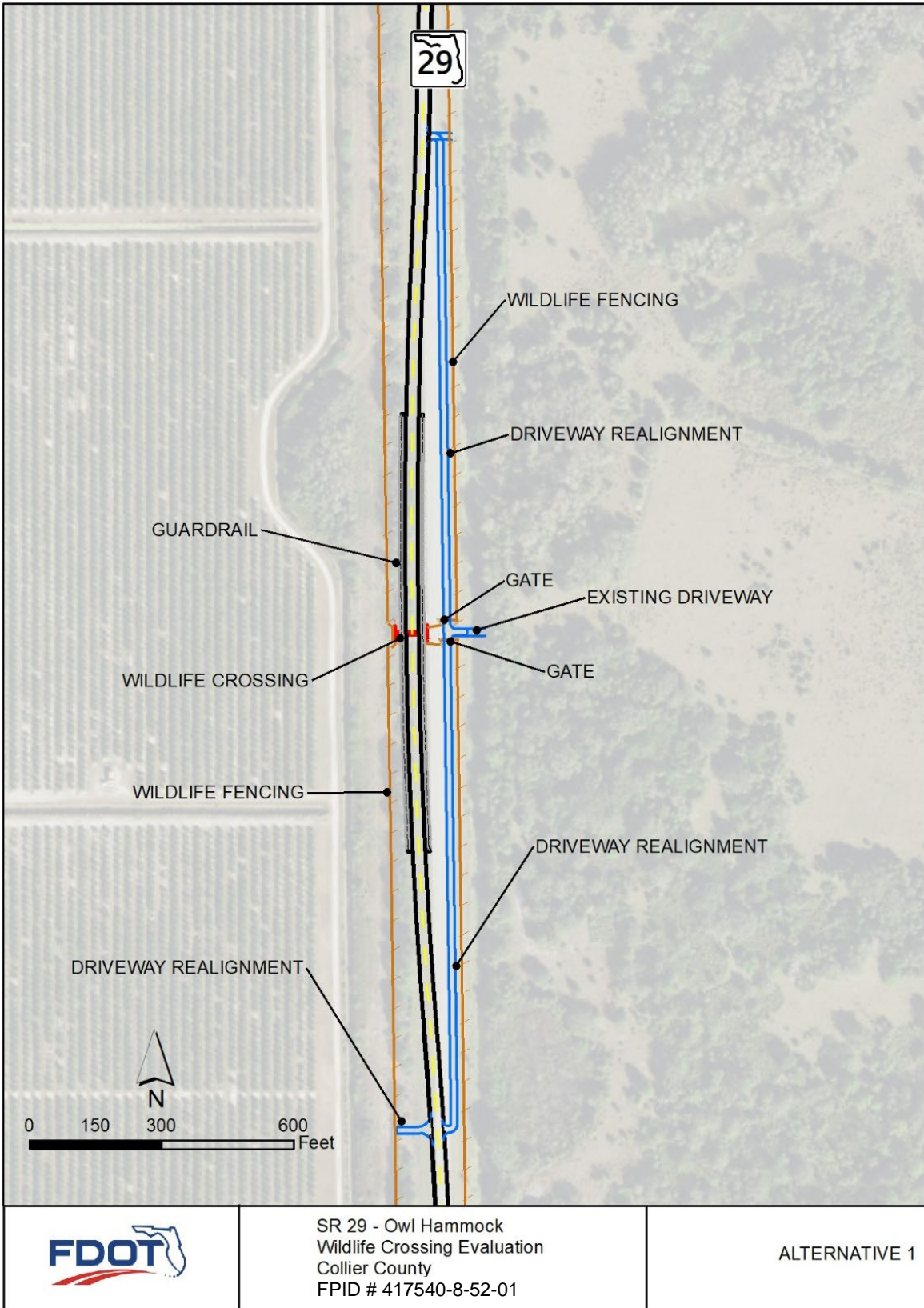


Figure 4-3: Alternative 1

This alternative is not anticipated to affect wetlands or surface waters. As this alternative does not affect wetlands, no 404 permit from FDEP is anticipated to be required.

It is anticipated that Option 1 would require stormwater treatment and attenuation due to the additional impervious area associated with driveway construction. However, since the Barron River Canal parallels every option, treatment could be provided at any location along this section of SR 29 for any combination of existing and/or proposed pavement required to provide treatment for an area equal to the additional impervious. Using 1.57 acres of additional impervious and a presumptive treatment depth of 2.5 inches as required by the SFWMD a total treatment volume of approximately 0.33 acre-feet or 14,400 cubic feet of treatment volume is required. Note that 1.57 acres is the maximum delta between the pre and post impervious area for all alternatives. Further, this delta could be reduced and additional storage volume gained through the removal of existing driveways that were previously culverted or bisected the existing ditch Owl Hammock is within WBID 3278W which is impaired for iron and nutrient removal calculations should not be required, but the SFWMD may request these calculations during the permitting phase.

The area between the Barron River Canal and SR 29 is the most logical location for a linear extended detention system. Linear extended detention is allowed by SFWMD and does not rely on percolation into the soil, but rather includes an outfall control structure with a bleed-down weir that can discharge or recover the treatment volume in as little as 24 hours. This provides the advantage of minimizing any impact to the roadway base.

Assuming an available width of approximately 30 feet between the edge of travel and the guardrail adjacent to the Barron River Canal as well as a flat width of 15 feet and a storage depth of 0.75 feet, a swale approximately 1300 feet long would be required to provide the necessary treatment volume. With respect to attenuation, 0.30 acre-feet of volumetric storage is the maximum volume required to provide the necessary attenuation for any given option. If the required attenuation volume is allowed to exist coincidentally with the treatment volume, then attenuation could be provided in the same swale that provides the treatment volume. As the Barron Canal is the common outfall for all alternatives and because the water management district will allow treatment of existing pavement in lieu of

new pavement to satisfy the regulatory requirements, this treatment approach could be applied at all alternative locations.

Alternative 1 is not anticipated to affect wetlands or surface waters.

4.2 Alternative 2

Alternative 2 is located approximately 2,300 feet north of Alternative 1. Alternative 2 is not located within a Panther Collision Hot Spot. Reviewing panther telemetry data, Alternative 2 is located within an area of likely panther activity. As this crossing is within a tangent section of SR 29, vehicle operators may be able to observe panthers to avoid collisions.

The crossing at Alternative 2 includes a new alignment west of the existing pavement. The horizontal location is based on the ensuring that the embankment approaching and departing the wildlife culvert can be constructed within the existing right-of-way (**Figure 4-4**). The resulting typical section consists of two 12-foot lanes with eight-foot shoulder pavement, shoulder gutter, and guardrail. The shoulder gutter and guardrail allow the new alignment to be constructed while maintaining traffic on the existing pavement. Similar to Alternative 1, the existing SR 29 roadway profile would have to be raised approximately 8 feet at this location (**Figure 4-5**). The vertical alignment is based on maintaining a minimum two feet of cover from the top of the culvert to the bottom of the proposed pavement base. Temporary barrier will be required along the west side of the existing SR 29 lanes, and minimal temporary overbuild may be required on the existing northbound shoulder.

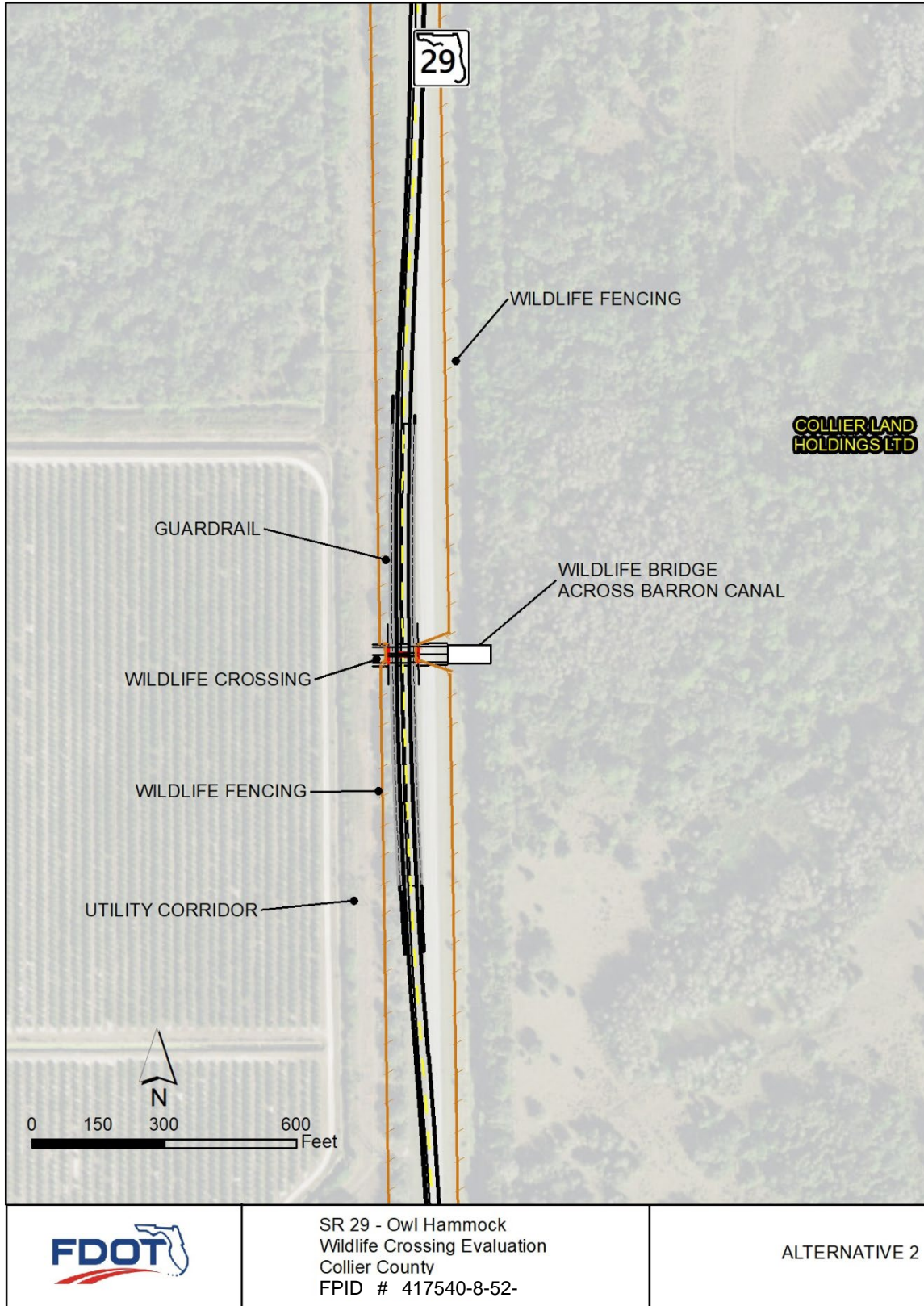


Figure 4-4: Alternative 2

Alternative 2 would require an additional wildlife bridge for wildlife to cross the Barron Canal (Figure 4-5). Alternative 2 evaluates the use of 30” prestressed concrete piles to clear the width of the canal. The piles would be placed side-by-side horizontally to create a 5 ft. walking surface to cross the canal. The use of concrete end blocks would be employed at the ends of the piles with slope protection to prevent any future bank erosion at the structure location. As shown in Figure 4-5, placement of this wildlife bridge across the Barron Canal will require additional right-of-way. In the vicinity of Owl Hammock, The Barron Collier Canal is located within privately-owned lands, but Collier County routinely conducts maintenance of the canal. The existing pavement will be removed and the area on the east side of the new culvert will be graded to meet the wildlife bridge.

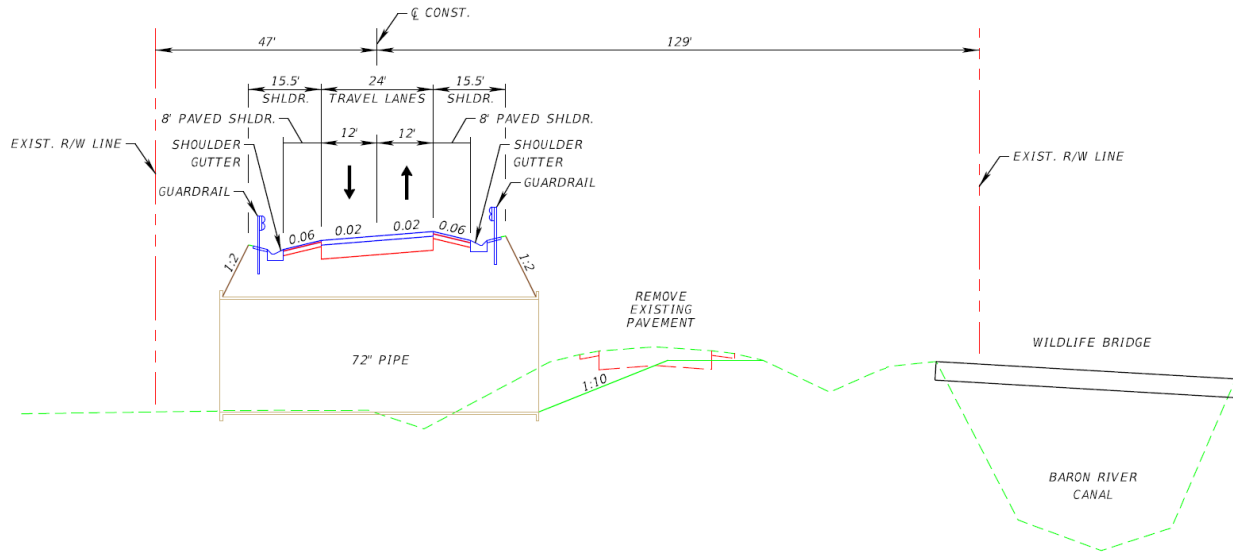


Figure 4-5: Alternative 2 Typical

Stormwater treatment and attenuation is not anticipated to be required for Alternative 2. This alternative includes improvements over the Barron Canal. Statute 62-330.439 provides criteria for issuance of a General Permit from the SFWMD for Construction or Maintenance of Culverted Driveway or Roadway Crossings, and Bridges of Artificial Waterways. As this alternative includes a bridge over the Barron Canal, this project will likely qualify for General Permit 62-331.217 from the FDEP.

As Alternative 2 includes a wildlife crossing over the Barron Canal, minor impacts to surface waters are anticipated. Alternative 2 is not anticipated to affect wetlands.

4.3 Alternative 3

Alternative 3 includes the replacement of Bridge No. 030303 over Gator Slough. As shown in Photo 2, during the wet season, Bridge No. 030303 does not have adequate vertical clearance to accommodate wildlife shelves. The existing SR 29 over Canal 303 bridge (Bridge No. 030303) will need to be replaced with a reinforced flat slab bridge. The new bridge geometry accommodates a ten-foot-wide shelf on the south side of the canal above the seasonal high water (SHW) elevation acting as the pathway for wildlife (**Figure 4-6**).

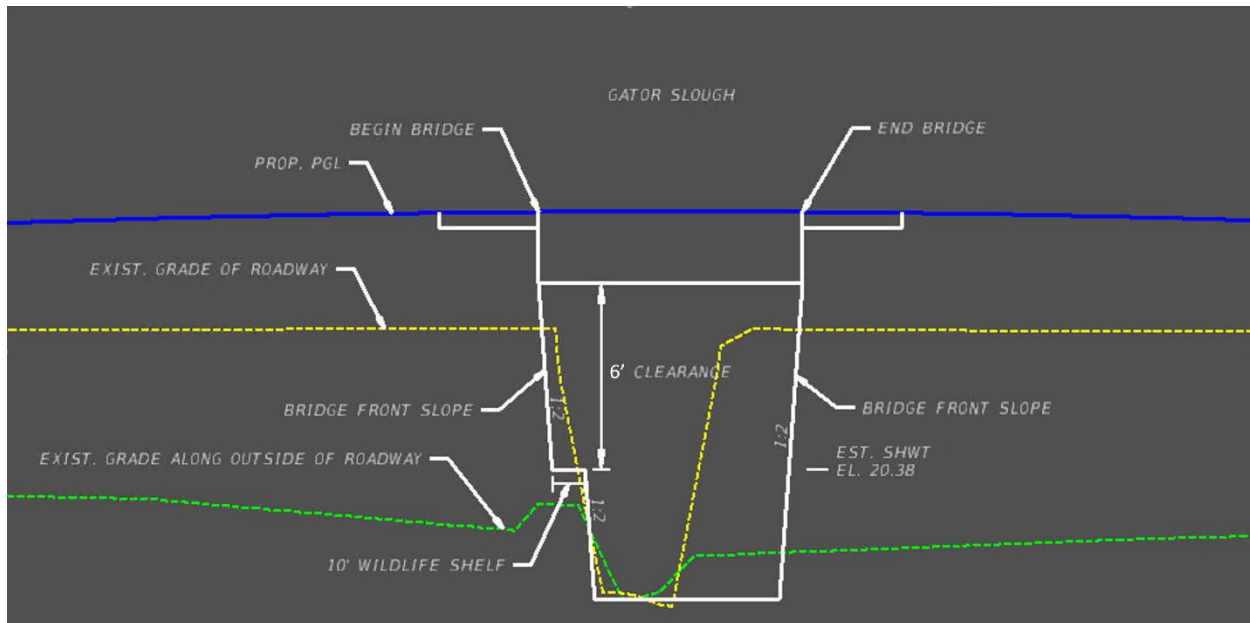


Figure 4-6: Gator Slough Cross Section

The Gator Slough bridge replacement concept alignment is located west of the existing alignment and bridge, with the horizontal offset to the new bridge set by the required embankment to meet the elevated bridge structure. The roadway typical section consists of two 12-foot lanes with eight-foot shoulder pavement, shoulder gutter, and guardrail leading to the bridge. The shoulder gutter and guardrail allow the new alignment to be constructed while maintaining traffic on the existing pavement (**Figure 4-7**). The bridge typical section consists of two 12-foot lanes with 10-foot shoulders. Temporary barrier will be required for construction, and minimal temporary overbuild will be required on the existing northbound shoulder.

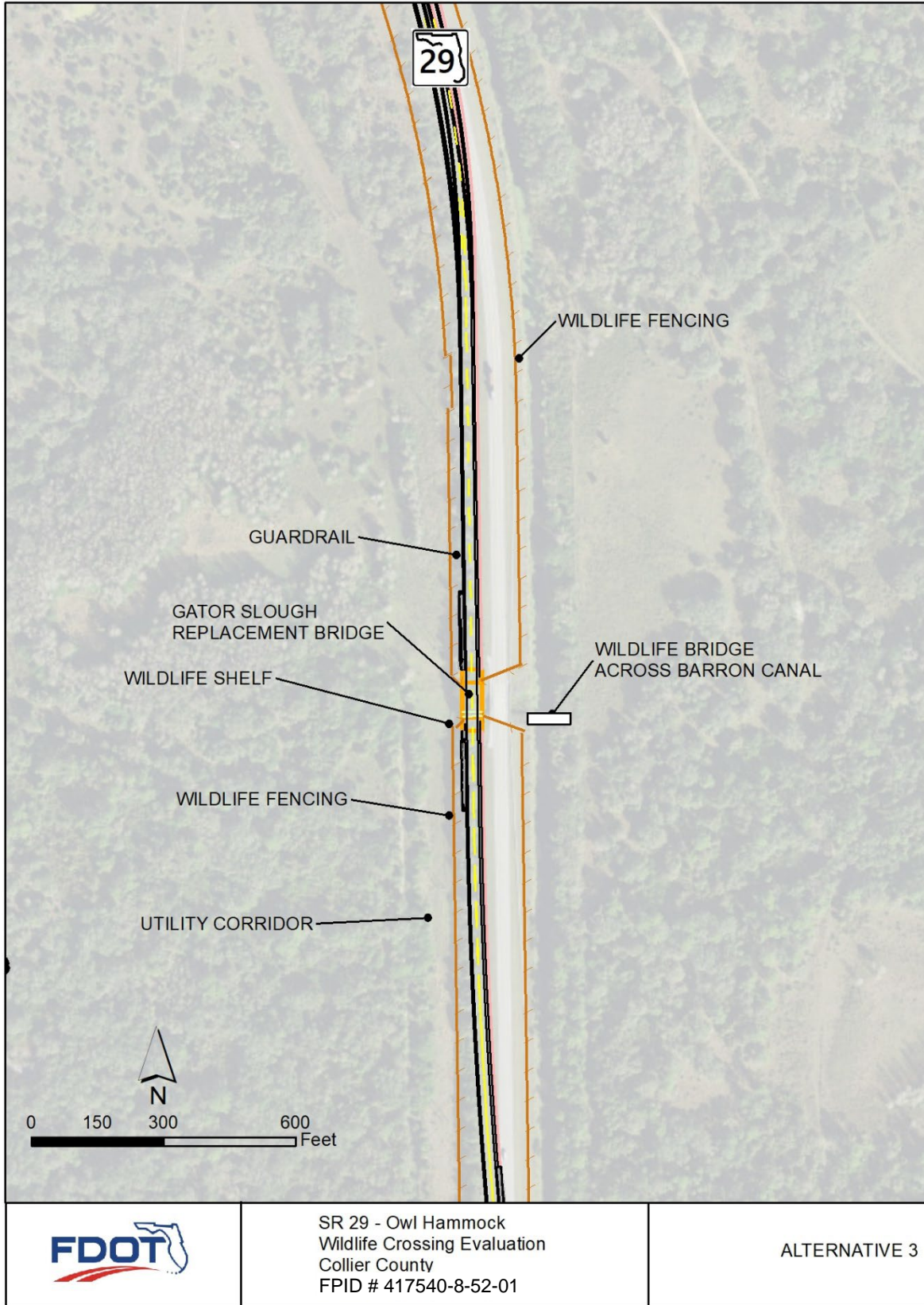


Figure 4-7: Alternative 3

The Alternative 3 vertical alignment is based on maintaining six feet minimum clearance from the proposed wildlife shelf (set at the approximate high-water elevation – estimated elevation 20.38) to the low member of the bridge (**Figure 4-8**). This results in a new bridge approximately three feet higher than the existing bridge. A wildlife concrete canal bridge over the Barron River Canal is also required.

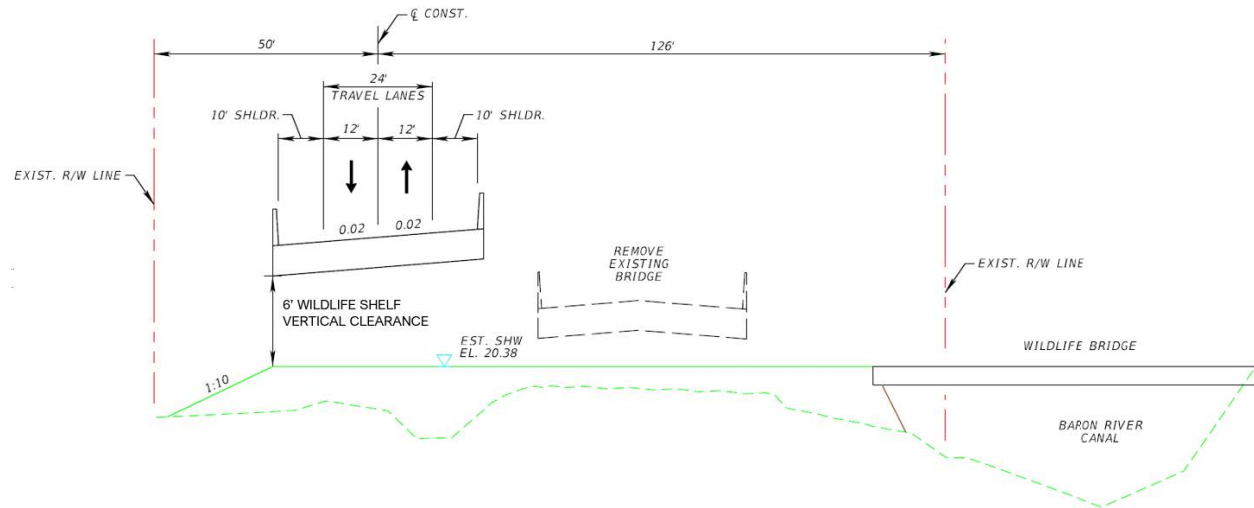


Figure 4-8: Alternative 3 Typical

Similar to Alternative 2, Alternative 3 would require an additional bridge for wildlife to cross the Barron Canal. Alternative 2 evaluates the use of 30” prestressed concrete piles to clear the width of the canal. The piles would be placed side-by-side horizontally to create a 5 ft. walking surface to cross the canal. The use of concrete end blocks would be employed at the ends of the piles with slope protection to prevent any future bank erosion at the structure location. As this alternative is located within the floodplain of Gator Slough, it is anticipated that high water during wet season will significantly limit wildlife usage at this crossing.

It is anticipated that this alternative will require an individual ERP from the SFWMD. As this alternative includes a bridge over the Barron Canal, this alternative will likely qualify for General Permit 62-331.217 from the FDEP. Temporary and permanent wetland impacts are anticipated for Alternative 3. Wetland mitigation is likely to be required for this alternative to mitigate for unavoidable wetland impacts.

4.4 Alternative 4

Alternative 4 is the northernmost alternative evaluated for this segment. Alternative 4 is located within an orange Panther Collision Hot Spot, representing five Florida panther vehicle collisions. Alternative 4 is located within the horizontal curve south of Sunniland Nursery Road at the existing Collier Holdings driveway. The cross section at this location includes a shifted two-lane section, with two 12-foot lanes 8-foot paved shoulders with shoulder gutter and guardrail (**Figure 4-9**). These lanes can be utilized as the northbound lanes in the ultimate four-lane condition. MSE wall will be required on the northbound outside shoulder in the ultimate four-lane section.

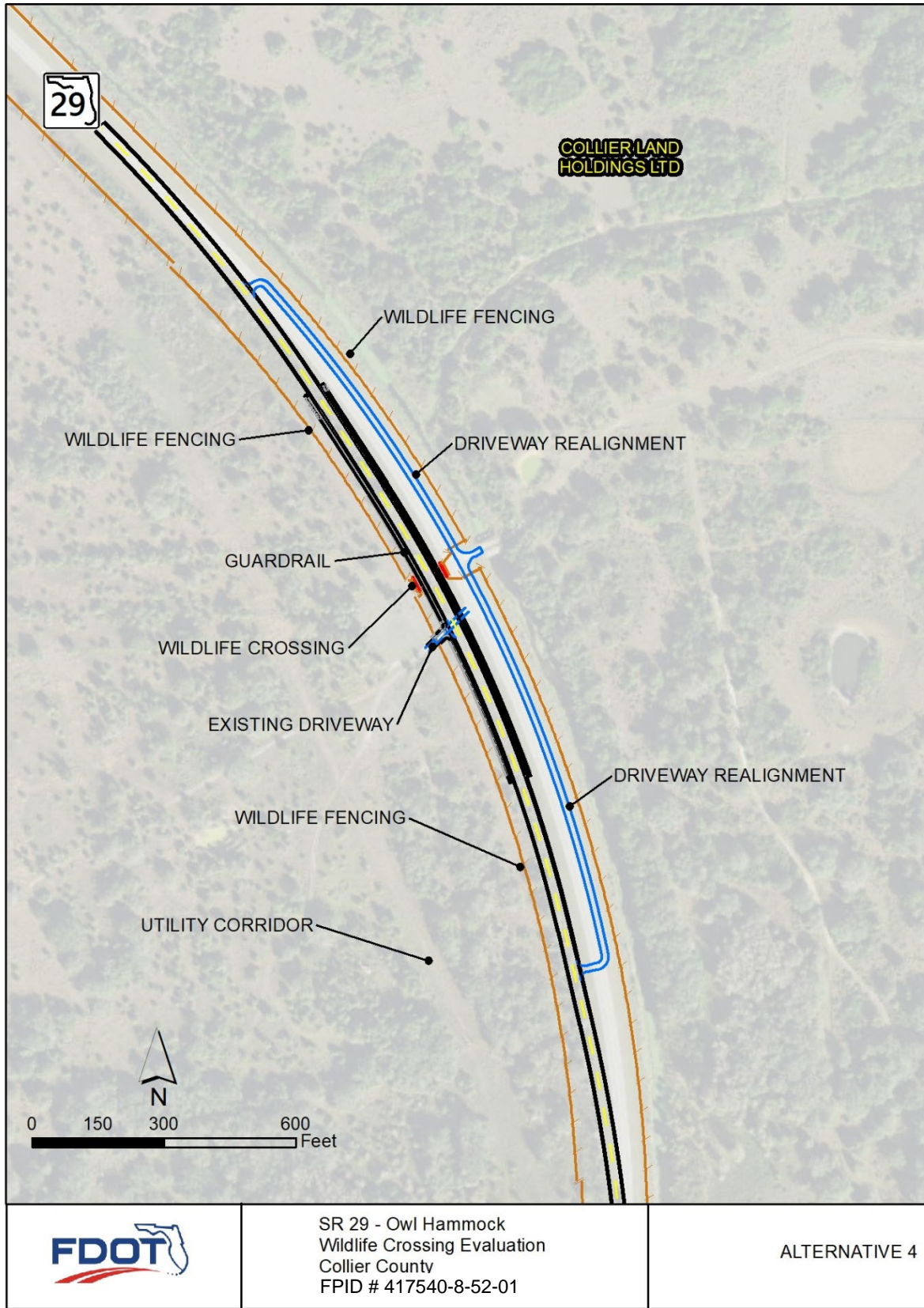


Figure 4-9: Alternative 4

Alternative 4 utilizes a 10-foot x 6-foot box culvert placed on western side of SR 29. To accommodate a vertical clearance of 6 feet, the existing SR 29 roadway profile would have to be raised approximately 8 feet. at this location (**Figure 4-10**). Shoulder gutter and guardrail is utilized to ensure the new alignment ties down within the existing R/W. The existing travel lanes would then be shifted to the west.

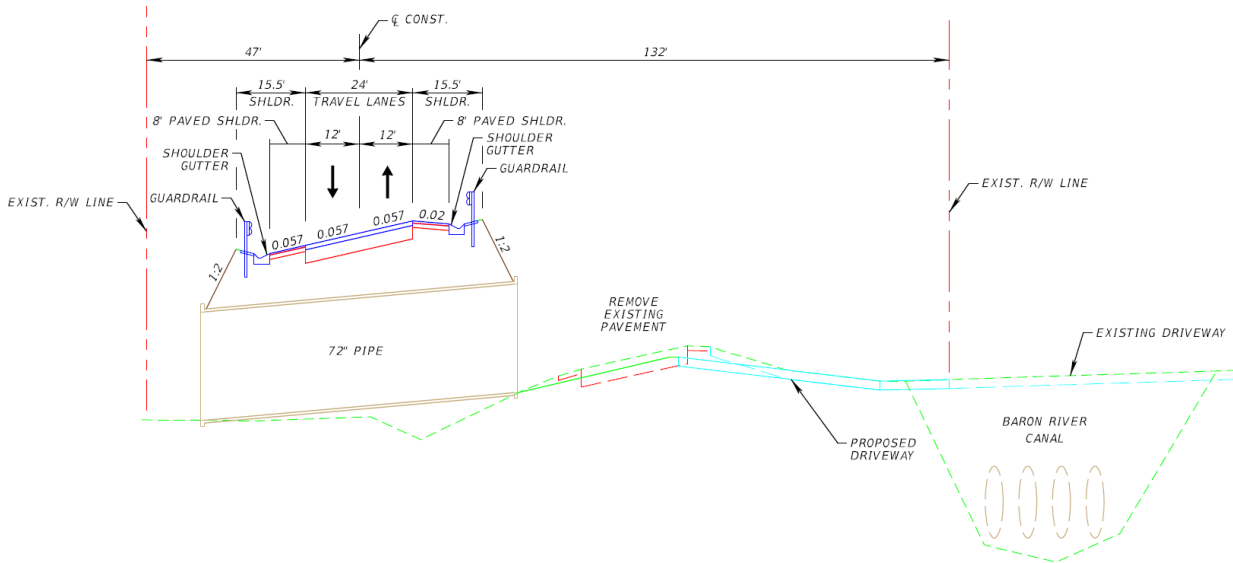


Figure 4-10: Alternative 4 Typical

Like Alternative 1, maintenance of the existing Collier Holdings driveway will require construction of a long frontage drive adjacent to the SR 29 mainline. The connection of this driveway frontage at each end of the new alignment will be challenging for entering and existing vehicles due to the limited right-of-way available and sight distance limitations around the curve and elevated alignment. This driveway connection constrains this alternative. Wildlife can utilize the existing driveway connection for Collier Holdings Ltd. over the Barron River Canal, eliminating the need for a new bridge, however wildlife gates would be required to channelize wildlife to the proposed box/pipe. The Mayaland LLC driveway on the west side of SR 29 will be adjusted to tie to the new alignment vertically.

This alternative is not anticipated to affect wetlands or surface waters. As this alternative does not affect wetlands, no 404 permit from FDEP is anticipated to be required.

It is anticipated that Alternative 4 would require stormwater treatment and attenuation due to the additional impervious area associated with driveway construction. However,

since the Barron River Canal parallels every option, treatment could be provided at any location along this section of SR 29 for any combination of existing and/or proposed pavement required to provide treatment for an area equal to the additional impervious. Using 1.57 acres of additional impervious and a presumptive treatment depth of 2.5 inches as required by the SFWMD a total treatment volume of approximately 0.33 acre-feet or 14,400 cubic feet of treatment volume is required. Note that 1.57 acres is the maximum delta between the pre and post impervious area for all alternatives. Owl Hammock is within WBID 3278W which is impaired for iron and nutrient removal calculations should not be required, but the SFWMD may request these calculations during the permitting phase.

4.5 Alternative 5

Alternatives 1 through 4 provide a crossing on a new adjacent alignment, allowing for the maintenance of traffic on the existing lanes during construction and minimizing the need for extensive traffic control measures and temporary diversions. This is a conservative estimate for the purpose of alternatives analysis, with the horizontal and vertical geometrics dictating where the crossing can be placed. All options could be constructed on the existing alignment, which would allow some additional flexibility of location options since locations would only be dictated by less restrictive temporary traffic control alignments, not permanent design criteria. As Alternative 4 is located within one-quarter mile of five fatal panther vehicle collisions, this location would be a primary location for placement of a wildlife crossing, based on wildlife connectivity. Due to the existing horizontal curve of SR 29 and driveway connections required within this curve, Alternative 4 is not considered the preferred option based on roadway safety considerations.

Alternative 5 was added to provide an additional viable alternative within the orange Panther Hot Spot. Alternative 5 is similar to Alternative 2, utilizing a similar typical section (**Figure 4-11**) with two 12-foot lanes with eight-foot shoulder pavement, shoulder gutter, and guardrail and a new bridge over the Barron River Canal. However, in order to place this option north of the existing bridge at Gator Slough and south of the existing horizontal curve, it is necessary to place the new crossing approximately on the existing alignment. As shown in **Figure 4-12**, the location of Alternative 5, 1200 feet north of Gator

Slough, is based on the vertical alignment to meet the approximate eight-foot elevation change required.

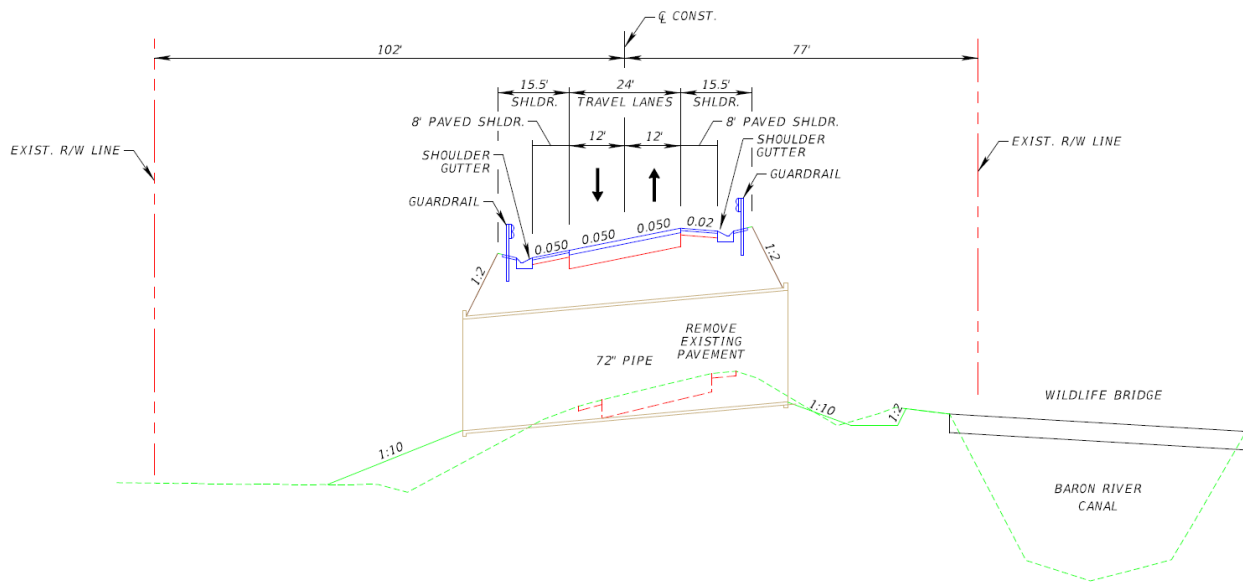


Figure 4-11: Alternative 5 Typical Section

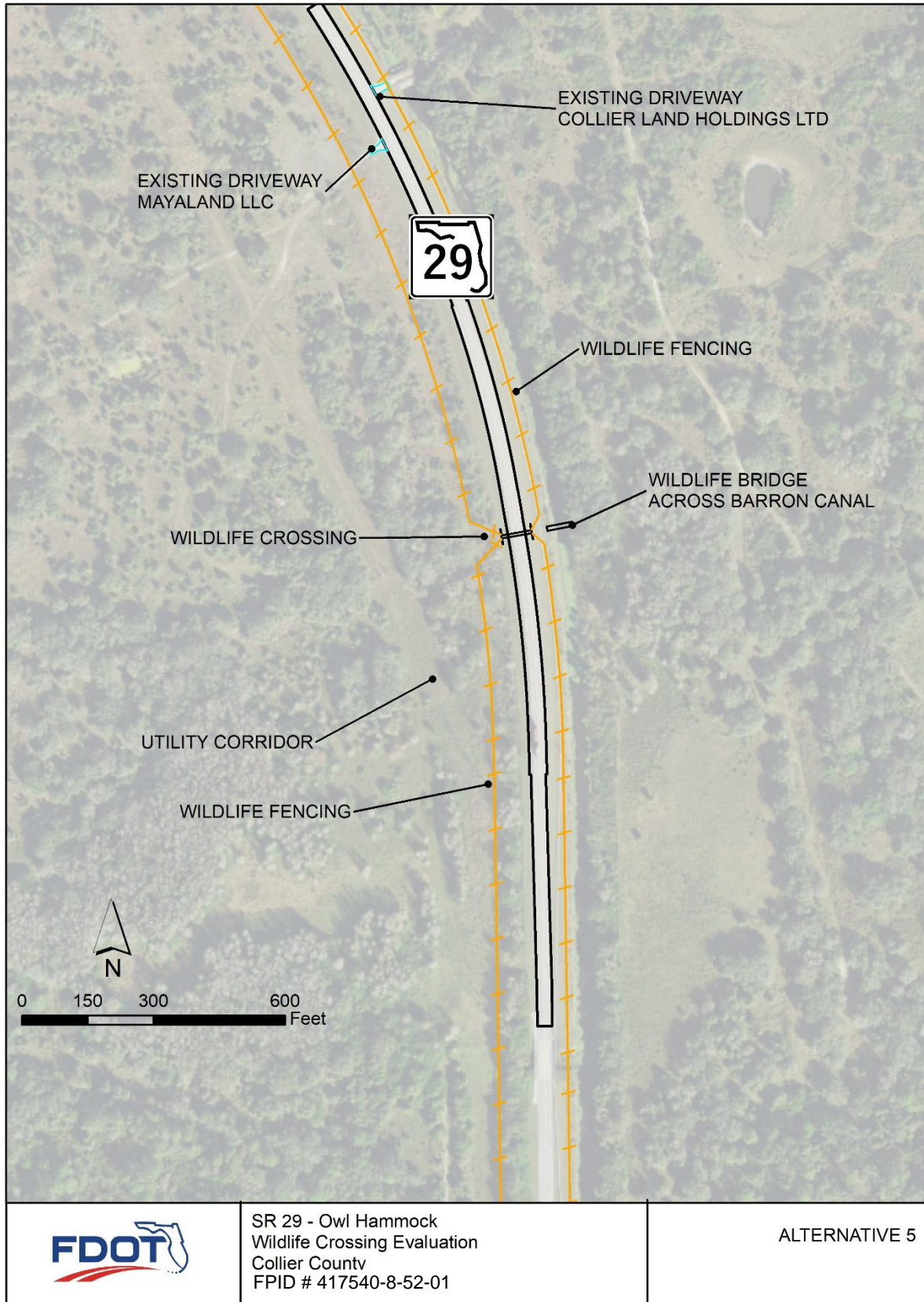


Figure 4-12: Alternative 5

5.0 Cost Estimates and Recommendations

Preliminary cost estimates for each alternative were based on FDOT statewide average unit costs. There has been no value engineering completed in evaluating these alternatives, so that each estimate can be reasonably compared to other alternatives. Alternatives 1, 2, and 4 could be constructed on the existing alignment, maintaining traffic during construction on temporary alignments to reduce costs.

Due to wildlife utilization, wildlife fencing is proposed for the entire Owl Hammock wildlife crossing evaluation segment. As shown on **Figure 4-1**, All alternatives include the same length of fencing, but the number of gates required vary by alternative. Each alternative utilizing a box culvert also includes an estimate for utilizing a 72" pipe culvert. A detailed cost estimate for each alternative is included as **Attachment 2**.

5.1 Alternative 1

Alternative 1 includes a box culvert or pipe and a driveway extension. The preliminary cost estimate for the box culvert is \$3,647,700.49. The preliminary cost estimate for the pipe culvert is \$3,553,103.84. Due to undesirable driveway extensions, this alternative is not recommended for further evaluation.

5.2 Alternative 2

Alternative 2 includes a box culvert or pipe and a wildlife bridge over the Barron Canal. Alternative 2 does not require any driveway modification. The preliminary cost estimate for the box culvert is \$3,262,746.64. The preliminary cost estimate for the pipe culvert is \$3,171,994.67. Although Alternative 2 is not located within a Panther Hot Spot, the addition of 2.05 miles of wildlife fencing is anticipated to channelize wildlife to this crossing. Alternative 2 would provide a viable pathway for wildlife to traverse the SR 29 corridor at Owl Hammock.

5.3 Alternative 3

Alternative 3 includes the replacement of the bridge over Gator Slough and an additional wildlife bridge over the Barron Canal. The preliminary cost estimate for Alternative 3 is

\$6,725,882.64. Due to the highest preliminary cost and reduced wildlife usage due to high seasonal high water, this alternative is not recommended for further evaluation.

5.4 Alternative 4

Alternative 4 includes a box culvert or pipe and a driveway extension. The preliminary cost estimate for the box culvert is \$3,405,007.12. The preliminary cost estimate for the pipe culvert is \$ \$3,311,479.11. As this alternative is located within a horizontal curve and requires significant driveway modifications, this alternative is not recommended for further evaluation.

5.5 Alternative 5

Alternative 5, a modified Alternative 2, can be located within the orange Panther Hot Spot (**Figure 4-1**) if placed on the existing alignment. Placing Alternative 5 approximately 1,200 feet north of Gator Slough aligns more closely with recent panther and black bear vehicle collisions. Due to the proximity to the Gator Slough bridge, a crossing at this location would need to be placed on the existing alignment. The geometric requirements for shifting the alignment temporarily to the west while constructing on the existing alignment are not as strict as a permanent shift, allowing the crossing to be located at the south end of the existing horizontal curve, without the need to relocate, or adjust existing driveway connections. Placing the crossing on the existing alignment requires the use of temporary pavement during construction. The preliminary cost estimate for the box culvert is \$3,277,118.34. The preliminary cost estimate for the pipe culvert is \$ \$3,186,366.37.

5.6 Preferred Alternative

As outlined above, Alternatives 1, 3, and 4 are not recommended for further evaluation. The preliminary cost estimates of Alternative 2 (\$3,262,746.64) and Alternative 5 (\$3,277,118.34) are within 0.4% of each other. Due proximity of Alternative 5 being located closer to documented wildlife usage, Alternative 5 is the preferred alternative.

6.0 References

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Attachment 1

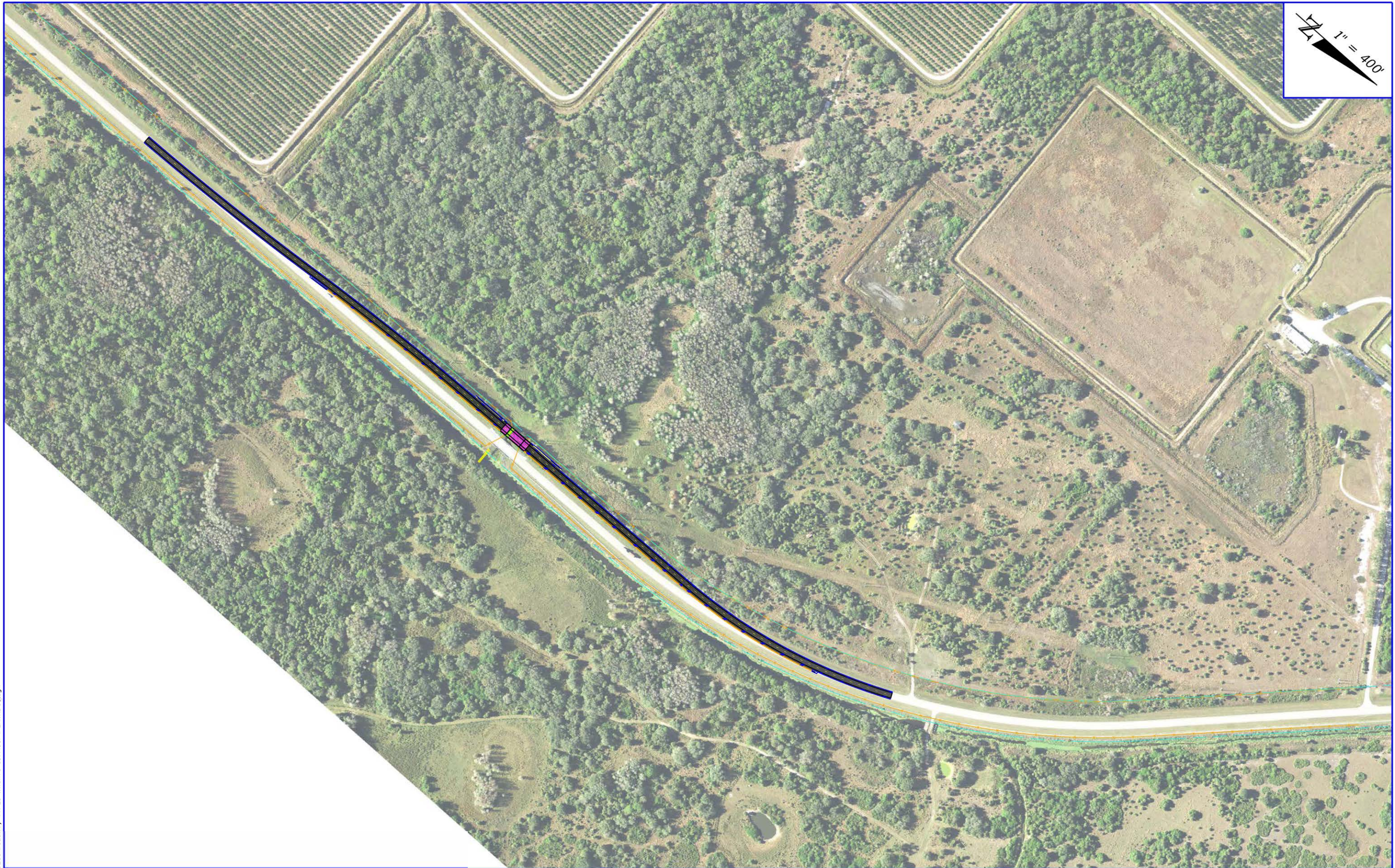
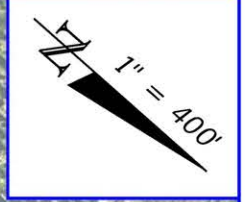
Roadway Plan Sheets

N 1" = 300'



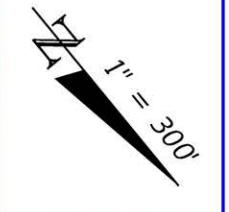
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				SR 29	COLLIER	417540-8-52-01		



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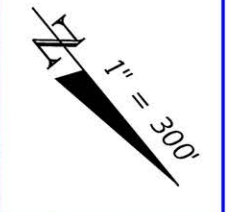
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PLAN SHEET - ALT 4



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DATE	DESCRIPTION	DATE	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		5
				SR 29	COLLIER	417540-8-52-01		

Attachment 2

Detailed Preliminary Cost Estimates

SR 29 WILDLIFE CORRIDOR - OWL HAMMOCK

Cost Options	Alternative 1	Alternative 2	Alternative 3	Alternative 4	Alternative 5
Structures/Bridge*					
Bridge			\$ 1,367,585.00		
Wildlife Bridge	\$ 34,250.00	\$ 34,250.00			\$ 34,250.00
MSE Wall			\$ 508,200.00		
Concrete Barrier			\$ 663,000.00		
Removal of Existing Bridge			\$ 192,000.00		
Construction over water (3%)	\$ 6,862.00	\$ 82,951.00			\$ 6,862.00
Structures Subtotal	\$ -	\$ 41,112.00	\$ 2,847,986.00	\$ -	\$ 41,112.00
Roadway					
Clearing and Grubbing (AC x \$20,865)	\$ 268,696.75	\$ 214,736.22	\$ 268,033.34	\$ 243,405.84	\$ 183,612.00
Embankment (Cubic Yards x \$7.59)	\$ 294,933.49	\$ 160,554.41	\$ 297,127.42	\$ 195,123.38	\$ 159,384.94
Stabilization (Square Yards x \$4.24)	\$ 91,518.49	\$ 76,154.59	\$ 79,417.11	\$ 81,123.92	\$ 56,064.99
Base Course (Square Yards x \$14.78)	\$ 204,666.71	\$ 210,597.92	\$ 246,839.14	\$ 186,410.29	\$ 146,055.96
Asphalt (Tons x \$107.17)	\$ 244,866.82	\$ 251,963.03	\$ 295,322.65	\$ 223,024.52	\$ 174,743.90
Guardrail (Linear Ft x \$16.82)	\$ 33,555.90	\$ 35,322.00	\$ 5,298.30	\$ 30,915.16	\$ 36,768.52
Shoulder Gutter (Linear Ft x \$30.00)	\$ 59,850.00	\$ 63,000.00	\$ 10,770.00	\$ 59,280.00	\$ 65,580.00
Inlets (Each x \$4000.00)	\$ 28,000.00	\$ 28,000.00	\$ 8,000.00	\$ 28,000.00	\$ 32,000.00
*72" Pipe Culvert (Linear Ft x \$1000.00)	\$ 74,000.00	\$ 69,000.00	\$ -	\$ 71,000.00	\$ 69,000.00
Endwall (Cubic Yard x \$1716.57)	\$ 49,780.53	\$ 49,780.53	\$ -	\$ 49,780.53	\$ 49,780.53
Reinforcing Steel (Pounds x \$0.22)	\$ 549.56	\$ 549.56	\$ -	\$ 549.56	\$ 549.56
Driveway Base Course (Square Yards x \$9.21)	\$ 37,777.60	\$ -	\$ -	\$ 30,169.91	\$ 1,324.19
Driveway Asphalt (Tons x \$110.00)	\$ 37,223.86	\$ -	\$ -	\$ 29,727.68	\$ 1,304.78
Gravity Wall (Cubic Yards x \$687.48)	\$ 79,747.68	\$ -	\$ 79,747.68	\$ 79,747.68	\$ -
Wildlife Fencing Cost (Linear Ft x \$60.00)**	\$ 1,267,200.00	\$ 1,267,200.00	\$ 1,267,200.00	\$ 1,267,200.00	\$ 1,267,200.00
Wildlife Gate Cost (Each x \$4000.00)	\$ 12,000.00	\$ -	\$ -	\$ 12,000.00	\$ -
Temporary Barrier Wall Type K (LF x \$9.68)	\$ 33,938.08	\$ 34,716.84	\$ 40,123.60	\$ 30,743.68	\$ 23,454.64
Special Detour (temporary pavement)	\$ -	\$ -	\$ -	\$ -	\$ 206,653.11
Roadway Subtotal	\$ 2,818,305.46	\$ 2,461,575.10	\$ 2,597,879.24	\$ 2,618,202.16	\$ 2,473,477.13
Project Subtotal	\$ 2,818,305.46	\$ 2,502,687.10	\$ 5,445,865.24	\$ 2,618,202.16	\$ 2,514,589.13
MOT (5%)	\$ 140,915.27	\$ 125,134.36	\$ 272,293.26	\$ 130,910.11	\$ 125,729.46
Mobilization (10%)	\$ 281,830.55	\$ 250,268.71	\$ 544,586.52	\$ 261,820.22	\$ 251,458.91
Project Total	\$ 3,241,051.28	\$ 2,878,090.17	\$ 6,262,745.02	\$ 3,010,932.49	\$ 2,891,777.50
Project Unknowns (5%)	\$ 162,052.56	\$ 143,904.51	\$ 313,137.25	\$ 150,546.62	\$ 144,588.87
Initial Contingency	\$ 150,000.00	\$ 150,000.00	\$ 150,000.00	\$ 150,000.00	\$ 150,000.00
Project Grand Total	\$ 3,553,103.84	\$ 3,171,994.67	\$ 6,725,882.27	\$ 3,311,479.11	\$ 3,186,366.37

* Concept costs include 72" pipe for wildlife crossing as cost savings measure. Cost to utilize box culvert shown below.

** Fencing cost based on engineering estimate.

Box Culvert Option					
Box Culvert (10' x 6')	\$ 202,671.00	\$ 194,487.00		\$ 198,786.00	\$ 194,487.00
72" Pipe Culvert (Linear Ft x \$1000.00)	\$ (74,000.00)	\$ (69,000.00)	\$ -	\$ (71,000.00)	\$ (69,000.00)
Endwall (Cubic Yard x \$1716.57)	\$ (49,780.53)	\$ (49,780.53)	\$ -	\$ (49,780.53)	\$ (49,780.53)
Reinforcing Steel (Pounds x \$0.22)	\$ (549.56)	\$ (549.56)	\$ -	\$ (549.56)	\$ (549.56)
Project SubTotal (Box Culvert)	\$ 2,896,646.37	\$ 2,577,844.01	\$ 5,445,865.24	\$ 2,695,658.07	\$ 2,589,746.04
MOT (5%)	\$ 144,832.32	\$ 128,892.20	\$ 272,293.26	\$ 134,782.90	\$ 129,487.30
Mobilization (10%)	\$ 289,664.64	\$ 257,784.40	\$ 544,586.52	\$ 269,565.81	\$ 258,974.60
Project Total (Box Culvert)	\$ 3,331,143.33	\$ 2,964,520.61	\$ 6,262,745.02	\$ 3,100,006.78	\$ 2,978,207.94
Project Unknowns (5%)	\$ 166,557.17	\$ 148,226.03	\$ 313,137.25	\$ 155,000.34	\$ 148,910.40
Initial Contingency	\$ 150,000.00	\$ 150,000.00	\$ 150,000.00	\$ 150,000.00	\$ 150,000.00
Project Grand Total	\$ 3,647,700.49	\$ 3,262,746.64	\$ 6,725,882.27	\$ 3,405,007.12	\$ 3,277,118.34
Difference in Cost	\$ 94,596.65	\$ 90,751.97	\$ -	\$ 93,528.01	\$ 90,751.97

Attachment B



Westclox St

Gopher Ridge Rd

Scrub-Jay Group B

Scrub-Jay Group A

New Market Rd



Lake Trafford Rd

Immokalee Regional Airport

Legend

- Pond Sites
- Segment 6 (417450-6)
- Segment 5 (417450-5)
- Segment 4 (417450-4)
- Upland Management Area
- Scrub-Jay Action Area
- Scrub-Jay Territory
- Scrub-Jay Habitat
 - Type I and II
 - Type III



Attachment C

MEETING MINUTES
FWC – URS
Mitigation Process
Airport Conservation Easement
SR 29 PD&E Study
From Oil Well Road to SR 82
Financial Project No.: 417540-1-22-01
FDOT Contract: C8N56
URS Project No. 12007302, File 106.04

November 1, 2013

Attendees:

Tom Pride – URS
Adam Purcell – URS
Marty Peate - URS
Richard McCann – FWC

On November 1, 2013 a conference call was held between the Florida Fish and Wildlife Conservation Commission (FWC) and the URS Corporation (representing FDOT D1). The call focused on the identification of mitigation options in addressing potential impacts on the Immokalee Airport Conservation Easement that may result from the development of SR 29 PD&E Central 2 Alternative. The meeting occurred at 1:30 in the afternoon.

Both a copy of the Conservation Easement Deed and diagram depicting the Central 2 Alternative in the area of the Conservation Easement were forwarded to Rick McCann on 17 September for review prior to the conference call. A copy of the information forwarded to FWC is attached.

The call opened with staff introductions and a brief description of the SR 29 project. Reference was made to the information transmitted in September. Mr. McCann was generally familiar with both the property in question and PD&E process.

Tom Pride posed the question, “What is the process that needs to be followed to use a part of the easement?”. Mr. McCann responded by identifying the following steps:

1. **Must Mitigate First:** Lands for mitigation must be identified, purchased, and dedicated to FWC prior to taking any action that impacts existing protected lands
 - a. **Adjacent Lands are Best:** In selecting property to serve as mitigation, lands contiguous to the property being impacted are preferred by FWC.
 - b. **Adjacent to Public Land:** If property adjacent to the impacted resource is not available, mitigation lands should be adjacent to other existing Public Lands.
 - c. **2:1 Ratio:** Two acres of “good” habit must be provided for every 1 acre of protected land impacted. (Result for impact depicted \approx 7 Acres of mitigation).

- d. **Tortoise Present:** Lands selected for mitigation of the Airport Easement which functions to protect the gopher tortoise, must already have tortoises present.
 - e. **Managed for Tortoise:** The area selected for mitigation must be managed for the gopher tortoise. FDOT must provide funding for management activity.
 - i. Mr. McCann noted current management costs range between \$20 and \$30 per acre per year for property that can be managed through controlled burns. Management costs in areas not able to be burned (require mechanical clearing) are much higher.
 - ii. FWC assumes 4% annual growth on funds dedicated for management.
2. Based on the criteria outlined in the previous step, FWC must then review the area proposed for acquisition, and agree to the site selected.

Mr. McCann offered Heather Rigby as the FWC regional point of contact for the mitigation effort related to the SR 29 project. He also noted Richard Mospens manages the FWC land acquisition program in Southwest Florida.

Attachments



Immokalee
Regional Airport
Conservation Easement

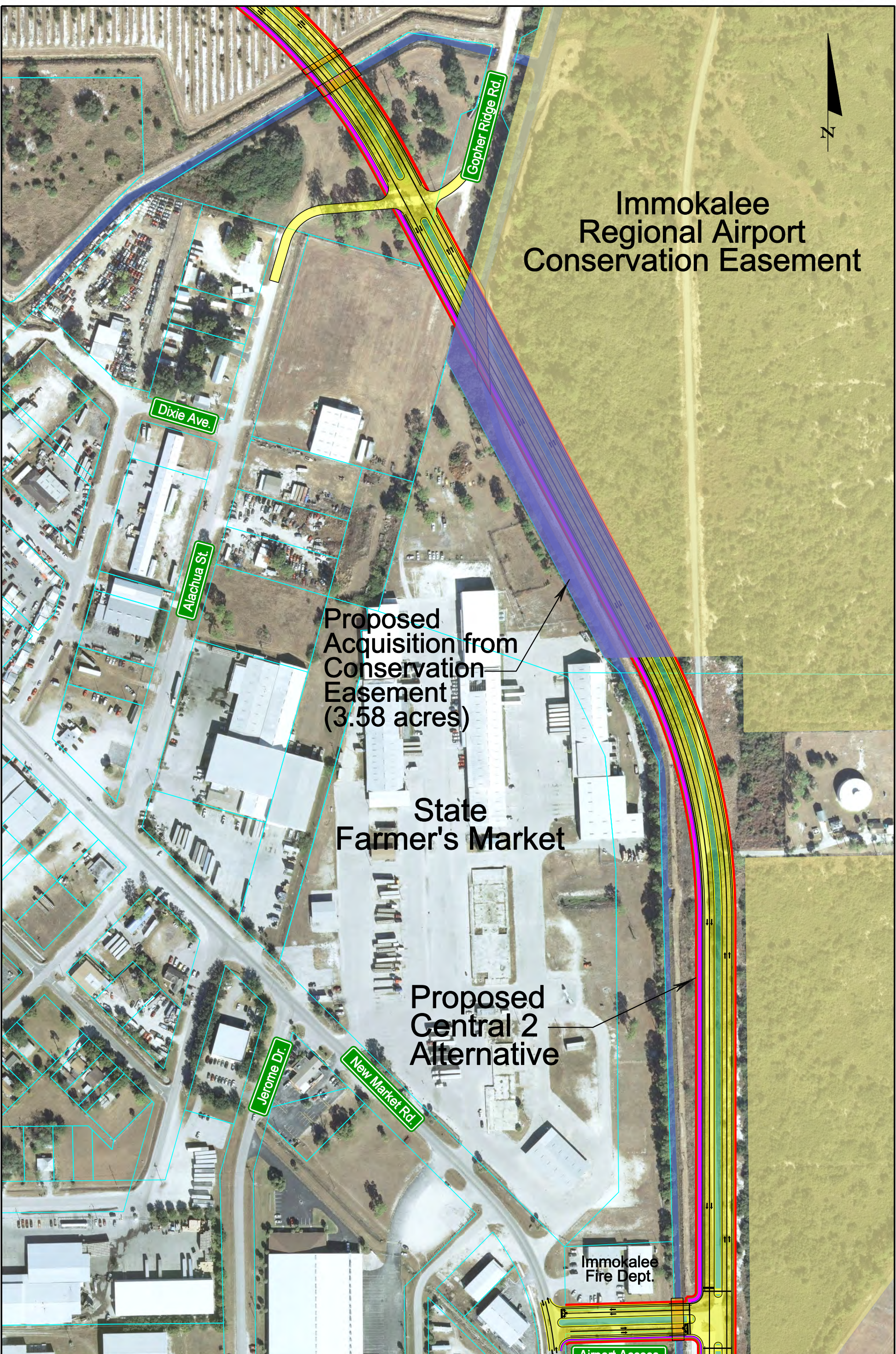
Proposed
Acquisition from
Conservation
Easement
(3.58 acres)

State
Farmer's Market

Proposed
Central 2
Alternative

Immokalee
Fire Dept.

Airport Access



AFFIDAVIT

STATE OF FLORIDA)
COUNTY OF COLLIER)

BEFORE ME, an officer duly qualified to administer oaths and take acknowledgments in Collier County, Florida, personally appeared the undersigned Affiant, who by me being first duly sworn, deposes and says:

1. I, the undersigned Affiant, am employed by the Collier County Airport Authority, a dependent district of the County of Collier, a political subdivision of the State of Florida. I am authorized to submit this Affidavit on behalf of the Airport Authority.

2. I hereby certify that attached hereto as Exhibit "A" is a "true copy" of a Deed of Conservation Easement that is yet to be recorded in the Public Records of Collier County. Exhibit "A" is exactly similar to a prior Deed of this same Conservation Easement, which prior Deed is recorded at book 2614, Page 0666, public records of Collier County, Florida.

3. I hereby certify that the only difference between the attached Exhibit "A" and the prior recorded Deed of Conservation Easement is that the words "incorporated herein by reference" have been inserted in the 5th line of paragraph numbered 1 of page 1 in the Deed. These words have been inserted into the Deed as only a clarification of the prior Deed. The initials on the right hand side of page one (1) are also additional for approval of change. Nothing substantive in the prior Deed of Easement is being changed by this amendment to the Deed.

SWORN TO AND SUBSCRIBED before me by _____;
who is personally known to me, this 13 day of September 2001.



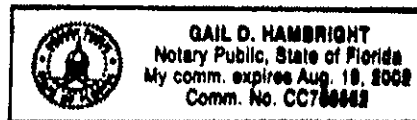
John Drury, Affiant



Notary Public, State of Florida at Large

Commission Number: CC 763562

My Commission expires: Aug 19, 2002



Retn:
COLLIER COUNTY AIRPORT AUTHORITY
2003 MAINSAIL DR
NAPLES FL 34114

2559937 OR: 2614 PG: 0666

RECORDED in the OFFICIAL RECORDS of COLLIER COUNTY, FL
11/23/1999 at 09:49AM DWIGHT H. BROCK, CLERK

RAC P&B	55.50
DOC-.70	.70
COPIES	12.00
MISC	1.00

DEED OF CONSERVATION EASEMENT

THIS DEED OF CONSERVATION EASEMENT is hereby granted by the Collier County Airport Authority and the County of Collier, a political subdivision of the State of Florida, whose mailing address is 3301 Tamiami Trail, Administration Building, Naples, Florida, 34112, ("Grantor") to the Fish and Wildlife Conservation Commission, an Agency of the State of Florida, whose address is 620 South Meridian Street, Tallahassee, Florida, 32399-1600, ("Grantee").

WITNESSETH:

WHEREAS, the Grantor is the owner of certain lands situated in Collier County, Florida, hereinafter referred to as the "Property," more specifically described in Exhibit "A" attached hereto and made a part hereof; and

WHEREAS, Grantor desires to protect and conserve certain uplands preserve areas; and

WHEREAS, Grantor in consideration of the consents of the Grantee to that certain Gopher Tortoise Incidental Take Permit issued to Grantor on the 3rd day of November, 1999, by Grantee, hereinafter referred to as the "Permit," whereby Grantor is required to grant and secure the enforcement of a perpetual conservation easement as defined in Section 704.06, *Florida Statutes* (1991), over the Easement Area.

NOW THEREFORE, consistent with the issuance of the Permit, Grantor hereby grants, creates, and establishes a perpetual conservation easement upon the Easement Area, which is depicted by the "cross-hatched" areas (generally west of runway 18-36) as shown on Exhibit "B." The Easement Area does not include 50 ft. wide existing roads and their associated rights-of-way and that proposed road to the hangar located within the Easement Area all identified on Exhibit "B." The easement shall run with the land within the Easement Area and shall be binding upon Grantor, its heirs, successors and assigns, and shall remain in full force and effect forever unless released by Grantee, its successors or assigns, as the case may then be.

1. The scope, nature, and character of this easement is to ensure, to the greatest extent now and/or hereafter allowed by law, that uplands preserve areas, including buffer zones, (Easement Area) shall be used only as conservation areas pursuant to Section 704.06, *Florida Statutes*, consistent with the Permit and the Management Plan ^(incorporated herein by reference). To carry out these purposes, the following rights are conveyed to Grantee by this easement:

(a) Authorized representatives of Grantee may enter upon the Property at reasonable times to enforce any and all rights herein granted upon prior notice to Grantor (or Grantor's successors or assigns as the case may then be) in a manner that will

PSM

MW

09H

Deed of Conservation Easement
Page 2

not unreasonably interfere with the use and quiet enjoyment of the Property by Grantor (its successors or assigns as the case may be) at the time of each such entry; and,

(b) Authorized representatives of (1) Grantee and/or (2) any other then authorized government entity may enjoy any activity on, or use of, the Property that is inconsistent with the purpose of this easement, and may enforce restoration of such areas or features of the Property that may be damaged by any inconsistent activity or use.

2. Collier County reserves to itself, its successors and/or assigns all rights as a non-charter County and as owner of the Airport, including rights to engage in all uses of the Airport that are not expressly prohibited herein and which are not inconsistent with the purposes of this easement as set forth in Section 704.06, *Florida Statutes*, as now exists or may hereafter be amended. Within the Easement Area, the following are prohibited uses/activities except as otherwise authorized elsewhere in this document, or as then required by applicable law(s), including Chapter 333, *Florida Statutes*, or the then current FAA FAR Part 77 Standards (or successor) in function of any of same:

(a) Construction or placing of buildings, roads, signs, billboards and other advertising, utilities, and/or other structures on or above the ground.

(b) Dumping or otherwise placing of trash, waste, or unsightly or offensive materials.

(c) Removal or alteration of trees, shrubs, or other vegetation unless allowed or mandated by the then current Management Plan and/or officials of Grantee.

(d) Excavation, dredging, or removal of loam, peat, gravel, soil, rock, or other material substance in such manner as to affect the surface except as necessary for maintenance of drainage ditches.

(e) Surface use except for purposes that permit the land or water area to remain predominantly in its natural condition.

(f) Activities detrimental to drainage, flood control, water conservation, erosion control, soil conservation, and/or fish and/or wildlife habitat preservation.

(g) Acts and/or uses that are detrimental to such retention of land.

(h) Alteration of the Easement Area except to the extent authorized by the Permit, by then applicable law(s) and/or the then current Management Plan.

Deed of Conservation Easement
Page 3

(i) Use of any pesticides within the Property without the prior written consent of the Grantee, which consent(s) will not be unreasonably withheld.

3. No right of access by any members of the general public to any portion of the Easement Area is conveyed by this easement.

4. Grantor agrees to bear all reasonable costs related to the normal operation, upkeep and maintenance of the Easement Area.

5. Grantor agrees that taxes and/or assessments levied on the Airport by competent authority will be paid.

6. Enforcement of this easement shall be at the discretion of Grantee. Any forbearance on behalf of Grantee to exercise any of its rights hereunder in the event of any breach hereof by Grantor (or its successors, personal representatives or assigns, as the case may be) shall not be deemed or construed to be any waiver of Grantee's rights hereunder in the event of any subsequent breach hereof.

7. Grantee will hold this easement exclusively for conservation purposes and will not assign this easement or any of its rights and/or obligations hereunder except to another organization then (a) qualified by law to hold such interests and/or perform those obligations under any then applicable law(s), and (b) then and thereafter committed to holding this easement exclusively for conservation purposes. If Grantor sells or otherwise conveys title to any part of the Airport or Easement Area to any person or entity, Grantor shall deliver written notice of such transfer to Grantee not later than thirty (30) days after recordation of the respective transfer in the public records of Collier County.

8. If any provision of this easement, or the application thereof to any person or circumstance, is found to be invalid, all other provisions of this easement, and the applications of any such provision(s) to persons or circumstances other than those as to which it is found to be invalid, shall not be affected thereby to the greatest extent then allowed by law.

9. All notices, consents, approvals or other communications pursuant hereto shall be in writing and shall be deemed properly received by the recipient if actually received via United States mail, certified, return receipt requested, addressed to the appropriate party (or successor-in-interest), at the addresses above set forth or such new address as either party may in writing deliver to the other party.

10. This easement may be amended, altered, released or revoked only by written agreement between the parties hereto, or their successors or assigns.

11. Grantor certifies to Grantee that the Airport is not subject to any mortgage or any other form of security that is superior to this easement or which needs to be subordinated.

12. This easement shall be recorded by Grantor in the public records of Collier County after receipt of acknowledgment of acceptance hereof by the Grantee.

13. Grantor agrees that if the property is subject to a mortgage or any form of security interest, Grantor shall provide documentation to verify that mortgage or security interest is subordinate to this Conservation Easement and such verification shall be provided and recorded either before or concurrent with execution of this Easement.

TO HAVE AND TO HOLD unto Grantees, their respective successors and assigns forever. The covenants, terms, conditions, restrictions and purposes imposed with this easement shall not only be binding upon Grantor but also upon its agents, authorized representatives, assigns, and all other successors in interest to it, and shall continue as a servitude running in perpetuity with the Airport unless and until released by Grantee or its successor or assigns, and then only to the extent of any such release(s).

IN WITNESS WHEREOF, Grantor has executed this easement on the 14 day of Sept, 1999.

ATTEST:
Dwight E. Brock, Clerk

BOARD OF COUNTY COMMISSION
OF COLLIER COUNTY, FLORIDA

By: [Signature]
Deputy Clerk
Attest as to Chairman's
signature only.

By: [Signature]
Pamela S. Mac'Kie, Chairwoman

Approved as to form
and legal sufficiency:

COLLIER COUNTY AIRPORT AUTHORITY
By: [Signature]
Michael Williams, Chairman

By: [Signature]
Thomas C. Palmer
Assistant County Attorney

GRANTEE'S ACCEPTANCE

The Fish and Wildlife Conservation Commission hereby approves and accepts the foregoing Conservation Easement and agrees to all of its terms and provisions this 26th Day of October, 1999.

**FLORIDA FISH AND WILDLIFE
CONSERVATION COMMISSION**

WITNESSES:

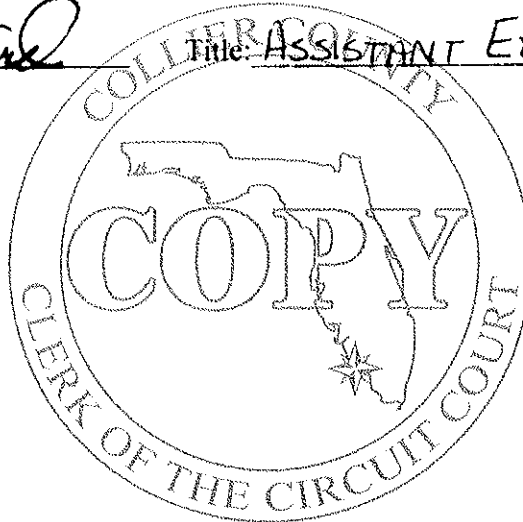
By: *Victor Heller*

Brenda Collins

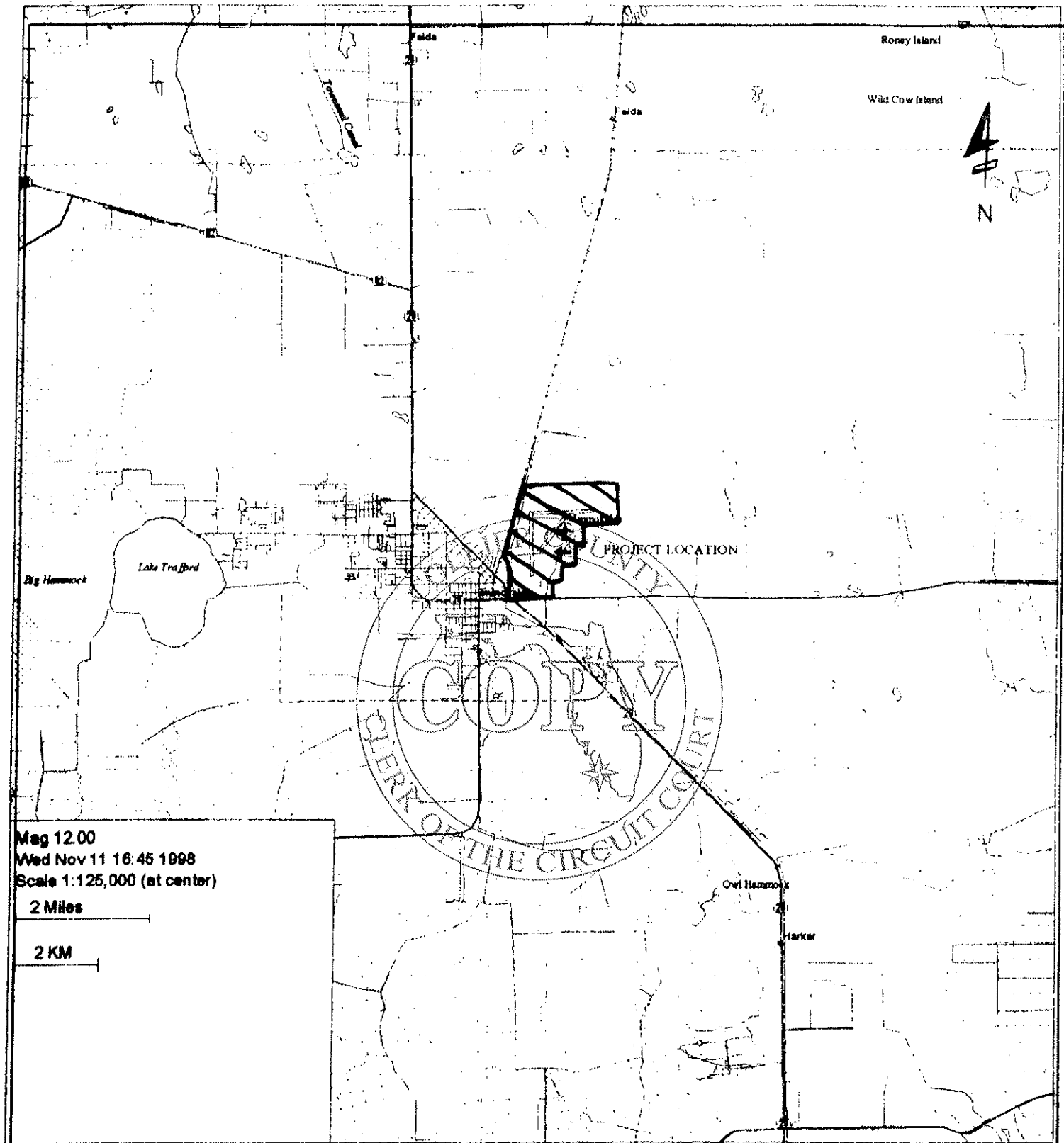
Name VICTOR J. Heller

Cynthia Ward

Title: ASSISTANT Executive Director



APPROVED AS TO FORM
AND LEGAL SUFFICIENCY
Victor J. Heller
Commission Attorney



OR: 2614 PG: 0671

DATE: 11-12-98

**IMMOKALEE REGIONAL AIRPORT
LOCATION MAP**

SCALE:

PROJECT NO: 94067.8

EXHIBIT A

**FLORIDA
ENVIRONMENTAL
INCORPORATED**

18505 Paulson Drive, Bldg. B
Port Charlotte, FL 33954
Tel (941) 624-2911
Fax (941) 629-7210

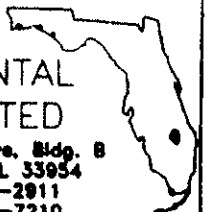
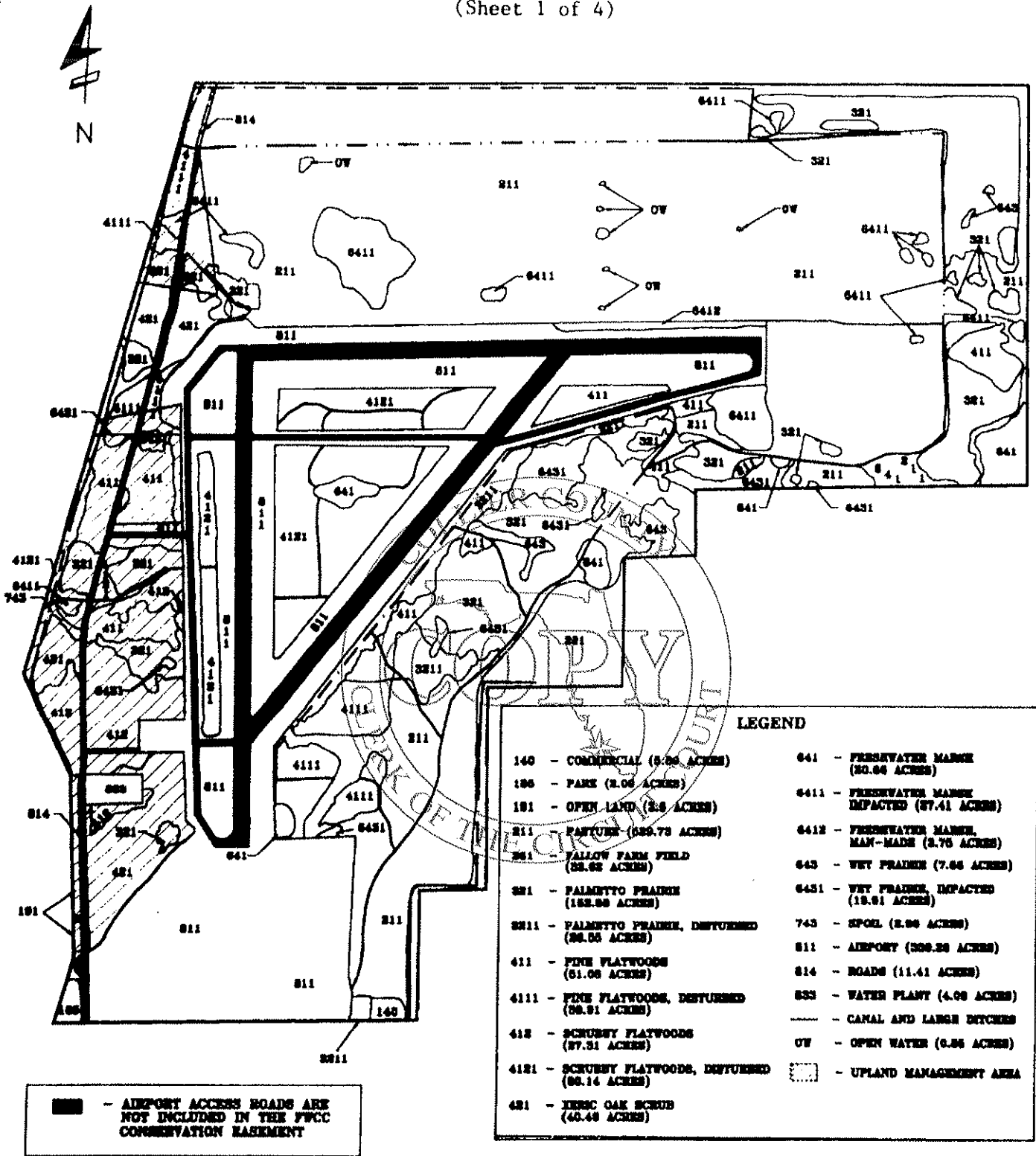


Exhibit B
(Sheet 1 of 4)



DATE: 7/12/99

SCALE: 1" = 1400'

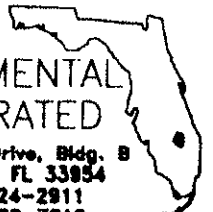
PROJECT NO: 94067.10

IMMOKALEE REGIONAL AIRPORT
UPLAND
MANAGEMENT AREA

EXHIBIT B SHEET 1 OF 4

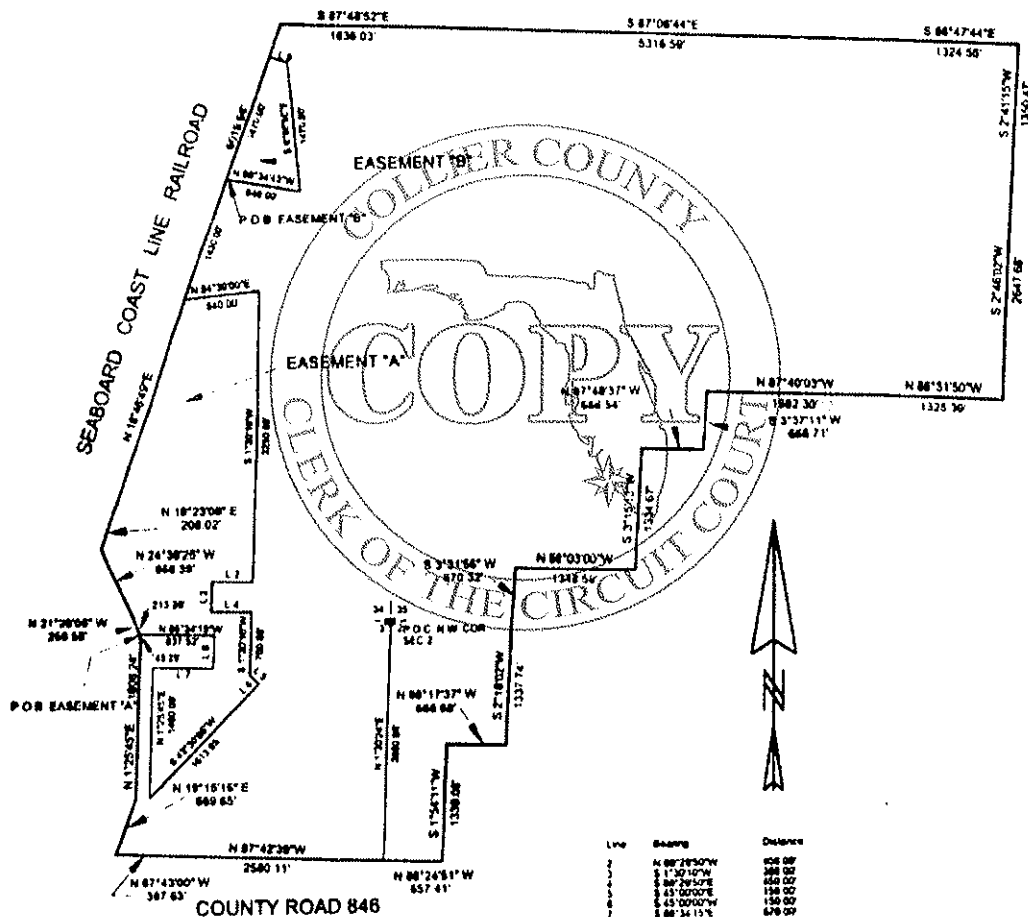
FLORIDA
ENVIRONMENTAL
INCORPORATED

18505 Paulson Drive, Bldg. B
Port Charlotte, FL 33954
Tel (941) 624-2911
Fax (941) 629-7210



PUBLIC WORKS ENGINEERING DEPARTMENT
3301 EAST TAMiami TRAIL. NApLES, FLORIDA 34112
(941) 774-8192

SKETCH OF DESCRIPTION
NOT A SURVEY



Line	Bearing	Distance
1	N 87°29'50"W	105.00'
2	S 1°30'10"W	288.00'
3	S 89°29'50"E	550.00'
4	S 43°00'00"E	194.00'
5	S 43°00'00"W	150.00'
6	S 89°34'15"E	470.00'
7	N 1°25'43"E	170.00'
8	S 71°15'11"E	210.00'

- GENERAL NOTES**
- 1) P.O.C. = POINT OF COMMENCEMENT
 - 2) P.O.B. = POINT OF BEGINNING
 - 3) SEC. = SECTION
 - 4) TWP. = TOWNSHIP
 - 5) RGE. = RANGE
 - 6) RAW = RIGHT OF WAY
 - 7) ALL DISTANCES ARE IN FEET AND DECIMALS THEREOF
 - 8) NOT VALID UNLESS SIGNED AND SEALED WITH THE EMBOSSED SEAL OF A PROFESSIONAL LAND SURVEYOR

DRAWN BY	CHECKED BY	SCALE	DATE	FILE NO.	
		NOT TO SCALE	OCTOBER 25, 1999	IAFSJE	SHEET 2 OF 4

PUBLIC WORKS ENGINEERING DEPARTMENT
3301 EAST TAMiami TRAIL NAPLES, FLORIDA 34112
(941) 774-8192

Easement Parcel "A"

PROJECT NO...

PARCEL NO...

FOLIO NO...

LEGAL DESCRIPTION (NOT A SURVEY)

COMMENCING AT THE NORTHWEST CORNER OF SECTION 2, TOWNSHIP 47 SOUTH, RANGE 29 EAST, COLLIER COUNTY, FLORIDA; THENCE SOUTH 1 DEGREE 30 MINUTES 24 SECONDS WEST, A DISTANCE OF 2680.88 FEET; THENCE NORTH 87 DEGREES 42 MINUTES 39 SECONDS WEST ALONG THE NORTH RIGHT OF WAY LINE OF COUNTY ROAD 846, A DISTANCE OF 2580.11 FEET; THENCE NORTH 87 DEGREES 43 MINUTES 00 SECONDS WEST, A DISTANCE OF 307.67 FEET; THENCE LEAVING SAID NORTH RIGHT OF WAY LINE NORTH 19 DEGREES 15 MINUTES 15 SECONDS EAST, A DISTANCE OF 669.65 FEET; THENCE NORTH 1 DEGREE 25 MINUTES 45 SECONDS EAST, A DISTANCE OF 1808.24 FEET; THENCE NORTH 21 DEGREES 20 MINUTES 05 SECONDS WEST, A DISTANCE OF 45.29 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING NORTH ALONG SAID LINE, A DISTANCE OF 213.26 FEET; THENCE NORTH 24 DEGREES 38 MINUTES 25 SECONDS WEST, A DISTANCE OF 856.39 FEET; THENCE NORTH 18 DEGREES 23 MINUTES 08 SECONDS EAST ALONG THE EAST RIGHT OF WAY LINE OF THE SEABOARD COASTLINE RAILROAD, A DISTANCE OF 208.02 FEET; THENCE NORTH 18 DEGREES 46 MINUTES 49 SECONDS EAST, A DISTANCE OF 2728.00 FEET; THENCE LEAVING SAID EAST RIGHT OF WAY LINE NORTH 84 DEGREES 30 MINUTES 00 SECONDS EAST, A DISTANCE OF 840.00 FEET; THENCE SOUTH 1 DEGREE 30 MINUTES 10 SECONDS WEST, A DISTANCE OF 3250.00 FEET; THENCE NORTH 88 DEGREES 29 MINUTES 50 SECONDS WEST, A DISTANCE OF 450.00 FEET; THENCE SOUTH 1 DEGREE 30 MINUTES 10 SECONDS WEST, A DISTANCE OF 360.00 FEET; THENCE SOUTH 88 DEGREES 29 MINUTES 50 SECONDS EAST, A DISTANCE OF 450.00 FEET; THENCE SOUTH 1 DEGREE 30 MINUTES 10 SECONDS WEST, A DISTANCE OF 700.00 FEET; THENCE SOUTH 45 DEGREES 00 MINUTES 00 SECONDS EAST, A DISTANCE OF 150.00 FEET; THENCE SOUTH 45 DEGREES 00 MINUTES 00 SECONDS WEST, A DISTANCE OF 150.00 FEET; THENCE SOUTH 43 DEGREES 30 MINUTES 06 SECONDS WEST, A DISTANCE OF 1613.95 FEET; THENCE NORTH 1 DEGREE 25 MINUTES 45 SECONDS EAST, A DISTANCE OF 1480.00 FEET; THENCE SOUTH 18 DEGREES 04 MINUTES 15 SECONDS EAST, A DISTANCE OF 670.00 FEET; THENCE NORTH 1 DEGREE 25 MINUTES 45 SECONDS EAST, A DISTANCE OF 370.00 FEET; THENCE NORTH 88 DEGREES 34 MINUTES 15 SECONDS WEST, A DISTANCE OF 837.52 FEET TO THE POINT OF BEGINNING, SAID DESCRIBED TRACT CONTAINING 137.084 ACRES (5,971,401) SQUARE FEET), MORE OR LESS.

IAFSJEA

PREPARED BY:

George R. Richmond
GEORGE R. RICHMOND

DATE: 11/2/89

PROFESSIONAL LAND SURVEYOR-FL. REG. # 2406
PUBLIC WORKS ENGINEERING DEPARTMENT.
COLLIER COUNTY GOVERNMENT COMPLEX
3301 EAST TAMiami TRAIL
NAPLES, FLORIDA 34112

SHEET 3 OF 4

PUBLIC WORKS ENGINEERING DEPARTMENT
3301 EAST TAMiami TRAIL NAPLES, FLORIDA 34112
(941) 774-8192

PROJECT NO.

PARCEL NO...

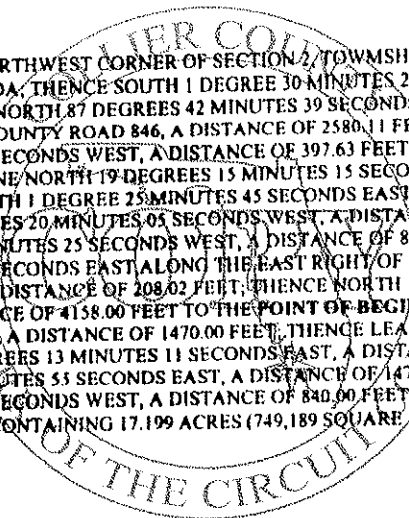
FOLIO NO...

LEGAL DESCRIPTION (NOT A SURVEY)

Easement Parcel "B"

COMMENCING AT THE NORTHWEST CORNER OF SECTION 2, TOWNSHIP 47 SOUTH, RANGE 29 EAST, COLLIER COUNTY, FLORIDA, THENCE SOUTH 1 DEGREE 30 MINUTES 24 SECONDS WEST, A DISTANCE OF 2680.88 FEET; THENCE NORTH 87 DEGREES 42 MINUTES 39 SECONDS WEST ALONG THE NORTH RIGHT OF WAY LINE OF COUNTY ROAD 846, A DISTANCE OF 2580.11 FEET; THENCE NORTH 87 DEGREES 43 MINUTES 00 SECONDS WEST, A DISTANCE OF 397.63 FEET; THENCE LEAVING SAID NORTH RIGHT OF WAY LINE NORTH 19 DEGREES 15 MINUTES 15 SECONDS EAST, A DISTANCE OF 669.65 FEET; THENCE NORTH 1 DEGREE 25 MINUTES 45 SECONDS EAST, A DISTANCE OF 1808.24 FEET; THENCE NORTH 21 DEGREES 20 MINUTES 05 SECONDS WEST, A DISTANCE OF 258.55 FEET; THENCE NORTH 24 DEGREES 38 MINUTES 25 SECONDS WEST, A DISTANCE OF 856.39 FEET; THENCE NORTH 18 DEGREES 23 MINUTES 08 SECONDS EAST ALONG THE EAST RIGHT OF WAY LINE OF THE SEABOARD COAST LINE RAILROAD A DISTANCE OF 208.02 FEET; THENCE NORTH 18 DEGREES 46 MINUTES 49 SECONDS EAST, A DISTANCE OF 4158.00 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING NORTH ALONG SAID LINE, A DISTANCE OF 1470.00 FEET; THENCE LEAVING SAID EAST RIGHT OF WAY LINE SOUTH 71 DEGREES 13 MINUTES 11 SECONDS EAST, A DISTANCE OF 210.00 FEET; THENCE SOUTH 6 DEGREES 06 MINUTES 53 SECONDS EAST, A DISTANCE OF 1470.00 FEET; THENCE NORTH 90 DEGREES 34 MINUTES 43 SECONDS WEST, A DISTANCE OF 840.00 FEET TO THE POINT OF BEGINNING; SAID DESCRIBED TRACT CONTAINING 17.199 ACRES (749,189 SQUARE FEET), MORE OR LESS.

IAFSJEB



PREPARED BY George R. Richmond DATE 8/25/92

GEORGE R. RICHMOND

PROFESSIONAL LAND SURVEYOR-FL. REG. # 2406
PUBLIC WORKS ENGINEERING DEPARTMENT
COLLIER COUNTY GOVERNMENT COMPLEX
3301 EAST TAMiami TRAIL
NAPLES, FLORIDA 34112

SHEET 4 OF 4

PUBLIC WORKS ENGINEERING DEPARTMENT
 3301 EAST TAMiami TRAIL NAPLES, FLORIDA 34112
 (941) 774-8192

PROJECT NO..

PARCEL NO..

FOLIO NO..

LEGAL DESCRIPTION (NOT A SURVEY)

COMMENCING AT THE NORTHWEST CORNER OF SECTION 2, TOWNSHIP 47 SOUTH, RANGE 29 EAST, COLLIER COUNTY, FLORIDA; THENCE SOUTH 1 DEGREE 30 MINUTES 24 SECONDS WEST, A DISTANCE OF 2680.88 FEET; THENCE NORTH 87 DEGREES 42 MINUTES 39 SECONDS WEST ALONG THE NORTH RIGHT OF WAY LINE OF COUNTY ROAD 846, A DISTANCE OF 2580.11 FEET; THENCE NORTH 87 DEGREES 43 MINUTES 00 SECONDS WEST, A DISTANCE OF 397.63 FEET; THENCE LEAVING SAID NORTH RIGHT OF WAY LINE NORTH 19 DEGREES 15 MINUTES 15 SECONDS EAST, A DISTANCE OF 669.65 FEET; THENCE NORTH 1 DEGREE 25 MINUTES 45 SECONDS EAST, A DISTANCE OF 1808.24 FEET; THENCE NORTH 21 DEGREES 20 MINUTES 05 SECONDS WEST, A DISTANCE OF 258.55 FEET; THENCE NORTH 24 DEGREES 38 MINUTES 25 SECONDS WEST, A DISTANCE OF 856.39 FEET; THENCE NORTH 18 DEGREES 23 MINUTES 08 SECONDS EAST, ALONG THE EAST RIGHT OF WAY LINE OF THE SEABOARD COAST LINE RAILROAD A DISTANCE OF 208.02 FEET; THENCE NORTH 18 DEGREES 46 MINUTES 49 SECONDS EAST, A DISTANCE OF 4158.00 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING NORTH ALONG SAID LINE, A DISTANCE OF 1470.00 FEET; THENCE LEAVING SAID EAST RIGHT OF WAY LINE SOUTH 71 DEGREES 13 MINUTES 11 SECONDS EAST, A DISTANCE OF 210.00 FEET; THENCE SOUTH 6 DEGREES 06 MINUTES 35 SECONDS EAST, A DISTANCE OF 1470.00 FEET; THENCE NORTH 80 DEGREES 34 MINUTES 43 SECONDS WEST, A DISTANCE OF 840.00 FEET TO THE POINT OF BEGINNING; SAID DESCRIBED TRACT CONTAINING 17.199 ACRES (749,140 SQUARE FEET), MORE OR LESS.

IAFSJEB

PREPARED BY *George R. Richmond* DATE *10/25/75*

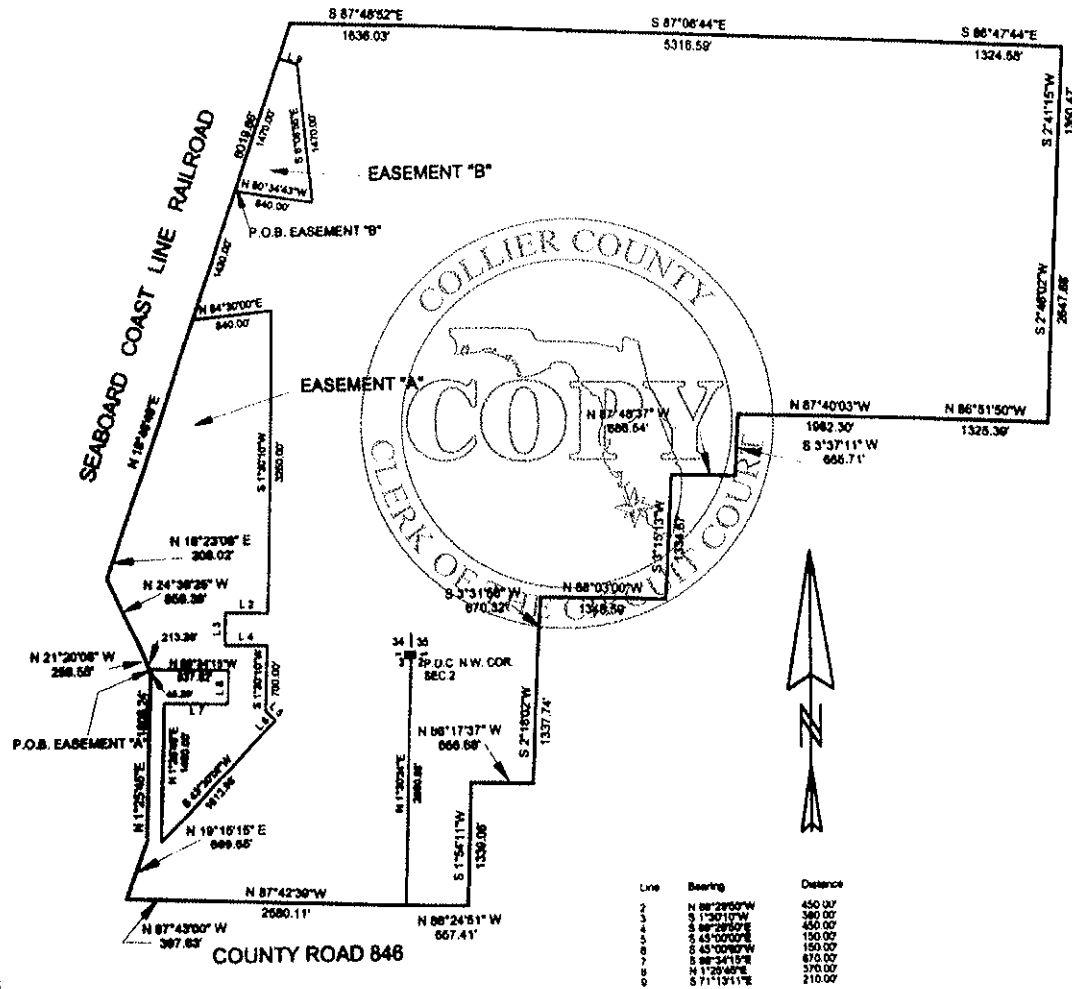
GEORGE R. RICHMOND

PROFESSIONAL LAND SURVEYOR-FL. REG. # 2406
 PUBLIC WORKS ENGINEERING DEPARTMENT.
 COLLIER COUNTY GOVERNMENT COMPLEX
 3301 EAST TAMiami TRAIL
 NAPLES, FLORIDA 34112

SHEET 1 OF 2

PUBLIC WORKS ENGINEERING DEPARTMENT
 3301 EAST TAMiami TRAIL NAPLES, FLORIDA 34112
 (941) 774-8192

SKETCH OF DESCRIPTION
 NOT A SURVEY

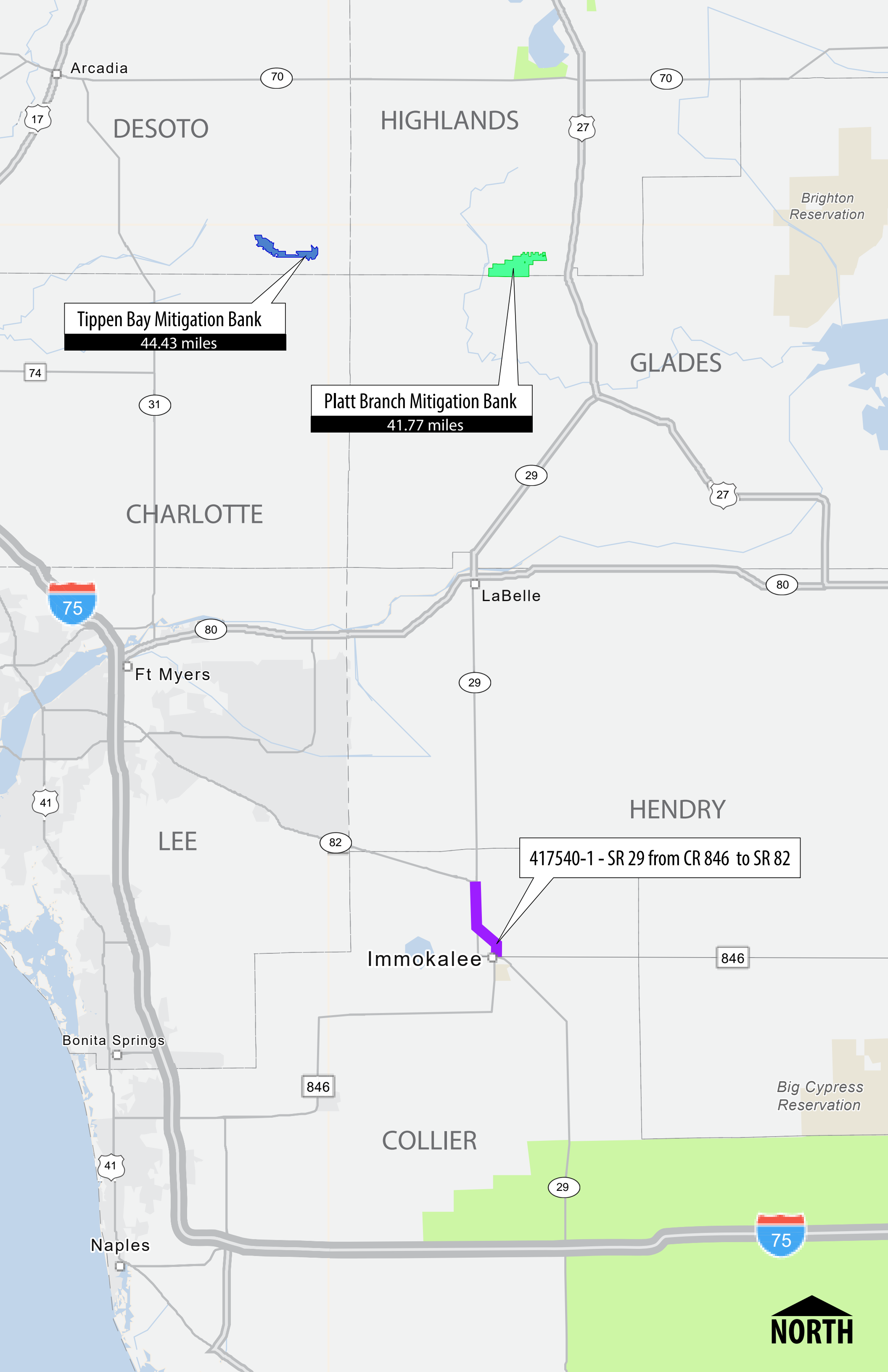


GENERAL NOTES

- 1) P.O.C. = POINT OF COMMENCEMENT
- 2) P.O.B. = POINT OF BEGINNING
- 3) SEC. = SECTION
- 4) TWP. = TOWNSHIP
- 5) RGE. = RANGE
- 6) RAW = RIGHT OF WAY
- 7) ALL DISTANCES ARE IN FEET AND DECIMALS THEREOF
- 8) NOT VALID UNLESS SIGNED AND SEALED WITH THE EMBOSSED SEAL OF A PROFESSIONAL LAND SURVEYOR

DRAWN BY	CHECKED BY	SCALE	DATE	FILE NO	SHEET 2 OF 2
		NOT TO SCALE	OCTOBER 25, 1999	IAFSJE	

Attachment D



Tippen Bay Mitigation Bank
44.43 miles

Platt Branch Mitigation Bank
41.77 miles

417540-1 - SR 29 from CR 846 to SR 82

NORTH

Attachment E

**PLATT BRANCH MITIGATION BANK
SPECIES CREDIT LEDGER**

DO NOT EDIT THIS TABLE						
Habitat Group	Species Combination FP = Florida Panther, SC = Sandhill Crane IS = Indigo Snake, SJ = Scrub Jay BB = Black Bear, FS = Fox Squirrel RCW = Red-Cockaded Woodpecker GT = Gopher Tortoise, GF = Gopher Frog	Original Acre-Credits	Acre-Credits Used	Acre-Credits Left	Original PFU's	PFU's Left
1	FP, SC	67.5	14.9	52.6	607.5	473.7
2	FP, IS	38.1	4.3	33.8	266.7	236.7
3	FP, SC	3.5	3.5	-	31.5	-
4	FP, IS, BB	3.5	-	3.5	31.5	31.5
5	FP, IS, RCW	5.7	5.7	-	51.3	-
6	FP, IS, FS, SJ	33.2	33.2	-	298.8	-
7	FP, IS, SC, FS	122.8	-	122.8	859.6	859.6
8	FP, IS, SC, RCW	301.0	-	301.0	2,709.0	2,709.0
9	FP, IS, FS, RCW	6.9	-	6.9	62.1	62.1
10	FP, IS, BB, FS	15.1	-	15.1	135.9	135.9
11	FP, IS, SJ, FS	6.0	-	6.0	60.0	60.0
12	FP, IS, SC, FS, SJ	17.7	17.7	-	123.9	-
13	FP, IS, SC, BB, RCW	78.4	-	78.4	705.6	705.6
14	FP, IS, BB, FS, RCW	117.4	-	117.4	1,056.6	1,056.6
15	FP, IS, SC, FS, GT, GF	34.3	34.3	-	205.8	-
16	FP, IS, SJ, FS, GT, GF	7.9	7.9	-	71.1	-
17	FP, IS, SJ, FS, GT, GF	124.4	124.4	-	1,244.0	-
18	FP, IS, FS, RCW, GT, GF	402.9	-	402.9	3,626.1	3,626.1
19	FP, IS, FS, RCW, SJ, GT, GF	82.3	10.4	71.9	740.7	647.1
20	FP, IS, FS, SJ, RCW, GT, GF	45.1	-	45.1	405.9	405.9
21	FP, IS, BB, FS, RCW, GT, GF	103.2	-	103.2	928.8	928.8
22	FP, IS, SC, FS, RCW, GT, GF	67.0	-	67.0	603.0	603.0
23	FP, IS, SJ, BB, FS, RCW, GT, GF	8.1	-	8.1	72.9	72.9

TOTALS: 1692.0 256.3 1435.7 14898.3 12614.5

417540-1 SR 29 PD&E fr CR 846 to SR 82		
Date: 12/14/2023		
Species (Column 1)+DN16:DP35DN 42DDN16:DP45	Non-Panther Credits Used (Column 2)	PFU's Used for Panther (Column 3)
Scrub Jay	19.6	
Scrub Jay	17.7	
Scrub Jay	119.6	
Scrub Jay	10.4	
Additional Project Info: 167.3 Scrub Jay credits required		

DO NOT EDIT THIS TABLE			
ID	Species	Original Acre Credits	Remaining Acre Credits
FP	Panther (PFUs)	14,898.3	12,614.5
SC	Sandhill crane	692.2	621.8
IS	Indigo snake	1,621.0	1,383.1
SJ	Scrub jay	324.7	131.1
BB	Black bear	325.7	325.7
FS	Fox squirrel	1,194.3	966.4
RCW	RCW	1,218.0	1,201.9
GT	Gopher tortoise	875.2	698.2
GF	Gopher frog	875.2	698.2



Attachment F

the species and improve our consultation process. Surveys results and reports should be transmitted to the Service at FBBsurveyreport@fws.gov or mail electronic file to U.S. Fish and Wildlife Service, Attention Florida bonneted bat surveys, 1339 20th Street, Vero Beach, Florida 32960. When formal consultation is requested, survey results and reports should be submitted with the consultation request to verobeach@fws.gov.

No effect: If the use of the Key results in a determination of “no effect,” no further consultation is necessary with the Service. The Service recommends that the Corps (or other Federal action agency) documents the pathway used to reach the determination in the project record and proceeds with other species analyses as warranted.

May Affect, Not Likely to Adversely Affect (MANLAA): In this Key we have identified two ways that consultation can conclude informally, MANLAA-P and MANLAA-C.

MANLAA-P: If the use of the Key results in a determination of “MANLAA- P,” the Service concurs with this determination based on the rationale provide above, and no further consultation is necessary for the effects of the proposed action on the Florida bonneted bat. The Service recommends that the Corps (or other Federal action agency) documents the pathway used to reach the determination in the project record and proceeds with other species analyses as warranted.

MANLAA-C: If the use of the Key results in a determination of MANLAA-C, further consultation with the Service is required to confirm that the Key has been used properly, and the Service concurs with the evaluation of the survey results. Survey results should be submitted with the consultation request.

May Affect, Likely to Adversely Affect (LAA) - When the determination in the Key is “LAA” technical assistance with the Service and modifications to the proposed action may enable the project to be reevaluated and conclude with a MANLAA-C determination. Under other circumstance, “LAA” determinations will require formal consultation.

Working with the Fish and Wildlife Foundation of Florida, the Service has established a fund to support conservation and recovery for the Florida bonneted bat. Any project that has the potential to affect the Florida bonneted bat and/or its habitat is encouraged to make a voluntary contribution to this fund. If you would like additional information about how to make a contribution and how these monies are used to support Florida bonneted bat recovery please contact Ashleigh Blackford, Connie Cassler, or José Rivera at 772-562-3909.

This revised Key is effective immediately upon receipt by the Corps. Should circumstances change or new information become available regarding the Florida bonneted bat and/or implementation of the Key, the determinations herein may be reconsidered and this Key further revised or amended. We have established an email address to collect comments on the Key and the survey protocols at: FBBguidelines@fws.gov.

APPENDIX T

***FWS Concurrence Letter for
FDOT Section 7 Consultation Commitment***

From: Wrublik, John <john_wrublik@fws.gov>

Sent: Friday, March 8, 2024 8:29 AM

To: Clark, Thu-Huong <Thu-Huong.Clark@dot.state.fl.us>

Cc: Carey, Robert L <robert_carey@fws.gov>; Cantrell, Mark A <mark_a_cantrell@fws.gov>; Marshall, Jennifer <Jennifer.Marshall@dot.state.fl.us>; Cornwell, Katasha <Katasha.Cornwell@dot.state.fl.us>; Turner, Jonathan <Jonathan.Turner@dot.state.fl.us>; Horne, Abra <Abra.Horne@dot.state.fl.us>; James, Jeffrey W <Jeffrey.James@dot.state.fl.us>; Setchell, Brent <Brent.Setchell@dot.state.fl.us>; Kuhn-Hendricks, Katlin <Katlin.Kuhn-Hendricks@dot.state.fl.us>; Monies, Nicole <Nicole.Monies@dot.state.fl.us>

Subject: Re: [EXTERNAL] FPID 417540-1 SR 29 from SR 82 to Oil Well Rd - USFWS RAI Response

Thu,

Your request to defer consultation for the State 29 from State Road 82 to Oil Well Road project is acceptable to the Service. Based on our teams call yesterday, I understand that funding is available for State Road 29 from State Road 82 to County Road 846, and this segment will likely go to design phase later in the year. Moreover, the FDOT will pursue widening of the segment of the State Road 29 from County Road 846 to Oil Well Road at some time in the future when funds become available. Based on this approach with respect to consultation pursuant to Section 7 of the Endangered Species Act, the Service considers each of the two segments of the current project as separate projects, and will conduct a separate formal consultation for each project. As such, when the design phase of State Road 29 from State Road 82 to County Road 846 is completed and you are ready to consult on it with the Service, please send me a new formal consultation request and biological assessment for this project.

Also FYI, as I discussed in our teams call yesterday. please be aware that based on past surveys and information provided to the Service on the original project (i.e., State Road from State Road 82 to Oil Well Road), the Service finds that the State Road 29 from State Road 82 to County Road 846 project will adversely affect and result in the take of the endangered Florida panther and the threatened Florida scrub-jay. Whereas, the State Road 29 from County Road 846 to Oil Well Road project will adversely affect and result in take of the endangered Florida panther, the endangered Florida bonneted bat, and the threatened Audubon's crested caracara. Please keep this mind when you make determinations for Federally listed species in association with your requests for formal consultation for each project.

If you have any questions, please let me know.

Sincerely,

John M. Wrublik
U.S. Fish and Wildlife Service
777 37th Street, Suite D-101
Vero Beach, Florida 32960
Office: (772) 226-8130
email: John.Wrublik@fws.gov

NOTE: This email correspondence and any attachments to and from this sender is subject to the Freedom of Information Act (FOIA) and may be disclosed to third parties.

From: Clark, Thu-Huong <Thu-Huong.Clark@dot.state.fl.us>
Sent: Thursday, March 7, 2024 3:51 PM
To: Wrublik, John <john_wrublik@fws.gov>
Cc: Carey, Robert L <robert_carey@fws.gov>; Cantrell, Mark A <mark_a_cantrell@fws.gov>; Marshall, Jennifer <Jennifer.Marshall@dot.state.fl.us>; Cornwell, Katasha <Katasha.Cornwell@dot.state.fl.us>; Turner, Jonathan <Jonathan.Turner@dot.state.fl.us>; Horne, Abra <Abra.Horne@dot.state.fl.us>; James, Jeffrey W <Jeffrey.James@dot.state.fl.us>; Setchell, Brent <Brent.Setchell@dot.state.fl.us>; Kuhn-Hendricks, Katlin <Katlin.Kuhn-Hendricks@dot.state.fl.us>; Monies, Nicole <Nicole.Monies@dot.state.fl.us>
Subject: RE: [EXTERNAL] FPID 417540-1 SR 29 from SR 82 to Oil Well Rd - USFWS RAI Response

John,

FDOT recognizes that Section 7 ESA consultation for SR 29 from SR 82 to Oil Well Road cannot be completed at this time as design details related to species impacts have not been finalized. FDOT is requesting to delay Section 7 ESA consultation until design and permitting phase for the following species: Florida panther, Florida Scrub jay, Florida bonneted bat and Audubon's crested caracara. FDOT requests concurrence from the USFWS to defer ESA consultation until design details become available. FDOT anticipates that the northern segment of the project (SR 29 from SR 82 to CR 846) will be in Design phase later this year. FDOT offers the following commitments to USFWS:

1. To re-initiate Section 7 ESA consultation with the USFWS during design and permitting following species: Florida panther, Florida Scrub jay, Florida bonneted bat, and Audubon's crested caracara. FDOT will provide additional information, as needed, that will allow the USFWS to complete their analysis of the project's effects on documented species and complete consultation, for the project in accordance with Section 7 of the Endangered Species Act of 1973, as amended.

2. To implement best management practices consistent with the FDOT Conservation Plan for the Florida Panther
3. To construct the wildlife crossing between CR 846 and Oil Well Road. This crossing was listed at the 2024 annual prioritization meeting (held January 17, 2024) of the FDOT Conservation Plan for the Florida Panther to determine priority for available funding. As part of the preferred recommendation, directional fencing associated with the proposed crossing would be consistent with the Florida Panther Conservation Plan and, as appropriate, the Wildlife Crossing Memorandum (June 2022)
4. To mitigate at a ratio of two acres per one acre of impact for the loss of 52.14 total acres of occupied scrub jay territory on the Collier property and a ratio of four acres per one acre of impact for the loss of 15.75 acres of the Upland Management Area. FDOT will provide a total of 167.28 acres of occupied scrub-jay habitat (104.28 associated with the loss of two scrub-jay territories within the Collier Property + an additional 63 acres associated with potential habitat loss within the UMA = 167.28) as a conservation measure to compensate for the loss of scrub-jay habitat resulting from the Project.
5. To contribute \$10,000 to the USFWS Florida Bonneted Bat Fund.
6. To implement Caracara conservation measures. Land clearing activities for the project will be conducted outside of the Audubon's crested caracara nesting season (December 1 through April 30) to the greatest extent practicable. Since caracara nesting season is from December 1 through April 30, clearing should be completed between May 1 and November 30. Should it be necessary to conduct land clearing activities within the nesting season, the FDOT or their designated agent will survey suitable caracara nesting habitat to determine if an active caracara nest occurs within or adjacent to the project area. If an active nest is observed within 300 meters (985 feet) of the project area, land clearing within 300 meters (985 feet) of the nest will not occur until monitoring has determined the nest has either been abandoned, or chicks within the nest have fledged and left the nest site.
7. To complete the cumulative effects analysis

Please feel free to reach out if you questions.

Sincerely,

Thu-Huong Clark
Environmental Programs Administrator
Office of Environmental Management
Office: 850-414-5327
